



Planning Committee

Wednesday, 24 April 2024 at 6.30 pm

Council Chamber - Civic Centre

Members of the Committee

Councillors: M Willingale (Chair), P Snow (Vice-Chair), A Balkan, T Burton, V Cunningham, T Gates, E Gill, C Howorth, S Jenkins, E Kettle, A King, C Mann, M Nuti, S Whyte and J Wilson

In accordance with Standing Order 29.1, any Member of the Council may attend the meeting of this Committee, but may speak only with the permission of the Chairman of the Committee, if they are not a member of this Committee.

AGENDA

- 1) Any report on the Agenda involving confidential information (as defined by section 100A(3) of the Local Government Act 1972) must be discussed in private. Any report involving exempt information (as defined by section 100I of the Local Government Act 1972), whether it appears in Part 1 or Part 2 below, may be discussed in private but only if the Committee so resolves.
- 2) The relevant 'background papers' are listed after each report in Part 1. Enquiries about any of the Agenda reports and background papers should be directed in the first instance to **Democratic Services, Democratic Services Section, Law and Governance Business Centre, Runnymede Civic Centre, Station Road, Addlestone (Tel: Direct Line: 01932 425623). (Email: Democratic.Services@runnymede.gov.uk).**
- 3) Agendas and Minutes are available on a subscription basis. For details, please contact Democratic.Services@runnymede.gov.uk or 01932 425620. Agendas and Minutes for all the Council's Committees may also be viewed on www.runnymede.gov.uk.
- 4) Public speaking on planning applications only is allowed at the Planning Committee. An objector who wishes to speak must make a written request by noon on the Monday of the week of the Planning Committee meeting. Any persons wishing to speak should email publicspeaking@runnymede.gov.uk.
- 5) In the unlikely event of an alarm sounding, members of the public should leave the building immediately, either using the staircase leading from the public gallery or following other instructions as appropriate.

6) Filming, Audio-Recording, Photography, Tweeting and Blogging of Meetings

Members of the public are permitted to film, audio record, take photographs or make use of social media (tweet/blog) at Council and Committee meetings provided that this does not disturb the business of the meeting. If you wish to film a particular meeting, please liaise with the Council Officer listed on the front of the Agenda prior to the start of the meeting so that the Chairman is aware and those attending the meeting can be made aware of any filming taking place.

Filming should be limited to the formal meeting area and not extend to those in the public seating area.

The Chairman will make the final decision on all matters of dispute in regard to the use of social media audio-recording, photography and filming in the Committee meeting.

7) Commonly used acronyms:

ACEP	Assistant Chief Executive (Place)
ADM	Assistant Development Manager
BCM	Building Control Manager
CHPEBE or HoP	Corporate Head of Planning, Economy & Built Environment (also referred to as Head of Planning for brevity)
DLPM	Deputy Local Plans Manager
DM	Development Manager
PPSM	Planning Policy and Strategy Manager

List of matters for consideration

Page

Part I

Matters in respect of which reports have been made available for public inspection

1. **Notification of Changes to Committee Membership**
2. **Minutes** 4 - 9

To confirm and sign, as a correct record, the minutes of the meeting of the Committee held on 27 March 2024.
3. **Apologies for Absence**
4. **Declarations of Interest**

Members are invited to declare any disclosable pecuniary interests or other registrable and non-registrable interests in items on the agenda.
5. **Planning Applications** 10
 - a) RU.21/0207 - Land West of Byfleet Road (rear of 98-138 Byfleet Road), New Haw, KT15 3LA 11 - 61
 - b) RU.22/0393 - Longcross South, Longcross Road and Kitsmead Lane, Longcross, KT16 0EE 62 - 165
 - c) RU.23/1035 - 16 A Beechtree Avenue, Englefield Green, Surrey, TW20 0SR 166 - 190
6. **Exclusion of Press and Public**

Part II

There are no exempt or confidential items on this agenda.

Runnymede Borough Council

Planning Committee

Wednesday, 27 March 2024 at 6.30 pm

Members of the Committee present: Councillors M Willingale (Chair), P Snow (Vice-Chair), A Balkan, V Cunningham, J Furey (In place of J Wilson), T Gates, E Gill, E Kettle, A King, S Lewis (In place of C Howorth), C Mann, I Mullens (In place of S Jenkins), M Nuti, D Whyte (In place of T Burton) and S Whyte.

Members of the Committee absent: None

In attendance: Councillors A Berardi and MD Cressey.

68 **Notification of Changes to Committee Membership**

Cllr D. Whyte substituted for Cllr T. Burton, Cllr I. Mullens substituted for Cllr S. Jenkins, Cllr S. Lewis substituted for Cllr C. Howorth and Cllr J. Furey substituted for Cllr J. Wilson.

69 **Minutes**

The minutes of the meeting held on 28 February 2024 were confirmed and signed as a correct record.

70 **Apologies for Absence**

There were no apologies for absence.

71 **Declarations of Interest**

Cllr E Gill declared a non registerable interest in RU.23/1214 due to a close association with the applicant. Cllr Gill withdrew from the chamber and took no part in the determination of the application.

Cllr T Gates declared a pre-determined view in application RU.23/1214 which he had previously expressed publicly. Cllr Gates withdrew from the chamber and took no part in the determination of the application.

72 **RU.23/1214 - Fairmont Windsor Park, Bishopsgate Road, Englefield Green, Surrey, TW20 0YL**

Proposal: Retention of hotel including associated hardstanding. Retention of car park extension to include the change of use of this land, formerly used as a stable block. Demolition of Parkwood Estate buildings.

A presentation was made to the committee by the Planning case officer and legal officer. Prior to the meeting a technical briefing had occurred as well as a member site visit.

The committee was addressed by Irum Khan-Williams on behalf of objectors to the scheme, and by Mandip Malhotra who spoke on behalf of the applicants.

The item then moved to committee debate. The debate considered the relevant material considerations for and against the proposal, the officer report, and sought clarity on a number of issues.

A Councillor sought reassurance that the building had been built as shown on the retrospective plans, as there was now a lengthy enforcement history at the site. Officers advised that site visits had been carried out that included spot checks of dimensions. Further spot checks were undertaken on the member site visit where members chose dimensions to spot check. The spot checks had been conducted on different parts of the building and the results of the areas spot checked showed that the dimensions were relatively accurate and within reasonable tolerances for a building of this scale. These checks had gone significantly beyond what would normally be undertaken on such an application.

Committee members acknowledged that this weight to be attributed to matters in the planning balance was open to their planning judgement. Officers made it clear to the Committee that due regard would have to be given to all material planning considerations including the fallback position relating to the extant lawful planning permission on the site for a hotel that extant permission could be implemented at any time. It was also pointed out that the planning obligations with regards Parkwood were an entirely new material consideration that had not been considered in previous applications and would have to be given appropriate consideration. The Enforcement Notice provided a mechanism to secure compliance with that extant permission or any subsequent permissions that may be granted.

Clarification was sought on the economic and employment benefits of both the existing scheme and the additionality. A member felt that the economic benefits were potentially overstated and should be given less weight.

It was noted that the original Savill Court Hotel employed around 80FTE, and that the 2016 permission had suggested that a new hotel would provide circa 144 jobs at the location, and that figure was not revised in the 2018 application. The new hotel, as built, however employs a total of 243 FTE staff and this represents an increase of 99 jobs from the original estimates. A report on the economic benefits assessment by Lichfield's had deduced that some thirty-three jobs would be lost from not retaining the additional unauthorised floorspace, along with another 14 from the supply chain in the local area, as set out in section 7.13 of the officer report. Other economic benefits were identified in the Lichfields report. The assessment had concluded that the hotel was a significant employer on both a local and regional level with clear economic benefits.

A member queried how many employees were residents of Englefield Green, the number who lived in this village who were employed by the hotel was unknown, though 20% of the jobs were stated to be to people in the Borough. It was noted that the hotel was located on the very edge of the Borough and as such it was considered likely that a greater proportion lived locally but just not in RBC administrative area. In any case, the committee's role was to consider the public interest, employment was not only a Runnymede issue but also a regional and national issue. The function planning was to work in the public interest rather than purely the interest of single villages or LPA areas, with jobs and economic benefits forming part of local and national policy. This was considered to be a significant amount of employment, the jobs already existed and as such this was not the overselling of employment benefits by an applicant, as the level of employment can be seen already on the site.

A member considered that the officer report did not give sufficient weight to the economic benefits of the scheme and that the employment and economic benefits were worthy of greater weight in the planning balance. Members noted that many employees of the hotel had attended the planning meeting.

With regards green belt impact, a member queried whether multiple sets of individual substantial weight should be applied to any possible green belt harms in relation to each test of the Green Belt. Officers clarified that harm to the green belt must be given

substantial weight, though there could be a scale of harm within substantial and that was a matter for the decision maker. All the green belt harms would need to be weighed up and effectively a conclusion reached on the level of substantial harm caused to the Green Belt, this would then need to be clearly outweighed if the matter were to be approved. It would not however be correct or appropriate to apply three different and individual sets of substantial harm to different to three different purposes of the green belt.

In response to debate on Very Special Circumstances, it was clarified that the established case law was that very special circumstances were not required to be a single very special circumstance that outweighed the harm to the green belt and any other harms, but VSC could, and nearly always was (when achieved), a cumulative set of special circumstances that would be put forward in such applications.

For the reasons set out in the report, Officers believed that a cumulative very special circumstances case had been demonstrated which included the extant fallback hotel planning permission, the demolition and giving up of permissions at Parkwood Estate which had spatial and visual green belt benefits as well as the economic/employment benefits.

The weight to be given to material planning considerations was a matter for the decision maker which was in this case the committee. Whilst it was in the gift of the committee to re-balance these factors to potentially tip the balance the other way, if the committee chose to do so they should set out a clear planning rationale for how this decision was reached.

A member queried whether there was the potential prospect of further negotiations for further sacrifices or mitigation measures that the applicant could offer to further enhance the planning offer or the site's green credentials, particularly in the context of the Council recently declaring a climate emergency. Members were advised of the tests for planning obligations and conditions. It was explained that the purpose of the green belt is not to meet the Council's climate change objectives, and whilst climate change is a material consideration additional measures can only be required where they can be reasonably justified and is necessary for the award of permission. More demolition could only reasonably be secured if there was a genuinely held belief that they were required to offset the harms of the development. If they were required for punitive or overcompensation reasons then they could not be justified. The officer position was this was a fair Green Belt case being advanced, however members were entitled to take a different position, however if they did they would need to indicate why and what their reasons for requiring further measures were and what they sought to achieve.

It was further clarified that all planning applications were considered on their own merits, and the outcome of this application would be unlikely to have any significant bearing on any future application at the location or retrospective applications in general. The circumstances, planning considerations and nuances that made up this application severely limited the prospect of setting a precedent for green belt applications given the uniqueness of the combined set of circumstances. In any case this case was considered to have a number of benefits in green belt terms including reducing the number of clearly developed sites from one to two. In reality, all cases were judged on their individual merits.

In response to a member question on the loss of two dwellings as part of the demolition being at odds with the Council's housing need and queries around how legally binding any negotiations with the applicant would be, the Head of Planning advised that the loss of dwellings had counted against the application and this was set out in the planning balance in the officer recommendation. It certainly weighed against the scheme and was 'other harm' however it represented only a very modest contribution to the Council's housing need and so moderate harm was considered the correct amount of harm to attribute in this balance.

A legal agreement would be required with regards the demolition at Parkwood and giving

up of a previous approvals, and would be done with appropriate advice and legal drafting and would be suitably robust.

Disappointment was noted about the prospect of losing 39 staff bedrooms that formed part of the previous hotel scheme. It was advised that whilst it would have been desirable to have staff accommodation on the site, these bedrooms would have been a windfall that was proposed for operational reasons rather than being a policy requirement. It was noted that other measures such as the conversion of the Sun Pub into staff accommodation had also happened in the meantime.

With regards the enforceability of the measures, it was noted that the high-profile nature of the development would ensure a high degree of vigilance by interested parties as to the activities carried out at the site. It was likely that measures secured within the legal agreement would have monitoring or updating requirements within them. The Head of Planning was therefore confident that any non-compliance would be reported or identified and that measures promised in the application could be secured and delivered.

A member queried whether more the weight could or should be given to the 2015 Written Ministerial Statement. Officers advised that in their view appropriate weight had been given in the officer recommendation to the written ministerial statement from 2015 with regards green belt protection and unauthorised development, due to the opportunity to limit or mitigate the harm by putting forward a package of works. A member indicated that they considered greater weight should be attributed to the Written Ministerial Statement. It was advised there was very limited precedent for applying high levels of harm and weight to the statement for development of this type and that the circumstances of the case and the purpose of the statement. The written ministerial statement was displayed in full on the screen in the Council Chamber and the purposes of the Written Ministerial Statement were read out. It was considered that the application provided opportunity to limit or mitigate the harms caused to the green belt, and as such in the officer view, anything more than limited adverse weight would be difficult to justify or defend.

In response to a further member query, it was later added that it would not be lawful for the Council to seek compensation for the time and resources dedicated to both the handling of the application and the enforcement action.

Several members expressed frustration at the nature of retrospective planning applications whilst acknowledging the need to follow national guidelines by treating them as new applications. There was further frustration that this particular applicant was relatively recently in a similar position relating to a separate retrospective application, where the owner of the site indicated to the planning committee in 2022 that further breaches of planning control would be avoided.

It was noted that this particular breach was uncovered late in the 4 year enforcement window, and from satellite imagery that work had clearly started on this breach around 2018 and so this particular breach had been undertaken in advance of the other breach of the tree house planning control, and as such was not a new breach undertaken after the owners commitment to the planning committee to avoid further breaches made in 2022.

Officers shared the committee's frustration around retrospective planning applications, paid tribute to the residents who uncovered the breaches of planning control and added that stern words were had with the applicant upon the discovery of this breach, which were followed swiftly by an enforcement notice.

Another member considered that going from two clearly developed sites to one developed site would have clear green belt benefits.

It was acknowledged that in the event that the application was approved it would still be

subject to final review by the National Planning Casework unit.

The lengthy debate thoroughly considered both sides of the planning argument and a motion to approve the development was advanced. A named vote was requested on the application, and the voting was as follows:

For (7)

Cllrs M. Willingale, P. Snow, A. Balkan, V. Cunningham, J. Furey, S. Lewis, M. Nuti.

Against (4)

Cllrs A. King, I. Mullens, D. Whyte, S. Whyte.

Abstain (2)

Cllrs E. Kettle, C. Mann.

Resolved that –

The Head of Planning was authorised to grant permission subject to i) no ‘call in’ being received from the Secretary of State to whom the application needs to be referred under the Town and Country Planning (Consultation) (England) Direction 2021, ii) the completion of a legal agreement to ensure that the development approved under application RU.14/1599 or any other such relevant permission is not carried out and all demolition as detailed in the officer report is carried out as required, iii) subject to the Head of Planning being satisfied that issues relating to the protection of bats have been resolved to his satisfaction and iv) subject to the suitable conditions in general accordance with the conditions section 10 of this report and the addendum.

The HoP was also authorised to refuse permission should the legal agreement not progress to his satisfaction or if any significant material considerations arise prior to issuing the decision notice that in his opinion would warrant refusal of the application. The reasons for refusal should relate to the harm to the green belt, as well as any other further matters that the HoP considers have arisen (if any).

73 RU.24/0067 - 4 Glebe Road, Egham, Surrey, TW20 8BT

Proposal: Conversion of Garage into habitable accommodation (retrospective)

Members noted the proposals. Following a minor clarification there were no significant issues arising and the motion to approve as set down on the agenda was moved and passed.

Resolved that –

The Head of Planning was authorised to grant planning conditions subject to conditions 1-2.

74 RU.23/1544 - Oak Tree Farm, Lyne Lane, Chertsey, Surrey, TW20 8QP

Proposal: The Change of Use of existing buildings and land to dog day care (Sui Generis), including the recladding of existing buildings and provision of hard and soft landscaping.

[This application was considered prior to RU.24/0067 – 4 Glebe Road, Egham, Surrey, TW20 8BT]

The committee were supportive of the proposal, but concern was raised around the proximity of the site to the Bourne stream on the northern boundary. Officers advised that

discharge into watercourses was dealt with by Environment Agency permits and therefore not necessary for planning purposes, but an informative would be added to the scheme to encourage any discharge of waste to be done in an environmentally friendly manner, and for the applicant to consider the creation of a buffer zone into the Bourne.

Resolved that –

The Head of Planning was authorised to grant planning conditions subject to conditions 1-10 and additional informative around the creation of a buffer zone to the Bourne.

(The meeting ended at 8.50 pm.)

Chair

5. Planning Applications

The planning applications to be determined by the Committee are attached. Officers' recommendations are included in the application reports. Please be aware that the plans provided within this agenda are for locational purposes only and may not show recent extensions and alterations that have not yet been recorded by the Ordnance Survey.

If Members have particular queries on the applications, please contact Ashley Smith, Corporate Head of Development Management and Building Control by two working days before the meeting

Copies of all letters of representation are available for Members and the public to view on the Planning pages of the Council website

<http://planning.runnymede.gov.uk/Northgate/PlanningExplorer/GeneralSearch.aspx>.

Enter the planning application number you are interested in, and click on documents, and you will see all the representations received as well as the application documents.

(To resolve)

Background Papers

A list of background papers is available from the Planning Business Centre.

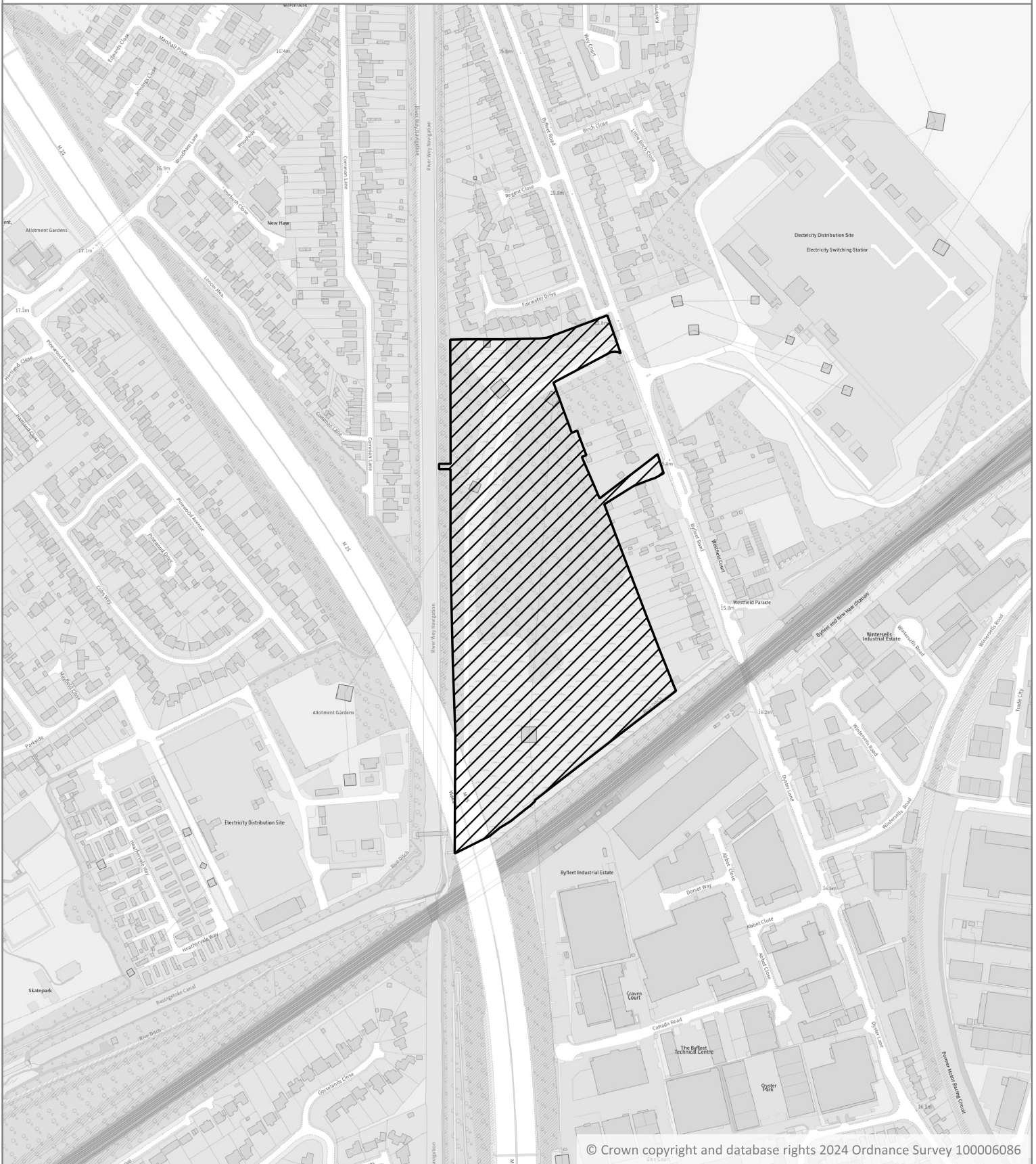


FOR LOCATION PURPOSES ONLY

Runnymede Borough Council
Runnymede Civic Centre
Station Road
Addlestone
Surrey KT15 2AH

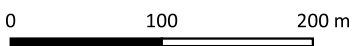
Date: 11/04/2024

Byfleet Road



© Crown copyright and database rights 2024 Ordnance Survey 100006086

Scale: 1:5,000



RU.21/0207



COMMITTEE AGENDA REFERENCE: 5A

APPLICATION REF:	RU.21/0207
LOCATION	Land West of Byfleet Road (rear of 98-138 Byfleet Road),New Haw, KT15 3LA
PROPOSAL	Development of the site to provide industrial, storage and distribution (Class E(g)/B2/B8) floorspace, with ancillary office accommodation, associated parking, landscaping and infrastructure works (Revised plans received 27/05/22)
TYPE	Full Planning Permission
EXPIRY DATE	Extension of time agreed to 29/05/2024
WARD	New Haw
CASE OFFICER	Katherine Appleby
REASON FOR COMMITTEE DETERMINATION	Major Development
<i>If you have questions about this report please contact Ashley Smith, Victoria Gibson or the case officer.</i>	

1. SUMMARY OF RECOMMENDATION

It is recommended the Planning Committee authorises the HoP:	
1.1.	To approve the application subject to the completion of a S106 under the Town and Country Planning Act 1990 (as amended) and relevant conditions.
1.2.	The HoP be authorised to refuse planning permission should the S106 not progress to his satisfaction or if any significant material considerations arise prior to the issuing of the decision notice that in the opinion of the HoP would warrant refusal of the application. Reasons for refusal relating to any such matter are delegated to the HoP.

2. DETAILS OF THE SITE AND ITS SURROUNDINGS

- 2.1. The site occupies an area of 7.54 ha and is located within the urban area and is a greenfield site comprised of open land with scrub vegetation and is bound on two sides by the River Wey to the west and the Rive Ditch to the south. The site is predominately flat with soft landscaping containing groups of trees, thick vegetation and grassed areas. There are two electricity pylons (four over the whole of the site) and a path that extends through the site to the river in the south with a fenced off electrical plant compound along the northern boundary. The site is located within flood zones 1, 2 and 3, with the south east corner of the site falling within flood zone 3, a central portion of the site falling within flood zone 2 and the remainder of the site falling within flood zone 1 and covered by

TPO422 (which comprises woodlands W1 and W2) along the northern and western boundaries with the M25 flyover to the west and to the south, the South West main railway line between Byfleet and New Haw Station and West Byfleet Station. The eastern and northern boundaries abut residential properties fronting Byfleet Road and Fairwater Drive. The West Weybridge Substation and its entrance is located east of the site as well as Addlestone Quarry which also uses the same entrance. Adjoining the site to the west is a non-statutory designated site, the Wey Navigation SNCI and the Basingstoke Canal SSSI which is also a conservation area. Part of the site is also located within the Biodiversity Opportunity Area. The site lies directly north of the established industrial and commercial areas of the Byfleet Industrial Estate and the Brooklands Centre. An area extending along the M25 motorway and into the application site close to the western boundary is located within the Runnymede Air Quality Management Area (AQMA).

- 2.2. Access into the site is via an existing access point off Byfleet Road located to the northeast of the site. The site was originally used to cater for the electrical cables that extend to and from the West Weybridge Substation located opposite to the site.

2.3. **BACKGROUND**

- 2.4. A previous similar application RU.19/0378 was submitted prior to the adoption of the new Runnymede Local Plan. It was subsequently withdrawn to allow time for the then emerging Local Plan to be further considered and to allow the applicant to consult further with the Council, relevant statutory consultees, and the local community.
- 2.5. The application site is included in the Council's 2016 Employment Land Review (ELR) and SHLAA 51 (2021). Following the adoption of the Runnymede 2030 Local Plan the application site is a site allocated for development by the Runnymede 2030 Local Plan, Adopted 16th July 2020 (by Policies SD2 and IE1).
- 2.6. In the context of the proposed development at the site, the applicant submitted a request to RBC for an Environmental Impact Assessment (EIA) Screening Opinion (LPA Ref. RU.20/1116). The proposed development was screened for a maximum of 17,515sqm of gross floorspace (GIA), comprising 15,317sqm of commercial / industrial floor space (Use Class E / B2 / B8) and 2,198sqm of ancillary office floor space in units ranging from 8.8m to 11m, with a maximum of 369 car parking spaces. The Council issued its formal Screening Opinion on 28 September 2020 confirming that an EIA was not required for the proposed development.
- 2.7. An online Public Consultation event was held prior to the submission of the application and responses received were in relation to traffic, building heights/layout, drainage and the demand for office space.

2.8. **APPLICATION DETAILS**

- 2.9. This is a full planning application for the development of 12 industrial units consisting of 17,128 sqm of floorspace (GIA): The applicants are applying for a flexible planning permission whereby each of the buildings could be used for a variety of industrial uses, these are as follows:

E(g)(ii) - Research and development (an example can include a research lab),

E(g)(iii) - Industrial processes, (being a use, which can be carried out in any residential area without detriment to the amenity of that area by reason of noise, vibration, smell, fumes, smoke, soot, ash, dust or grit),

B2 -(General Industrial) (an example can include vehicle repair company) and

B8 - (Storage or distribution) (many uses fall within this definition, such as warehouse used for distribution, a self-storage company, indeed the nearby Toolstation with a trade counter would fall under B8).

Ancillary office accommodation is also proposed.

All of the proposed units include car parking and loading areas for the direct unloading of materials into the commercial units. Permission is sought for up to 1,712sqm (10%) of the proposed development as Use Class B2 (General Industrial) floorspace, not inclusive of ancillary office space. The development is proposed in two phases, with the first phase comprising Units 2 – 9, and Phase 2 comprising Units 10 – 13.

- 2.10. A range of unit sizes are proposed to suit a range of business uses and sizes. Within Phase 1, units range from 730sqm to 2,486sqm GIA and in Phase 2 units range from 1,200sqm to 1,944sqm GIA. In total, 10,574sqm of employment floorspace is proposed on Phase 1, comprising industrial / warehouse and distribution floorspace and ancillary office space. Within Phase 2, 6,554sqm of industrial / warehouse and distribution floorspace and ancillary office space is proposed.
- 2.11. The proposed scheme delivers a total of 367 car parking spaces, spread evenly across the site, and each unit features its own car parking area. 227 car spaces are proposed as part of Phase 1 and the remaining 140 will be provided as part of Phase 2. Disabled parking provision will be provided in accordance with the standards, corresponding to a minimum of approx. 5% of overall parking provision. Landscaping including boundary treatments is proposed and lit, secure cycle parking (35 spaces) will be provided located close to the entrances to the units.
- 2.12. A new 5m wide segregated pedestrian/cycle path connecting Byfleet Road to the River Wey towpath is proposed. The western and eastern boundaries are lined with mature trees which offer effective visual screening of the site from its surroundings. The large tree belt to the north and western boundaries of the site would be retained.
- 2.13. The applicant is seeking planning permission for these buildings to have the ability to operate 24 hours a day, seven days a week.
- 2.14. Pre-application discussions have taken place regarding the proposed redevelopment of the site. Following these discussions the proposals were amended to incorporate a footpath to improve connectivity to the River Wey Navigation, and the proposed units were separated to reduce the bulk of the units. The current proposals result in a considerable reduction in overall floor area compared with the previous scheme from 19,632sqm to 17,128 sqm and changes in the massing and layout of the proposed units in order to minimise their impact on adjacent residential properties.
- 2.15. According to the applicant the constraints that dictate the layout are the electrical installations. It is these areas that have defined the development zone within the site. The TPO trees have been retained and are incorporated with the development layout which would include new landscaping. Flood risk has been accommodated within the drainage design to ensure the development does not restrict or increase the risk. Environment Agency access to the Rive Ditch has been retained to ensure riparian rights are preserved. The location of the residential properties has been considered in the proposed layout to reduce and restrict the risk of noise. The current site access requires amending to improve

ingress and egress on to Byfleet Road. The required changes will improve access to ensure traffic navigates effectively on to the existing network.

- 2.16. Following the submission of the application concerns were raised by National Grid in relation to the proximity of unit 1 to overhead lines and as a consequence of this the scheme was amended to reduce the footprint of the proposals and increase the setback from pylons running through the site which has resulted in a reduction in the proposed floor space and the removal of unit 1 and thus a reduction in the number of proposed units from 13 to 12 with associated changes to landscaping and car parking (and why the units are numbered 2-13). Other changes include the widening of the segregated pedestrian/cycle path connecting Byfleet Road to the River Wey towpath to 5m, amendments to the original flood compensation scheme and the relocation of a proposed toucan crossing.

3. RELEVANT PLANNING HISTORY

- 3.1. The following history is considered relevant to this application:

Reference	Details
RU.19/0378	Redevelopment of the site to provide Class B1c/B2/B8 floorspace, with ancillary office accommodation, associated parking, landscaping and infrastructure works.- Withdrawn 18/04/19

4. SUMMARY OF MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

- 4.1. The Borough's current adopted Development Plan comprises of the Runnymede 2030 Local Plan which was adopted on 16 July 2020 and the policies have to be read as a whole. The relevant policies are considered to be:
- SD1 – Spatial Development Strategy
 - SD2 – Site Allocations
 - SD3 – Active & Sustainable Travel
 - SD4 – Highway Design Considerations
 - SD5 – Infrastructure Provision & Timing
 - SD7 – Sustainable Development
 - SD8 – Renewable & Low Carbon Energy
 - SL1 – Health and Wellbeing
 - EE1 – Townscape and Landscape Policy
 - EE2 – Environmental Protection
 - EE9 – Biodiversity, Geodiversity and Nature Conservation
 - EE11 – Green Infrastructure
 - EE13 – Managing Flood Risk
 - Policy IE1: Employment allocations
 - Policy IE2: Strategic Employment Areas
 - Policy IE3: Catering for modern business needs

Other Material Considerations

- 4.2. National Planning Policy Framework (revised December 2023)- acts as guidance for local planning authorities and decision-takers, both in drawing up plans and making decisions about planning applications. At the heart of the NPPF is a presumption in favour of sustainable development. The document, as a whole, forms a key and material consideration in the determination of any planning permission. The supporting National Planning Policy Guidance (NPPG) is also a material consideration for decision making, as is the National Design Guide (2019) and the Nationally Described Space Standards (2015).
- 4.3. SPDs which can be a material consideration in determination:
- Runnymede Borough Parking Guidance (2022)
 - Runnymede Design Supplementary Planning Document (2021)
 - Green and Blue Infrastructure Supplementary Planning Document (2021)
 - Thames Basin Heaths Supplementary Planning Document (2021)
 - Infrastructure Delivery and Prioritisation (2020)
 - Parking Strategy: Surrey Transport Plan (2020)
- 4.4. Other material considerations include the Runnymede Strategic Flood Risk Assessment (2016 and 2017)

5. CONSULTATIONS CARRIED OUT

Consultees responses

Consultee	Comments
Natural England	No objection
Environment Agency	No objection subject to conditions
National Highways (formerly Highways England)	No objection subject to conditions
Basingstoke Canal Authority	No objection
The National Trust – Wey Navigation	No comments received
SCC County Highway Authority	No objection subject to conditions
SCC Lead Local Flood Authority	No objection subject to conditions
SCC Archaeology	No comments received
SCC Rights of Way	No comments received
RBC Arboricultural Officer	No objection subject to conditions
RBC Contaminated Land Officer	No objection subject to conditions
RBC Environmental Health Officer	No objection subject to conditions

RBC Deputy Direct Services Manager	No comments received
RBC Planning Policy	No objection subject to conditions
RBC Heritage Officer	There will be some harm to the Conservation Area however this harm is at the lowest end of the scale. <i>Officer Comment – subject therefore to the development resulting in public benefits that over weigh this small amount of harm the proposal can be considered to comply with the NPPF (Heritage Assets)</i>
National Grid	Object to the proposal which is in close proximity to High Voltage Transmission Overhead Lines and Underground Electricity Cables.
Adjoining Authority Woking BC	No objection subject to no impact on Nos.1-17 Gorselands Close, which are the closest residential receptors within Woking Borough. Surrey County Council, in their capacity as the County Highway Authority, should also be satisfied that the proposal would not give rise to an unacceptable impact on highway safety and that the residual cumulative impacts on the road network would not be severe, particularly upon nearby Oyster Lane (A318) and Parvis Road (A425).
Network Rail	No objection
South West Trains	No comments received
UK Power Networks	No objection
SSE Power Distribution	No comments received
Surrey Wildlife Trust	No objection subject to conditions
Surrey Bat Group	No objection subject to condition
Thames Water Utilities	No objection subject to condition
West Surrey Badger Group	No objection

Representations and comments from interested parties

- 5.1. 192 Neighbouring properties were consulted in addition to being advertised on the Council's website in the local press, and by notices displayed at the site and 230 letters of representation have been received in regard which can be summarised as follows:
- Flooding
 - Layout, density and siting of building(s)
 - Overlooking/loss of privacy/visual harm/shadowing
 - Visual impact on local area
 - design of the proposal
 - Impact on the character and appearance of the area
 - Impact on the amenities of neighbouring properties
 - Construction impacts
 - Impact on the amenity of the River Wey Navigation Conservation Area, recreation resource

- Noise and disturbance from proposal
- Impact on air quality
- External design and appearance and materials
- Impact on road network including servicing vehicles
- Adequacy of parking/loading/turning
- Road access and highway safety
- Nature conservation and loss of trees
- Landscaping

5.2. I letter of objection has also been submitted on behalf of 28 (plus 4 residents wishing to remain anonymous) local residents which can be summarised as follows:

- the lack of adherence by the applicant to national and local planning policies and guidance.
- the lack of engagement and advice sought from Statutory Consultees and local residents.
- the lack of understanding of the impact of the proposals, not only on residents, but the whole infrastructure of the site and surrounding area, particularly its vital natural environment with respect to flooding / climate change; and
- the adverse impact on travel and pollution in an already very congested part of Runnymede Borough.

5.3. A further letter of objection has been submitted from the New Haw Residents Association which includes a petition from around 127 local and non-local residences which can be summarised as follows:

- there will be an enormous increase in traffic, particularly from large HGV's and delivery vehicles.
- loss of open space for exercise
- loss of wildlife habitat

5.4. A letter of objection has also been received from the Byfleet, West Byfleet and Pyrford Residents' Association which can be summarised as follows:

- The revised Plans do little if anything to address the many concerns.
- This proposal is unfit for purpose in the chosen location.
- The Application talks about redevelopment of the site, but this land has never been developed.
- Additional trips on the local highway network would be around 734 per day including 61 HGVs.
- The single access and exit is dangerous.
- The environmental impact on the surrounding community will be so detrimental that it almost does not bear contemplating.
- Flooding issues
- Loss of open green space.

5.5. 38 residents have also signed individual generic letters of support which can be summarised as follows:

- The site is currently unused and contributes nothing to the local area.
- The plans will provide high quality new business space, bringing hundreds of much needed jobs and investment to Runnymede.

- The site has been allocated for employment use by Runnymede Borough Council's Local Plan
 - The development will include substantial landscaping and sustainability measures.
 - The applicant has made changes to the layout and building heights in response to local feedback
- 5.6. Cappagh – raises concerns regarding its shared access and the off-site highway works associated with the proposed Toucan (signalised) crossing immediately south of their access which will adversely affect the safety and use of that access – (The proposals have since been amended to provide a Toucan crossing to the north of the application site access – Despite being advised of this, no further comments have been received from Cappagh)

6. PLANNING CONSIDERATIONS

6.1. In the determination of this application regard must be had to the Development Plan and National policy within the NPPF. The application site is located within the urban area where the principle of such development is considered to be acceptable subject to detailed consideration. This must be considered in light of the presumption in favour of sustainable development advocated by the NPPF. The following key planning matters are also considered relevant:

- Principle of development and the presumption in favour of sustainable development
- Design Considerations
- Impact on the setting of the adjacent Conservation Area
- Highways Considerations
 - Highway Capacity
 - Proposed access and wider highway safety considerations
 - Parking Considerations
 - Sustainable transport/ highways capacity considerations
 - Highways Conclusion
- Impact on Neighbouring Amenity
 - Potential impact in terms of noise and disturbance
 - Potential impact in terms of overlooking, loss of privacy and /or overbearing impact
 - Potential impact in terms of lighting
 - Neighbouring Amenity Conclusions
- Flooding Considerations
 - The need for the Sequential and Exception Test
 - Flood protection and mitigation
 - Sustainable Urban Drainage
- Ecology and biodiversity
- Renewable Energy
- Other Considerations
 - Air quality
 - Contaminated Land
 - Archaeology
- Other matters

6.2. **Principle of development and the presumption in favour of sustainable development**

- 6.2.1. In the determination of this application regard must be had to the Development Plan and National policy within the NPPF. The Runnymede 2030 Local Plan was adopted on 16th July 2020 and within this Policy SD1 of the Local Plan advises that a business park in the region of 20,000sqm has been allocated at Byfleet Road, New Haw to meet identified employment need during the period of the Local Plan (2015-2030). The application site is also included in the Council's 2016 Employment Land Review (ELR) and SHLAA (2021).
- 6.2.2. Policy SD2 states that sites listed within this policy are expected to deliver the level of development subject to complying with specific requirements set out in the individual site allocation policy which in this case is Policy IE1 which has allocated the site for development to provide in the region of 20,000 net additional sqm of B1C/B8 floorspace.
- 6.2.3. Policy EE13 of the Local Plan is also clear that development proposals must also avoid causing flood risk whether on site or elsewhere. Any proposal that might lead to additional flood risk either on site or elsewhere, would be considered contrary to the development plan. Following the review of new information in the revised Level 2 Strategic Flood Risk Assessment (SFRA) The Environment Agency raised no objection in principle to the development of 'less vulnerable' use within the site subject to it not causing additional flood risk, either on the site or elsewhere.
- 6.2.4. Local Plan Policies IE1 and EE13 provide a strong and clear basis upon which to enable employment development of the site whilst being able to refuse any planning application which fails to agree an appropriate scheme of flood mitigation and compensation measures.
- 6.2.5. Policy IE3 seeks to attract businesses to the Borough; support the retention, creation and development of local businesses, promote business competitiveness and allow for flexibility to cater for the changing needs of the economy including by encouraging a range of types and sizes of new employment floorspace and supporting provision of small warehousing units. Paragraph 8.22 of the supporting text states that "it is essential that existing Small Medium Enterprises (SME) are supported and new business encouraged so that a thriving business ecosystem is nurtured". A range of units are proposed, ranging from 730sqm to 2,486sqm. The smaller units in particular will be well suited to the requirements of small to medium sized enterprises (SMEs) and new emerging industrial sectors. Furthermore, should market conditions require additional smaller industrial units, the units proposed as part of this scheme could be further subdivided to suit this purpose.
- 6.2.6. According to the submitted Planning Statement in support of this planning application the proposed development will result in the creation of up to 413 Full Time Equivalent (FTE) new jobs on the site during operation. These are in addition to jobs created through the construction phase. This increase in local workforce will result in significant economic and social benefits, including through increased local spending.
- 6.2.7. In addition to the above, paragraph 81 of the NPPF (2021) sets out that decisions should help create the conditions in which businesses can invest, expand and adapt. The NPPF also states that significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development.
- 6.2.8. The proposal is for the redevelopment of the site to provide a total of 17,128 sqm of flexible employment floorspace which fall under the use class order of being Classes E(g), B2 (General industrial) and/or B8 (storage and distribution) use, with ancillary office accommodation associated parking, landscaping and infrastructure works. In

terms of the principle of the proposed development, the policies seek to support proposals such as this which seek to intensify employment generating uses. As such the principle of the development accords with the Development Plan policies and in particular Criteria a) and b) of Policy IE1 which will help to deliver Local Plan Objective 12 and is appropriate development subject to wider considerations set out below.

6.3. Design Considerations

6.3.1. Policy EE1 sets out that all development proposals will be expected to achieve high quality and inclusive design which responds to the local context including the built, natural, and historic character of the area while making efficient use of land. Development proposals will be supported where they:

- Create attractive and resilient places which make a positive contribution to the Borough's townscape, public realm and/or landscape setting.
- Create developments which promote social interaction and design out crime.
- Contribute to and enhance the quality of the public realm/ and/or landscape setting through high quality and inclusive hard and soft landscaping schemes.

6.3.2. The NPPF (2023) sets out that there is a clear focus that proposed developments should function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development and are visually attractive as a result of good architecture, layout and appropriate and effective landscaping. The NPPF (2023) sets out that the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve, with an emphasis that development should function well and add to the overall quality of the area and are visually attractive as a result of good architecture, layout and appropriate and effective landscaping.

6.3.3. Although the site is currently undeveloped and whilst there are some residential properties to the east, north and west of the site, to the south of the site sits the established industrial and commercial areas of the Byfleet Industrial Estate and the Brooklands Centre, which offer a mix of employment uses and the main railway line to Byfleet & New Haw railway station, with the Byfleet Electricity Switching Station and Addlestone Quarry to the east and the M25 flyover to the west. The site is surrounded by commercial, industrial and residential uses and as such, the proposal would not have a significant urbanising effect in a previously non urbanised area. There are also a number of physical constraints on the site which dictate where development can take place and which impact the form of development which include the principal access from Byfleet Road, the pylons with overhead electricity wayleaves, flooding, the River Wey to the west, the Rive Ditch to the south, and railway line to the south, the M25 flyover and the protected trees. However, the site is relatively level and regular in shape, which is able to accommodate the rectangular proportions ideal for the type of buildings proposed.

6.3.4. Although the buildings would have a utilitarian design, the elevations of the proposed units would vary and would be built in high quality, contemporary long lasting materials and would also be designed to suit the varying and changing demands of the industrial, distribution and logistics industry. A combination of metal cladding profiles and colours have been incorporated in the warehouse design to break up the elevations. The office entrance areas incorporate curtain walling and glazed entrance doors and the walls comprise horizontal profile cladding panels in various colours, separated by vertical and horizontal flashings and ribbon windows that visually break the walls into smaller components.

- 6.3.5. Phase 1 would comprise 4 individual blocks with various sized units and Phase 2 would comprise 1 block with 4 units. The scale and massing of the proposed buildings varies across the site providing flexibility for future occupiers as well as to respond to the constraints across the site, such as the existing pylons and to protect the amenity of nearby residents.
- 6.3.6. The site is visually well contained by the existing established belts of woodland and individual trees on the northern and western boundaries and at the site entrance off Byfleet Road, by the adjoining railway embankment with associated wooded slopes to the south, the M25 flyover to the west and Byfleet Road residential area and associated garden trees and hedges to the east. In terms of landscaping, landscaping plans and a landscaping maintenance and management report have been submitted in support of this planning application. More importantly the existing site boundaries and woodland which surrounds this part of the site would not be affected by the proposal. Thus, whilst these buildings have large floorplates and scale, they will have limited visual impact within the wider street scene. Whilst they have been designed to be industrial buildings, their size, scale and bulk mean that they would not be an overly prominent, dominant and visually overbearing form of development. Full details can be secured by way of recommended conditions. Further biodiversity enhancements and strengthening of the existing woodland areas and landscaping are discussed further below. As such the impact on the character and appearance of the area in general is considered acceptable.

6.4. Impact on the character of the adjacent Conservation Area

- 6.4.1. Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires that special attention be paid in the exercise of planning functions to the desirability of preserving or enhancing the character or appearance of the Conservation Areas. The NPPF (2021) states that in determining planning applications, local planning authorities should take account the desirability of new development making a positive contribution to local character and distinctiveness. When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation. The more important the asset, the greater the weight should be.
- 6.4.2. Policy EE5 of the Local Plan also sets out that development within or affecting the setting of a Conservation Area, including views in or out, should protect, conserve, and wherever possible enhance, the special interest, character, and appearance of the Conservation Area.
- 6.4.3. The site is adjacent to the Towpath and River Wey Navigation Conservation Area (designated August 1999). The designation of the Runnymede section of the Wey Navigation as a Conservation Area formed part of the comprehensive strategy to designate a linear conservation area along the total length of the Wey and Godalming Navigations. It was considered that this whole area, some 20 miles in length, merited Conservation Area designation by reason of its antiquity, appearance and special quality.
- 6.4.4. The proposed 12no. structures, by reason of their scale, massing, design, and materiality, would be industrial in character and appearance. Although this reflects the industrial history of the River Wey (particularly in the nineteenth century), this section of the conservation area is no longer characterised by industrial or commercial activity, and it would be in stark contrast to the existing and historic undeveloped and open character of the Site as it exists today. As the buildings would be visible from within the conservation area, particularly in winter months, they will have an adverse visual impact on the setting of the conservation area.

- 6.4.5. The loss of the open and green character of the Site, the loss of views across the Site, and the introduction of industrial buildings would result in an adverse impact on the setting of the conservation area and less than substantial harm to the heritage asset, making Paragraphs 206, 208 and 212 of the NPPF relevant. Given that the impacts are indirect (although it is acknowledged that there will be minimal direct impacts arising from the proposed new footpath which will link Byfleet Road to the Wey) and that the impact is confined to a relatively small section of the conservation area, it is considered that the harm is at the lowest end of the scale.
- 6.4.6. As such, it is considered that proposals would fail to preserve or enhance the significance of the Wey Navigation Conservation Area and the proposals would constitute a scheme which would lead to 'less than substantial' harm to the designated heritage asset. Whilst this harm is considered to be limited it remains that the NPPF (2023) is clear that an assessment is therefore required to weigh this harm against the public benefits of the proposal.
- 6.4.7. The National Planning Policy Guidance on Historic Environment sets out that public benefits could be anything that delivers economic, social or environmental objectives. It is clear that they should be of a nature or scale to be of benefit to the public at large and not just be a private benefit. However, benefits do not always have to be visible or accessible to the public in order to be genuine public benefits.
- 6.4.8. One of the key benefits of this site is the provision of allocated strategic employment land which has been designated for such uses in the Council's Local Plan. The other public benefits which flow from the development are largely the economic benefits of the proposed development including the creation of construction and operational jobs. Overall and given the level of harm associated with the impact on the significance of the Conservation Area it is considered that the public benefits outweigh the harm. As such the proposed development is considered acceptable adjacent to the Conservation Area.

6.5. **Highways Considerations**

- 6.5.1. Policy SD4: Highway Design Considerations states that the Council will support development proposals which maintain or enhance the efficient and safe operation of the highway network and which take account of the needs of all highway users for safe access, egress and servicing arrangements. The NPPF (2023) is clear that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.
- 6.5.2. The applicant is seeking planning permission for a "flexible" employment use where a variety of different end users could occupy the proposed buildings. Future users are unknown, and it is important, in supporting employment generating uses that permission allows for flexible uses to occupy the building. However, this in turn means ensuring that the matters regarding highway safety, capacity and parking are fully considered from the onset.

Highway Capacity- residual cumulative impacts on the road network.

- 6.5.3. One of the key areas of discussion is the activities associated with the proposed use of these buildings. In highways terms one of the considerations is the impact on both the

local road network and the wider strategic network (which includes local motorways and junctions- i.e., the M25 and Junction 11, and to some lesser extent the A3 and the Cobham junction). Again, it is important to highlight that the NPPF is clear that a refusal on cumulative impacts on the road network should only be where it is demonstrated that a proposal would have a severe impact.

- 6.5.4. When this site was allocated under Policy IE1 for employment use in the Local Plan, high level highways capacity work was undertaken to assess the potential impact the proposed Plan would have on existing traffic networks, particularly given the Council's strategy to include this site for employment generating uses. However, given this was high-level and the varying uses which could take place for an employment generating use it is necessary to assess in further detail.
- 6.5.5. The manner in which this is modelled is based on using Trip Rate Information Computer System (TRICS) which is an industry recognised standard for assessing trip generation of new developments. The Transport Evidence submitted by the applicant includes all the supporting TRICS information to understand fully the assumptions which have been made. This has all been reviewed by the Highway Authority. The applicants have modelled the potential trip generation of all buildings operating in the above uses. The assessment is also one where you look at potential vehicle trips, with a focus on peak hours. The focus on peak hours is to assess if the proposed development would result in increased pressures on the local and strategic highway network, notably at peak rush hour times where there is inherently a greater level of traffic. That is not to say that there would be no vehicle movements outside of peak hours, only that the potential impact on highway capacity would not be as great an issue. The numbers being referred to are 12-hour (7-7) as the TRICS outputs do not provide 24-hour rates and most surveys are 12-hour only. To provide a robust assessment, the higher vehicle trip generation figures in Table 6 have been used for assessment purposes in the submitted Transport Assessment.
- 6.5.6. The proposals would generate a significant level of vehicle movements with employees coming to and from said offices, particularly during peak times (08:00-09:00 and 17:00- 18:00). The TRICs data in Table 6 below shows that as an industrial estate during the morning peak there could be up to 74 vehicle trips in the AM peak (largely those coming to the site) and around 57 vehicle trips in the PM peak (largely those leaving the site) and up to 6 HGV trips in the AM peak and around 2 HGV trips in the PM peak.

Table 6 – Vehicle Trip Rates and Trip Generation (Industrial Estate)

	Morning Peak (08:00 – 09:00)			Evening Peak (17:00 – 18:00)			12 Hour (07:00 – 19:00)		
	IN	OUT	TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL
Vehicles									
Trip Rates per 100sqm	0.271	0.152	0.423	0.083	0.241	0.324	2.068	2.127	4.195
Trip Generation	47	27	74	15	42	57	362	372	734
HGVs									
Trip Rates per 100sqm	0.015	0.018	0.033	0.006	0.008	0.014	0.173	0.178	0.351
Trip Generation	3	3	6	1	1	2	30	31	61

Summary Table: vehicle trips modelled

- 6.5.7. In terms of HGVs, it has been assumed that the vast majority of traffic would route to/ from the north via Byfleet Road due to the 2.9m height restriction at the railway bridge to the south. Different locations for the scope of assessment were agreed with Surrey County Council in their role as the highway authority and undertaken for the existing situation and for a 2025 future year at full build out which included the site access, New Haw Road/ Byfleet Road/ Woodham Lane Roundabout, Byfleet Road Railway Bridge and the A425/ A318 Roundabout. No reduction in trips to allow for future mode shift as a result of the implementation of a Travel Plan was included within these assessments, to provide a robust analysis.
- 6.5.8. As set out in the consultation response from the highway authority, it is recognised that the proposed use of the site is expected to generate additional trips on the local highway network. It is also recognised that currently congestion occurs towards the Railway Bridge, where this can bottleneck during peak hours due to the traffic lights and single lane access under the bridge. However, it is also the case that the Byfleet Road the access to the application site is an A class road, the A318. This is a local distributor route, which by its nature carries significant levels of traffic on a daily basis. Considering the impact of this sites redevelopment in this context, it is considered that the resultant increase in traffic on the capacity of the network is acceptable and will not result in a severe impact.

Proposed access and wider highway safety considerations

- 6.5.9. Vehicle access to the site is currently achieved through the access point in the north east. This access junction is to be improved as part of the proposed development to facilitate safe and efficient vehicle access and egress.
- 6.5.10. A Stage 1 Road Safety Audit (RSA) of the site access proposals was originally undertaken which considered two potential crossing arrangements on the A318 Byfleet Road to the south of the site access. Following concerns raised regarding possible conflicts between the crossing arrangements and the existing access from Addlestone Quarry, a further Stage 1 Road Safety Audit (RSA) was carried out and the proposed Toucan crossing has been relocated to the north of the proposed access that will benefit pedestrians and cyclists and will enable the crossing of Byfleet Road in safety.
- 6.5.11. Significant works are required in order to ensure that HGV access only takes place according to a right turn in, left turn out arrangement (There would be a 'Turn Left All HGVs' sign at the site entrance). This is to limit the risks associated with bridge strikes for the railway bridge adjacent to Byfleet & New Haw Railway Station. Additional signage will also be provided to forewarn drivers of both approaches to the bridge along Byfleet Road and Oyster Lane.
- 6.5.12. The Transport Assessment also reviewed personal injury collision data for the local highway network which does not highlight any existing safety issues that would need to be mitigated as part of the development proposals. Nevertheless, due to the number of railway bridge strikes in previous years on Byfleet Road to the south of the site a S106 contribution will be made to improving warning signage on the approach to the railway bridge, which would include electronic displays (VAS – Over Height Vehicle Activated Signs), to reduce the number of larger vehicles that may attempt to use that route.
- 6.5.13. Highways England has been consulted as it has an interest in the M25 and a small part of the motorway and its supporting pillars below are located in the south western corner of the site. They have raised no objection to the proposal. However, the proposal will be required to maintain Highways England Access for maintenance.

- 6.5.14. Overall, in terms of highway safety it is considered that the Applicant has demonstrated that an access can be constructed that will comply with current highway standards and provides appropriate visibility to ensure that both users of the access and users of Byfleet Road will not be compromised. As such the proposal is considered acceptable in this regard.

Parking Considerations

- 6.5.15. Policy SD4 of the Local Plan states that parking standards for vehicle and cycle parking within development proposals will be assessed against the Council's current adopted guidance. The Council have recently adopted Parking Guidance SPD in November 2022. This guidance sets out recommended parking standards for different uses. However, as set out above the uses being sought for permission vary in terms of the need for parking provision. The recommended parking for a B2 (general industry) use is 1 car space per 30sqm with no lorry parking required and a warehouse (distribution) use would require 1 car space per 70 sqm with 1 lorry space per 200sqm. This SPD sets out that some larger scale non-residential developments may benefit from a bespoke car parking scheme, appropriate to that use and/or its location, particularly when taking account of other policies and practices in place and which are associated with the operation of the development. In such circumstances, a site-specific parking and travel plan can take detailed account of the location of the development, the ability of people to walk, cycle or travel by public transport to the development and the policy of the institution to provide or subsidise public transport services, and/or restrict car travel to their site. It is considered that this is one of such planning applications where a bespoke car parking scheme is necessary in order to ensure that the correct level of parking is provided to cater for such a wide-ranging uses which could take place.
- 6.5.16. Parking is shown to be provided throughout the site predominantly in front of each of the units and along the sides of units 2,10 and 13 with a total of 367 car parking spaces being proposed across the site. 227 car spaces are proposed as part of Phase 1 and the remaining 140 will be provided as part of Phase 2 with a total of 15 docking bays for HGVs. Disabled parking provision will be provided in accordance with the standards, corresponding to a approx. 5% of overall parking provision.
- 6.5.17. The applicant has stated that they are committed to providing Electric Vehicle Charging Points at the site. Under the Council's Parking SPD 20% of the spaces need to be EV fast charging and a further 20% need to be passive. The wider area is currently experiencing existing electric power supply issues. Therefore, it is proposed to provide 10% EV fast charging points which will be increased as soon as the electric power supply to the site allows. In order to deal with this matter the applicants have also subsequently agreed that all-other parking space (including those for lorries) will be fitted out as passive EVC spaces in the form of EV ducts running to each car parking space, so that pillars and cables can be connected at any time in the future. This would go beyond the 20% passive provision required by planning policy. This is one measure which they are seeking to go beyond policy to "future proof" the buildings so that parking for the site is adaptable to future needs as required. This can be secured by way of condition.

Sustainable transport/ highways capacity considerations

- 6.5.18. Policy SD3 of Local Plan deals with Active and Sustainable Travel. This sets out that the Council will support proposals which enhance the accessibility and connectivity between people and places by active and sustainable forms of travel. This includes supporting developments which integrates with or provide new accessible, safe and attractive active and sustainable travel networks and routes to service and

employment centres and rail interchanges. The policy also requires proposals which generate significant traffic movement to submit and implement Travel Plans demonstrating how active and sustainable travel options have been considered.

- 6.5.19. Ultimately a number of the likely uses associated with the proposed development could result in increased deliveries and HGV movements. The impact of this in terms of highway capacity or highway safety is considered above. However irrespective of the proposed uses there would be a number of employees working at the proposed buildings and it is important to support that future users seek to utilise sustainable modes of transport.
- 6.5.20. The site benefits from connection to an existing network of footways within the vicinity of the site. The A318 Byfleet Road, from which the site takes access, is subject to a 30mph speed limit and is lit, with a 2.5m shared foot/cycleway running along the eastern side of the carriageway. A footway is provided intermittently along the western side of Byfleet Road. The closest bus stops to the proposed development are located approximately 130m north of the site access. These are served by the no. 593. Additional bus services can be accessed circa 650m to the north on New Haw Road/Woodham Lane. Byfleet & New Haw Railway Station is located approximately 300m to the south of the site access. The station, which is operated by South Western Railway, provides regular and direct connections to London, Waterloo and Guildford. Overall, the site is in fairly sustainable location where active and public transport modes can be utilised by those who work at the site.
- 6.5.21. The proposals have been revised and a 5m wide segregated pedestrian/cycle path from Byfleet Road connecting to the Wey Navigation towpath and hence to the Basingstoke canal opposite via the footbridge over the Wey is proposed. This in turn will provide connection to National Cycle Route (NCR) 221 which runs from West Byfleet to Brookwood along the Basingstoke Canal to the south-west of the development site. This will open up an alternative route for walkers and cyclists, avoiding the busy Parvis Road between West Byfleet and Byfleet.
- 6.5.22. A Framework Travel Plan (FTP) has been submitted in support of this planning application which sets out a strategy for facilitating and encouraging travel by non-car modes, focussed on reducing single occupancy car trips. Prior to occupation of the site, the developer will appoint a site-wide Travel Plan Co-ordinator (TPC) to implement the FTP. The future occupiers of the site will need to appoint a member of staff to work with the TPC in implementing and monitoring the impact of the FTP. This will include introducing measures to encourage and facilitate travel by sustainable modes of transport. Cycle parking will be provided in accordance with the Council's standards and will be lit, secure and appropriately signed, and located close to the entrances to the units and to support this cycle storage and shower facilities are proposed where appropriate. This can be secured by way of condition. The Surrey CC Travel Plan Officer has reviewed the draft Travel Plan and has raised no objections.
- 6.5.23. In terms of public transport, travel packs are proposed for new employees to make them aware of options. In terms of monitoring and reporting it is suggested that the travel plan last for a 5 year period from first occupation but if the identified targets are not met in Year 5 then monitoring will continue to Year 9. The requirement of the overall travel plan would need to be secured by way of a planning obligation.

Highways Conclusion

- 6.5.24. In conclusion, the transport movements associated with various uses which could take place at this site have been modelled. In considering the potential impact of the proposed development on the local highway network, assessments were undertaken

at a number of agreed locations. Overall, the proposal would have an acceptable impact in terms of highway capacity and certainly would not have a severe impact on the highway network (which is the test for the impact on highway capacity). In terms of access, it is considered that the proposal would not raise issues in terms of highway safety (subject to appropriate mitigation) and provides suitable level of parking.

- 6.5.25. In addition the proposal would provide measures to encourage active or sustainable travel through a Toucan crossing and a footway/cycle way link that will benefit pedestrians and cyclists and will enable the crossing of Byfleet Road in safety. It would also provide additional opportunities, through the proposed Travel Plan, for people to travel by means of transport other than the private car.

6.6. **Impact on Neighbouring Amenity**

- 6.6.1. As set out above the proposal is for flexible employment use and the operations which could take place under these employment uses vary significantly. The applicants are looking for the buildings to operate as flexibly as possible and that means the operations could take place 24 hours a day, seven days a week, however this will very much depend on the end user. As the end user is currently unknown the following officer assessment, in terms of impact on neighbouring amenity, is taken on a worst-case scenario.

- 6.6.2. The closest residential properties potentially affected by the proposed development include the two storey predominantly detached dwellings located to the east of the site, most notably numbers 98-138 Byfleet Road which are set back from the road frontage. To the north of the site are the two storey detached properties numbers 72 and 74 Byfleet Road and to the rear of these numbers 1-7 Fairwater Drive. To a lesser degree numbers 103 -119 Byfleet Road, Westfield Court, 3 storey high Westfield Parade and 2½ storey high flatted block 1-5 White Oaks with dwelling numbers 7-9 behind and 55-69 Byfleet Road which are sited opposite and to the north of the site. Across the canal are properties 64 – 92 Common Lane.

6.7. Potential impact in terms of noise and disturbance

- 6.7.1. In addition to the above considerations the justification for policy EE1 highlights how the Government's Noise Policy Statement (NPSE) for England sets out the importance of promoting good health and quality of life through the effective management of noise in relation to sustainable development. The NPPG on Noise also sets out a noise exposure hierarchy of when action including mitigation, avoidance or prevention is likely to be required where external noise impacts exceed the Lowest Observed Adverse Effect Level (LOAEL) or the Significant Observed Adverse Effect Level (SOAEL).

- 6.7.2. The NPPF requires new development to be appropriate for its location taking into account the likely effects (including cumulative effects) of pollution on health, living conditions and the natural environment, as well as the potential sensitivity of the site or the wider area to impacts that could arise from the development. In doing so they should mitigate and reduce to a minimum potential adverse impact resulting from noise from new development – and avoid noise giving rise to significant adverse impacts on health and the quality of life.

- 6.7.3. The NPPF makes reference to the NSPE, but neither of these documents, nor the PPG section on Noise, provide any numerical definitions of LOAEL or SOAEL. Rather, these must be judged on the circumstances of each individual case. That said, the NPSE does refer to the World Health Organisation (WHO) Guidelines for Community Noise (1999)76, which suggests that guideline values for amenity which relate to external

noise exposure are set at 50 or 55 dB(A)⁷⁷, representing daytime levels below which most of the adult population will be protected from becoming moderately or seriously annoyed, respectively.

- 6.7.4. As set out above, although the site is a greenfield site comprised of open land with scrub vegetation, four electricity pylons are established across the site, the Wey Navigation and associated Towpath is situated adjacent to the western boundary, a small part of the M25 flyover is situated to the south west of the site and to the south east lies the South West main railway line. As such the site, and the wider area is already subject to significant levels of background noise from this transport infrastructure. That being said, this proposal would introduce new industrial uses close to an areas where there are a number of residential properties. Accordingly, the following assessment is regarding the potential impact on noise and disturbance due to the activities associated with the proposed development which could have an impact on the amenities of the occupiers of surrounding properties. However, the focus of this assessment is in terms of potential impact during late evenings, night-time, and early mornings and weekends where there would be an expectation of a greater level of amenity than that experienced during other times of the day.
- 6.7.5. A Noise Assessment has been submitted in support of this planning application and has identified the 10 closest noise source receptors (NSRS) as properties along Byfleet Road, Fairwater Drive and Common Lane. A “baseline” survey was undertaken at 2 locations representative of the receptors. The primary source of noise at the residential properties was identified as the road traffic from the M25 with secondary noise sources being local road traffic and low-level rail noise. The original Noise Assessment submitted with the application has been updated during the consideration of the application, following comments received from the Council’s Environmental Health Officer and a peer review carried out by an independent noise consultant at the request of the Council. Whilst it is acknowledged that the baseline survey data is from 2018, the baseline conditions and dominant noise sources in the area are not considered to have materially changed given the location of the site and the return of road traffic noise levels back to pre-pandemic levels and having viewed DfT data, the noise consultant and the Council’s EHO considers that this is reasonable.
- 6.7.6. The main anticipated sources of operational noise from the development are from heavy goods vehicles (HGV) movements including loading/unloading activity occurring during both the day (07:00-23:00 hours) and night-time period (23:00-07:00 hours). The Noise Assessment is based on a number of assumptions including that the data provided to the noise consultants shows the average 24-hour HGV profile for the development has been broken down into hourly flows which has resulted in a peak hour of HGV activity for both the daytime and night-time which is 13 HGVs during 10:00-11:00 and 17 HGVs during 06:00-07:00. Other hours contain lower levels of activity, therefore it is likely that these would result in lower levels of operational noise. Following the guidance provided in BS4142:2014+A1:2019, predictions of noise from HGV activities (reverse, load/unload and pull-away) were carried out for the peak hour of the day and for a 15-minute period during the peak hour of the night. For the daytime period although it is unlikely that there would be the arrival of 8 HGVs, loading/unloading of 13 HGVs and 8 HGVs departing in the same hour, however this has been assumed as the worse-case scenario for the modelling. For night-time period although it is unlikely, it has been assumed that within the peak 15 minutes 2 HGVs would arrive, 3 HGVs would be loaded or unloaded and 2 HGVs would depart, however this level of operation has been assumed to provide a robust assessment.
- 6.7.7. The average 24-hour car profile for the development has also been broken down into hourly flows which has resulted in a peak hour of car activity for both the daytime and

night-time which is 50 arrivals and 26 departures during 10:00-11:00 and 35 arrivals and 13 departures during 06:00-07:00. Other hours contain lower levels of activity, therefore it is likely that these would result in lower levels of operational noise. Predictions of noise from car parking activities (reverse, door open/close, pull-away and pass by) were carried out for the peak hour of the day and the peak hour of the night. Noise levels have been predicted from different locations and heights (daytime assessment have been predicted at ground floor level at 1.5 m and night-time assessments have been predicted at first floor level at 4.5 m) and by using noise modelling software.

- 6.7.8. The assessment shows that all but two of the noise sensitive properties have predicted noise levels that exceed the typical background sound level. The exceedance above the typical background levels varies between 2 decibels (dB) and 6dB for the residencies, with all exceedances being during the night-time period. As per BS4142:2014+A1:2019 'A difference of around +5 dB is likely to be an indication of an adverse impact, depending on the context'. The predicted level at 74 Byfleet Road of +5 dB above the typical background level would suggest an adverse impact during the night time. The potential impact of maximum sound levels that may occur during operational activity at night has also been considered at each receptor, the maximum source level might occur during the coupling of a HGV trailer, although it is unlikely that this activity would occur on a regular basis. From the information submitted the predicted maximum noise levels during the night-time at 5 noise sensitive residences exceed the required nighttime design criteria of 60 dB (albeit this value is currently exceeded by existing noise sources according to the noise survey).
- 6.7.9. To mitigate the predicted noise levels that exceed the typical background noise level, a 2.4m high acoustic barrier is proposed on either side of the entrance road which would reduce both daytime and night-time noise to levels that are equal to or below the typical background levels. The greatest insertion loss provided by the barrier is at 74 Byfleet Road during the daytime with a decrease in level of 8 dB. It is therefore considered unlikely that there will be adverse impact at any of the noise sensitive receptors. Acoustic heat maps showing the predicted noise from the sources to the receptors with and without the inclusion of the acoustic barrier clearly show the improvements with the proposed mitigation measures. Details of the barrier can be secured by way of condition.
- 6.7.10. The Assessment shows that with a maximum increase in road traffic noise of 0.4db it is considered that adverse impacts are highly unlikely. The predicted levels from car park activities at the noise sensitive receptors are also unlikely to cause any adverse impacts.
- 6.7.11. As there is no information regarding the proposed occupiers of the units to determine the level of potential level of noise break out from within the units, and each plot is likely to be occupied by a different business, undertaking their own activities it is difficult to determine the level of break-out noise with any certainty. For example, the occupant could be a trade counter (in which case there would be no breakout noise) or it could be an occupier that fabricates parts, which could potentially generate high levels of noise. Subject to a minimum specification covering the performance of the building envelope which would mitigate this concern for trade counter, or similar uses that typically generate low levels of noise, a condition would be imposed requiring a noise impact assessment before occupation of the units demonstrating a Rating Level equivalent to or below the representative background sound levels provided in table 4 of the Vanguardia report (November 22 revision 5) and where necessary, to identify what further mitigation would be required to achieve this.

6.7.12. During construction there is likely to be a short-term increase in noise levels from construction vehicles and plant especially to residential properties along Byfleet Road, Fairwater Drive and Common Lane which would be managed through the compliance with legislative requirements e.g. appropriate working hours and mitigation measures secured through a CEMP. The noise consultant and the Council's EHO have raised no objections subject to the recommended conditions which shall also include a condition to secure any alternative/additional mitigation if needed as a result of the scheme being built out in phases.

6.8. Potential impact in terms of overlooking, loss of privacy and/or overbearing impact

6.8.1. Policy EE1 sets out that "all development proposals will be expected to ensure no adverse impact ...to neighbouring property or uses". The Runnymede Design SPD states that "All dwellings must be designed with high quality internal and external space, in an appropriate layout, to accommodate different lifestyles and a range of private and communal activities. Accommodation must be designed to provide suitable levels of natural daylight and sunlight to new and existing properties ...". The document also provides further guidance of such matters including, sunlight and privacy.

6.8.2. Paragraph 135 of the National Planning Policy Framework also sets out that all proposals are expected to provide high standard of amenity for all existing and future users.

6.8.3. In regard to the amenities of the neighbouring properties, the proposed units range in height from approx. 8.8m to 10.5m. Units 2 – 6, being those closest to the nearest residential neighbours 98 -114 Byfleet Road, the ridge height is limited to 8.8m and is slightly sloping so as to minimise impacts on the nearest residential properties. Units 7-9 the ridge height is 10.5m as these units would be located further away from the nearest residential properties 116- 138 Byfleet Road and toward the rear of the site, where there is no sensitive receptors, the phase 2 units 10-13 would also be a similar height. However, it is noted that the existing ground level would also be raised approx. 0.3m-1.3m within the development footprint which is part of the flooding mitigation measures and although this would still be lower than land along the adjacent Towpath it would result in the overall total height of the units ranging from approx. 9.6m to 11.3m.

6.8.4. At its narrowest, units 2-3 are set back 7.4m from the property boundary. This increases to a minimum of 12.4m for units 7 and 8, with Unit 9 being setback 11.4m from the boundary. The rear gardens from Fairwater Drive would face towards the access road into the site with the closest premises being unit 2, however between this is an existing significant green buffer which would be retained and reinforced as well as the introduction of 2.4m high acoustic fence along the access road into the site. A separation distance of approx. 80 m to unit 2 would be maintained.

6.8.5. The units are arranged in two rows, with the car parks, turning areas and access track predominantly located between the two rows of units. This ensures that activity associated with the site will be contained within the development site, with the units themselves acting as an acoustic barrier, preventing potential impacts on the surrounding dwellings such as those associated with noise from vehicles moving within the property boundary.

6.8.6. The nearest residential dwellings along Byfleet Road are a considerable distance from the proposed units. At their closest, a separation of approximately 40.1m is provided, with this extending to approximately 51.3m at the furthest point. The western and eastern boundaries are lined with mature trees which offer effective visual screening of the site from its surroundings. The large tree belt to the north and western boundaries of the site would be retained. Vegetated slopes of the adjoining railway embankment to the south

of the site, and the associated garden hedges and trees of the residential area to the east, further increase the visual containment of the site. The rear of Units 2 to 9 will be planted with a native hedgerow with occasional trees to enhance the planting within the gardens. This approach will soften the built form when viewed from the gardens.

- 6.8.7. Given the proposed elevations of the units (varying from 9.6m to 11.3m), some adverse visual impacts are likely to be experienced by nearby receptors such as the residential properties which abut the eastern boundary off Byfleet road, particularly towards the south eastern corner of the site where unit elevations are the highest and the current screening ability of the vegetation is limited due to immaturity which is a negative of the scheme. However, the scale and massing of the proposed units has carefully considered their potential impact on the nearest residential dwellings, with heights being minimised where they have the potential for impact and setbacks to the property boundary maximised where possible, taking into account the constraints of the site. Further, units have been broken up with gaps provided between, introducing three breaks of 3-4m so there are now four groups of units; to allow views through and to reduce the apparent bulk of the building when viewed from the east.
- 6.8.8. It is important to note that the height of the units where they are closest to the nearest residential dwellings are broadly similar in height to a two storey dwelling, with difference in height being largely attributed to the change in ground level. Changes have been made to the scheme originally submitted which include a reduction in the maximum height of Units 7-9 backing onto Byfleet Road to 10.5m to ridge (a 500mm reduction) and a reduction of the eaves height to 8.6m (a 500mm reduction), an alteration in the shape of the roof to reduce the impact of the height and moving units 7-9 further away from the boundary by a further 5m (increased to a minimum of 11.4m).
- 6.8.9. On this basis, and given the separation distances and landscape screening proposed, as well as the heights, orientation and layout, it is not considered that the proposed development will result in significantly unreasonable impacts on the neighbouring properties in terms of overbearing, or in relation to daylight and sunlight. Further, given there are no windows to the relevant elevations, there will also be no impacts associated with loss of privacy or overlooking of those dwellings.

Potential impact in terms of lighting

- 6.8.10. There will be outdoor lighting for each unit. However, the orientation of the proposed units/ proposed lighting will be such that light emissions will be focused inwards at the site. Therefore any impact will be localised and will not significantly affect sensitive receptors in the surrounding area, such as the residential dwellings or ecological receptors. The proposed elevations of the commercial units will not exceed 11.3m, therefore, given absence of sufficiently tall structures, significant daylight/sunlight impacts are not likely. In addition, the buildings will be designed to maximise the use of natural daylight but minimise solar glare and solar gain. As such details of lighting matters could be satisfactorily managed through conditions.

Neighbouring Amenity Conclusions

- 6.8.11. In summary, the applicant has provided a number of supporting documents and assessments which provide evidence on the potential impact on the amenities of the occupiers of surrounding residential properties. This includes a noise assessment which demonstrates that subject to mitigation measures, notably the installation of acoustic fences along the access road into the site, the proposed development would not result in a significant increase of noise over existing background levels, particularly at night-time.

- 6.8.12. In terms of external lighting, this site is located on the edge of a suburban setting where there is already some level of background lighting. Any potential lighting overspill will largely be contained to the site and lighting overspill can be designed to minimise impact on residential properties and on wildlife through conditions.
- 6.8.13. The site is well screened and is not visually prominent in the landscape. The elevations of the proposed units would vary with the tallest units and parking being located away from residential properties. This ensures that much of the external activity during the operational phase of the development is contained within the middle of the site with the proposed units acting as a physical barrier between this and the wider area, reducing the impacts on the surrounding area. The large tree belt to the north and western boundaries of the site would be retained. There will be an adverse effect on townscape quality during construction works but these are considered to be short term and intermittent. Transient receptors (in vehicles or bicycles and on foot) on sections of Byfleet Road with views to the site and at occasional points off the footpath along the River Wey Navigation may also experience some adverse visual impacts. However, due to a combination of elevation, vegetation (both woodland blocks and tree belts), together with existing built form, substantial separation distances, views of the site and therefore visual impacts are not considered to extend beyond the immediate site boundary. The site is allocated for employment land adjacent to the M25 and a railway line and built development is already an established and frequent component of the visual character of the area.

6.9. **Flooding Considerations**

The need for the Sequential and Exception Test

- 6.9.1. The site is in flood zone 2 and partly in flood zone 3a. The NPPF (2023), as well as policy EE13 of the Local Plan sets out how to consider the principle of such development in the flood zone. As the proposal is for a conforming use on an allocated site the Sequential Test is not required.
- 6.9.2. The proposal would fall within the category of “less vulnerable” development. As set out in the National Planning Guidance which supports the NPPF (2023) the Exception Test is not required for this type of development located in flood zone 2 or 3a.

Flood protection and mitigation

- 6.9.3. Given the importance of the flood mitigation and compensation works in defining the development capacity of the site, any scheme of flood mitigation and compensation works agreed forms an integral part of the planning application. The site is partially located with Flood zone 2 and 3a and adjacent to the River Wey and Rive Ditch. In compliance with the requirements of the National Planning Policy Framework, the development should not increase flood risk to the wider catchment area as a result of suitable management of surface water runoff discharging from the site.
- 6.9.4. Employment uses proposed are considered to be less vulnerable development in terms of flood risk and therefore is considered appropriate on the site. The development footprint does fall within the 1 in 100-year plus 35% climate change fluvial floodplain, however this is to be mitigated through level for level floodplain compensation in the south of the site and along the eastern boundary. It is recommended to set finished floor levels to be a minimum of 300mm above the maximum fluvial flood depth of 16.02m AOD within the proposed footprint of the development during the 1 in 100-year plus 35% climate change event. It is also recommended that the finished floor levels of each plot are set 150mm above external ground levels to mitigate against the residual risk of pluvial, groundwater and sewer flooding within the site and ground levels should be profiled to encourage pluvial runoff and overland flows away from the built development

and towards the nearest drainage point. Therefore, the proposed units would have a finished floor level at 16.8m AOD in order to help manage the risk of flooding from surface water runoff and the local watercourse.

- 6.9.5. The original FRA and floodplain compensation assessment identified a total lost floodplain volume of 2681.6m³ and a compensation volume of 3493.8m³ which would have represented an 812.2m³ net gain in floodplain volume. However, following objections raised by the Environment Agency to the original proposals the FRA and floodplain compensation assessment was revisited to address the Environment Agency's request for a lesser depth cut off when delineating the floodplain extent which required the assessment of a larger area over a greater range of elevations, and a much more detailed assessment. As a result of this, the predicted amount of over compensation increased as a total lost floodplain volume of 1416.9m³ and a compensation volume of 5584.2m³ were identified. This represents an 4167m³ net gain in floodplain volume which is a betterment of the scheme. Therefore, more floodplain will be available following completion of the scheme.
- 6.9.6. The agreed floodplain compensation scheme is contained entirely within the site. The strategy will prevent the proposed development from making the existing flood risk issues in the wider area any worse. It does not set out to resolve the existing flood risk issues outside of the site. However, some betterment could be expected through the interception and attenuated storage of rain water falling on the development, as well as the additional floodplain storage offered in the compensation scheme.
- 6.9.7. The Environment Agency have reviewed the revised information and are happy with the proposed compensatory storage. Concerns regarding boundary treatment along the eastern boundary that could impede flood flows have also been removed. The eastern boundary is currently comprised of a mix of residential garden fences, and the development proposals do not propose to alter or replace them and the landscaped drawings show that a proposed native hedgerow planting is proposed on the eastern boundary will be permeable to floodwater. Acoustic barriers will be sited within the northern part of the site and whilst the EA has ask for all fencing in flood zone 3 to be permeable due to flood mitigation design, they would not represent a barrier to flood flows or loss of floodplain.
- 6.9.8. The existing pluvial floodplain in the development area will be addressed by the proposed surface water drainage strategy. Therefore, separate compensation for the pluvial floodplain is not required.

Sustainable Urban Drainage (SuDs)

- 6.9.9. In terms of Sustainable Urban Drainage (SuDs), Policy EE13 of the Local Plan requires all new development to ensure that sustainable drainage systems are used for the management of surface water unless demonstrated to be inappropriate. Paragraph 173 of NPPF states that all 'major' planning applications must incorporate sustainable drainage systems unless there is clear evidence that this would be inappropriate. SuDS must be properly designed to ensure that the maintenance and operation costs are proportionate and sustainable for the lifetime of the development.
- 6.9.10. A surface water drainage strategy has been submitted and to mitigate the development's impact on the current runoff regime, the surface water drainage strategy will incorporate attenuation and limit the discharge rate of surface water to the equivalent greenfield Mean Annual Flow Rate (QBAR). The drainage network will discharge to the Rive Ditch at the south site boundary, in accordance with the Hierarchy of Drainage. Subject to the details of the design of a surface water drainage scheme and providing effective attenuation on the site to be submitted and approved prior to commencement, Surrey County Council as Lead Local Flood Authority (LLFA) is satisfied this meets the

requirements set out in the technical Standard and Planning Policy Guidance. It is therefore considered that the site is capable of dealing with surface water drainage for the development in a sustainable manner which complies with the NPPF and therefore there are no issues of flooding arising from the proposal.

- 6.9.11. Thames Water has also requested further conditions in respect of Foul water. Overall, the proposed development is considered to demonstrate it would not cause new or exacerbate existing flooding problems, either on the proposed development site or elsewhere. The risk of flooding is also considered to be low and a suitable drainage strategy can be employed subject to conditions already set out above. The Environment Agency raises no objection, the Council's Drainage Officer also raises no objections and it is considered that the proposal is acceptable in flooding and drainage terms and complies with Policy EE13 and the NPPF.

6.10. **Ecology and biodiversity**

- 6.10.1. Policies SD7 of the Runnymede 2030 Local Plan sets out that development proposals will be supported where they protect existing biodiversity and include opportunities to achieve net gain in biodiversity. Policy EE9 of the Runnymede 2030 Local Plan also sets out that the Council will seek net gains in biodiversity, through the creation/expansion, restoration, enhancement, and management of habitats and species. Paragraph 180 of the National Planning Policy Framework states that planning decisions should protect and enhance sites of biodiversity and paragraph 186 sets out that opportunities to improve biodiversity in and around developments should be integrated as part of their design.
- 6.10.2. In terms of biodiversity proposals, planning policies of relevance to ecology and nature conservation at the time of submission of the application in February 2021 were summarised in Section 6 of the Ecological Assessment (December 2020). In relation to biodiversity net gain, it is important to note that there was no adopted or emerging planning policy relevant to the application which required specific consideration in relation to Biodiversity Net Gain through the prism of calculations using metrics, nor any requirement to use these tools to assist with the consideration of the planning application. Since this time, it is noted that the Environment Act became law in November 2021, with this legislation setting out the requirement for new planning applications to undertake specific assessment in relation to BNG utilising the Defra metric. However, this was adopted some nine months after submission of the planning application and the Government's planning practice guidance on Biodiversity Net Gain specifies that *"biodiversity net gain has only been commenced for planning permissions granted in respect to an application made on or after 12th February 2024. Permissions granted for applications made before this date are not subject to biodiversity net gain"*. It is clear that this legislation does not apply in relation to the current proposals which should be assessed against the NPPF to deliver general net gains.
- 6.10.3. It is therefore necessary for proposed development to first protect and avoid against any impact on ecology, where this is not possible they should then mitigate and then provide biodiversity net gains. The submission by the applicants includes an Ecological Assessment, an Outline Reptile Mitigation Strategy (ORMS) and an Arboricultural Impact Assessment.
- 6.10.4. Adjoining the site to the west is a non-statutory designated site, the Wey Navigation SNCI which is also a conservation area and covered by TPO422 along the northern and western boundaries (which include Woodlands W1 and W3) and its associated Biodiversity Opportunity Area (BOA) R04 (River Wey & tributaries). Whilst there are a number of habitats present on the site such as broadleaved woodland, semi improved grassland, dense scrub, scattered trees and scrub and tall vegetation with Bracken, and linear features including hedgerows and ditches which are able to support a number of

protected and notable species such as birds, bats, reptiles, great crested newt and other amphibians, rabbits, badger, hedgehog, otter and other mammals, stag beetle and other invertebrates the submitted surveys detected a bat roost in T53, a low population of Slow-worm, Common Lizard and Grass Snake. Invasive plant species have also been recorded.

- 6.10.5. The proposed development involves the clearance of existing areas of semi-natural woodland, grassland and scrub in the central and eastern part of the site. In the areas proposed for the commercial units and hard landscaping, the loss of habitat would be permanent in nature and will therefore reduce the amount of wildlife habitat on site. In turn, this would (in the absence of mitigation) result in fewer opportunities for faunal species through the loss of supporting habitats. Once operational, increased vehicle movements on site and noise and light impacts associated with the proposed development could also potentially adversely affect ecological receptors across the wider site in the absence of mitigation.
- 6.10.6. However, under the development proposals Woodlands 1 and 3 would be retained with the development footprint set back from the woodland edge. The landscape planting types and the species specified throughout have been chosen for their amenity value, opportunity for ecological enhancement and suitability to the local area. The work will involve felling, thinning, infill planting, pruning of dead and damaged wood, creation of habitat log piles and introduction of bird and bat boxes. The aim will be to achieve an age diverse structure to provide screening with habitat enhancement. Throughout the construction period the woodland / trees to be retained will be protected by temporary fencing as illustrated on Drawing 1919/18/02F Tree Retention, Removal and Protection Plan.
- 6.10.7. The alignment of the combined footpath / cycleway through the TPO woodland to the River Wey Navigation has been determined to best suit access to the river and wider area from the site. The route will not result in the loss of any significant trees. A 'no dig' construction will be adopted with either a permeable surface or combination of permeable surface and timber boardwalk / bridge.
- 6.10.8. The site is very constrained in regard to opportunity for further tree planting due to service easements, both underground and overhead and for maintenance access. However, the aim in developing the scheme has been to introduce complementary habitat types in the form of species rich grassland and native hedgerows with occasional new copses of trees. These frame the development with the core active area (roads, car parks, building entrances and service yards) planted with a selection of ornamental shrub species.
- 6.10.9. The access off Byfleet Road will be framed by the retained woodland with a 2m mown grass verge and adjoining species rich grassland. The rear of Units 2 to 9 will be planted with a native hedgerow with occasional trees to enhance the planting within the gardens. This approach will soften the built form when viewed from the gardens. A shade tolerant species rich grassland mix will be sown throughout this margin to enhance the ecological diversity.
- 6.10.10. The scrubby tree and bramble cover on the site side of the Rive Ditch (southern boundary) as well as the invasive species on the site will be removed to allow maintenance access to this EA easement and sown with species rich grassland. Copses of native trees and thicket will be introduced into this area where service and maintenance easements allow. The rear of Units 10 to 13 will be sown with species rich grassland up to the edge of the interface with the TPO woodland. Ultimately the landscape strategy aims to create a development that sits comfortably within its surroundings and is in keeping with the character of the area. Within the development the strong landscape structure as described and illustrated on the scheme drawings will

be enhanced to complement the existing adjoining mature woodland areas. This will create enhanced biodiversity and a positive relationship between the development and its surrounding environment.

- 6.10.11. The submitted Landscape Masterplan demonstrates how development can take place to provide space between buildings and the protected trees and other existing landscape features to ensure adequate amenity for the buildings occupiers and to prevent future pressure for the trees' removal which form part of the character of the area. The proposal has been designed so as to protect existing mature landscaping within the site and the proposed landscaping works are considered to be acceptable. On balance the development has the potential to have a neutral effect on the tree cover accordingly. The Councils Tree Officer does not object to the works subject to landscaping conditions.
- 6.10.12. Following the submission of the initial surveys an Outline Reptile Mitigation Strategy (ORMS) was submitted. The ORMS provides further detail and information regarding the assessment of likely impacts arising to reptiles and to set out mitigation to safeguard reptiles from harm which could potentially otherwise occur as a result of development. It is proposed that all reptiles caught at the site will be translocated to an on-site receptor site and additional land, which comprises approx. 3.5ha in total where there is scope to significantly enhance opportunities for reptiles such that it can support both existing populations that may be present and additional opportunities for the group. Following completion of the translocation exercise, all reptiles would be moved from the development footprint for the construction of the development, following which the reptile fencing will be removed allowing reptiles to recolonise additional habitats.
- 6.10.13. The submitted assessment confirms the presence of a bat roost associated with tree T53 in the central part of the application site which is to be removed. Surrey Wildlife Trust has raised no objections to the development subject to obtaining a protected species licence and conditions including the submission of a bat roost assessment prior to the felling or works to T53, a Landscape and Ecological Management Plan (LEMP), a Construction Environmental Management Plan (CEMP) and a Reptile Mitigation Strategy to ensure that losses to habitats will be mitigated, and that an enhancement to the biodiversity value of the site will be achieved. As such, it is considered that the development proposals would not lead to significant effects in biodiversity terms and it is considered that the proposed development can be carried out without any harmful impacts on protected species or habitats and the scheme complies with Policies EE9, E11 and the NPPF.
- 6.10.14. With the adoption of appropriate avoidance and mitigation measures as outlined above, it is considered that the proposed development would not be likely to lead to any adverse effects upon the Basingstoke Canal SSSI and the Wey Navigation SNCI. The Environment Agency have also requested a scheme for the provision and management of an 8 metre wide buffer zone alongside the watercourse to be submitted which can be secured as a condition. In view of this and for above reasons, it is also considered that the design of the proposals respect the boundaries of the site and can help to deliver against BOA objective R04/O3 (Targets T3b, T3c & T3e) as set out in Policy IE1.

6.11. **Renewable Energy**

- 6.11.1. New development is expected to demonstrate how it has incorporated sustainable principles into the development including construction techniques, renewable energy, green infrastructure and carbon reduction technologies.
- 6.11.2. Policy SD8 promotes renewable energy and seeks to incorporate sustainable design elements into new development including renewable energy and/or low carbon energy. Given the size of the development proposed on this site, the applicant is required to submit an energy statement outlining how they will meet the 10% energy requirement

and that they have explored feasibility of connecting to existing renewable/low carbon/decentralised energy networks in the local area. An Energy Sustainability Statement has been submitted with this application which applies the energy hierarchy set out in the policy and which sets out how the development's energy needs would be met from renewable and/or low carbon technologies by using air source heat pumps (ASHPs) and solar Photo Voltaic panels. The energy statement also considers whether connection to decentralised energy networks is possible. It is acknowledged in the Energy report that heat pumps can generate noise and as such, to protect the amenities of occupiers of nearby properties a condition is recommended. In conclusion, the proposed energy and sustainability measures are acceptable in order to achieve the requirements of Policies SD7 and SD8.

6.12. Other Considerations

Air Quality

- 6.12.1. Local Plan Policy EE2 requires proposals that may give rise to adverse impacts on air quality to be accompanied by an air quality assessment, and where the air quality assessment shows the development will have an adverse impact on air quality, planning permission will only be granted where mitigation measures to reduce impacts to acceptable levels can be secured. Paragraph 194 of the NPPF states that, 'the focus of planning policies and decisions should be on whether the proposed development is an acceptable use of land, rather than the control of processes or emissions (where these are subject to separate pollution control regimes). Planning decisions should assume that these regimes will operate effectively'.
- 6.12.2. In terms of air quality, in Addlestone Town Centre there is an area associated with a four-way traffic light-controlled junction (on the A318) which has been declared an AQMA. Whilst the Addlestone AQMA is located approx. 2km to the north of the application site, in terms of HGVs, it is likely that the vast majority of traffic would route to/ from the north via Byfleet Road. This is due to the 2.9m height restriction at the railway bridge to the south which limited the routes an HGV can take to this site, particularly from the strategic highway network. As such it is necessary to consider the potential impact of the proposals would have (cumulative or otherwise) on the AQMA, as well as on air quality more widely.
- 6.12.3. In making this assessment, it should also be noted that the general trend is an improvement in air quality. It is also noted that the site is allocated for employment generating uses in an up to date Local Plan. At the plan making stage consideration was given to air quality in terms of the vehicle movements associated with the proposed allocations. This would have included general assumptions regarding an employment use on the site and the likely traffic movements associated with them.
- 6.12.4. Furthermore Environmental Health Officers have advised that the Borough's Air Quality forecasts takes account of the improving vehicle fleet and growth in traffic that this then suggests that if the national forecast is applied in the two Runnymede AQMA's in terms of traffic growth and fleet turnover the annual mean NO₂ concentrations will be below 30 µg/m³, i.e. less than three quarters of the objective by 2024 and 2026 in the M25 and Addlestone AQMAs respectively.
- 6.12.5. As there are year to year variations in air quality largely due to the weather, but other factors such as road closures/diverted traffic can also impact on measured concentrations, Defra requires three years of monitoring data below the objective to revoke an AQMA. If the 2023 monitoring data for the M25 AQMA continues to be well below the objective the Council will consider revoking this AQMA in 2024. Similarly, if the NO₂ concentrations in the Addlestone AQMA remain below

the objective in 2023 and 2024, the Council will consider revoking this AQMA in 2025.

- 6.12.6. The Air Quality Assessment submitted in support of this planning application predicted a moderate adverse impact on annual mean NO₂ within the Addlestone Air Quality Management Area (AQMA) as a result of traffic generated by Phase 1 of the development. This was based on Phase 1 being completed and operational in 2020. In reality, Phase 1 is not likely to be operational until 2024 at the earliest, and further anticipated reductions in base concentrations have taken place in the intervening period. Furthermore, as set out in the Air Quality Assessment, Phase 2 of the development is not expected to be completed until 2025 at the earliest, when NO₂ concentrations within the AQMA are predicted to have declined sufficiently so that the operational impact of Phase 2 would be negligible. This phasing can be secured by way of condition.
- 6.12.7. In addition, and further to a third-party Peer Review on the air quality issues associated with this planning application a Low Emissions Strategy (LES) has been submitted in support of this planning application. This strategy sets out measures that will be employed at a site with the aim of reducing emissions from *all* possible sources rather than focusing on a single emission source such as vehicles.
- 6.12.8. The LES sets out measures that will be implemented at the site during the operational phase to reduce emissions principally through the appointment of a LES Coordinator to work alongside the Travel Plan coordinator on the site to oversee the rollout of measures and targets within the LES. These include:
- The appointment of an LES coordinator within the first three months of the site becoming operational.
 - Ensuring that the cycle parking and electric vehicle charging spaces proposed are installed prior to operations.
 - Undertake an annual review of the update of electric vehicle spaces and the potential for additional charging points.
 - Consider potential for electric vehicles within delivery fleet(s) of occupiers.
 - Annual review of the LES and the effectiveness of associated measures to include a survey of the use of alternative modes of transport by employees and staff.
- 6.12.9. The implementation of the LES strategy will be undertaken in conjunction with the site-specific Travel Plan which will seek measure for efficient deliveries to minimum HGV use where possible.
- 6.12.10. Turning to the construction phase, air quality can be dealt with through Construction Environmental Management Plans to secure measure to reduce the pollution associated with construction.
- 6.12.11. When considered as a whole and subject to the wider package of air quality mitigation measures which are secured though; the phasing of the development, Low Emissions Strategy and Travel Plan the scheme is considered to have an acceptable impact on air quality.

Contaminated Land

- 6.12.12. The site is greenfield land largely covered in vegetation and has not been used for agricultural purposes. Therefore, contamination associated with the use of fertilisers is not likely. The majority of the site has not been previously developed, therefore ground contamination is unlikely. There is an existing access road and, previously, a hardstanding compound area that has since been broken up. However, it is considered that any contamination would not be unusual or complex in nature and could effectively be dealt with through standard construction practices, controlled through the CEMP, so

that significant environmental effects would not arise. The Council's Environmental Protection Section considers that any source of contamination found could be satisfactorily managed during the application and through conditions. The Council's Contaminated Land Officer has raised no objections to the proposals subject to conditions.

Archaeology

- 6.12.13. As the site covers an area greater than 0.4ha Policy EE7 of the Local Plan requires the applicant to carry out an archaeological review of the site. The submitted report concludes that the site has a low archaeological potential and that no further archaeological work will be required. However, this conclusion does not take account of the lack of previous archaeological fieldwork carried out within the study area which means that the potential would be best described as uncertain rather than low. In order to clarify the archaeological potential of the site, it is considered that it would be appropriate to require further archaeological investigations. In the first instance this should comprise of an archaeological evaluation trial trenching exercise, which will aim to rapidly establish whether archaeological assets are present and enable suitable mitigation measures to be developed. These mitigation measures may involve more detailed excavation of any archaeological assets, but in the event of a find of exceptional significance then preservation in situ is the preferred option. Given that the assessment does not suggest that remains worthy of preservation in situ will be present, It is considered that it would be reasonable and proportionate to secure the evaluation and any further works by condition in compliance with the requirements of Policy EE7.

Other Matters

- 6.12.14. Regarding the letters received many concerns raised have been discussed above. Safety and security concerns have been considered. The building(s) will either be under the tenants' own management or that of the landlord's site management team. CCTV will be provided in the development with the CCTV cameras and cabling to be installed by occupiers. Car parking will be overlooked by the offices where possible and any dark areas will be lit (subject to a sensitive lighting). The unit(s) will be designed to ensure a sense of ownership by the occupier and security could be enhanced by enclosing potentially vulnerable areas with fencing (subject to the landscaping condition). The main activity in the development will be that of the business itself (i.e. industrial / warehousing) and will take place within the building and its service areas and along the access roads to the site. Activity creates general surveillance and is beneficial for security. A dedicated management team will be charged with maintenance, landscaping and security of the site.
- 6.12.15. Regarding National Grid's comments it is not a Statutory Consultee and the matter of Overhead Lines and underground electricity cabling falls outside the scope of this assessment. the above assessment has been made having regard for all surrounding units and residential properties, irrespective of if they are in the Borough of Runnymede, Woking or Elmbridge.

7. PLANNING OBLIGATIONS/COMMUNITY INFRASTRUCTURE LEVY (CIL)

- 7.1. In line with the Council's Charging Schedule the proposed development would be CIL liable. However, the rate for such a development in our adopted charging schedule is however £0.

- 7.2. As set out above the following planning obligations are considered necessary in order to make the development acceptable in planning terms:
- Travel Plan
 - £6150 Travel Plan auditing fee.
 - Low Emissions Strategy (LES) which shall include a monitoring fee.

9. EQUALITY AND HUMAN RIGHTS CONSIDERATIONS

- 10.1 Consideration has been given to Articles 1 and 8 of the First Protocol of the European Convention on Human Rights. It is not considered that the decision would result in a violation of any person's rights under the Convention.

Consideration has been given to s149 of the Equality Act 2010 (as amended), which has imposes a public sector equality duty that requires a public authority in the exercise of its functions to have due regard to the need to:

- (a) Eliminate discrimination, harassment, victimisation and any other conduct prohibited by the Act
- (b) Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
- (c) Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

It is considered that the decision would have regard to this duty.

10. CONCLUSIONS

- 11.1 This is a site allocated for development by the Runnymede 2030 Local Plan (by Policies SD2 and IE1). The quantum of development proposed makes an efficient use of an appropriate site and the traffic and highway safety aspects of the application have been reviewed by the County Highway Authority who raises no objections and conclude that the proposed access is safe, and no harmful impacts would arise in respect of the highway network in the area. The development is considered acceptable to have a limited detrimental impact on the Wey Navigation Conservation Area, but this is outweighed by the public benefits. The proposal has an acceptable impact on residential amenity, trees, ecology, contaminated land, archaeology, air quality, noise, flooding matters and other policy requirements. There are economic benefits which flow from this proposed development, including bringing the site into an employment generating use on land designated for employment generating use which weighs significantly in favour of the proposed development. No other technical planning issues have been identified that would prevent planning permission being granted in accordance with the development plan and the NPPF.
- 11.2 The application has been the subject of a number of objections and the planning issues raising concerns have been discussed above. The proposals would include the protection of key trees and new planting as part of a landscaping scheme which includes biodiversity mitigation and enhancement and safeguards protected species. There would be no significant adverse impact on the character of the area, the visual amenities of the street scene or the amenity of adjoining residents. There would be no harmful effects on archaeology and there are not considered to be any detrimental impacts on highway safety, noise or air quality and flooding.
- 11.3 The development has been assessed against the relevant policies in the Runnymede 2030 Local Plan, the policies of the NPPF, guidance in the PPG, and other material considerations including third party representations. It has been concluded that the development would not

result in any harm that would justify refusal in the public interest. The decision has been taken in compliance with the requirement of the NPPF to foster the delivery of sustainable development in a positive and proactive manner.

12. FORMAL OFFICER RECOMMENDATION

Recommendation Part A:

The HoP be authorised to grant planning permission subject to the completion of a Section 106 legal agreement under the Town and Country Planning Act 1990 (as amended) to secure the following obligations:

1. Travel Plan which shall include measures based on the Framework Travel Plan.
2. £6150 Travel Plan auditing fee.
3. Low Emissions Strategy (LES) which shall include a monitoring fee
4. Off-site Highway works to be completed prior to first being occupied or first opened for trading.
 - VAS (Vehicle Activated Signage) located on Byfleet Road north and south of the Byfleet and New Haw restricted height railway bridge
 - Pedestrian and cycle link from Byfleet Road to the Wey Towpath
 - Toucan crossing on Byfleet Road

And the following conditions:

1. Full application (standard time limit)

The development for which permission is hereby granted must be commenced not later than the expiration of three years beginning with the date of this permission.

Reason: To comply with Section 51 of Part 4 of the Planning and Compulsory Purchase Act 2004.

2. List of approved plans

The development hereby permitted shall not be carried out except in complete accordance with the following approved plans;

- Rive Ditch Floodplain Compensation LWB-BWB-ZZ-XX-DR-YE-0004, Rev P02
- TREE CONSTRAINTS PLAN Drawing No.01 Rev B
- Landscape Sections Drawing No.04 Rev H
- PROPOSED RIVER WEY NAVIGATION FOOTPATH LINK Drawing No.05 Rev D
- 31118-PL-200 SITE LOCATION PLAN
- 31118-PL-201E SITE LAYOUT PLAN
- 31118-PL-202A UNITS 2_3 FLOOR PLANS

- 31118-PL-203A UNITS 2_3 OFFICE FLOOR PLANS
- 31118-PL-204A UNITS 2_3 ELEVATIONS
- 31118-PL-205 UNITS 4_6 FLOOR PLANS
- 31118-PL-206 UNITS 4_6 OFFICE FLOOR PLANS
- 31118-PL-207 UNITS 4_6 ELEVATIONS
- 31118-PL-208 UNITS 7_8 FLOOR PLANS
- 31118-PL-209 UNITS 7_8 OFFICE FLOOR PLANS
- 31118-PL-210 UNITS 7_8 ELEVATIONS
- 31118-PL-211 UNIT 9 FLOOR PLANS
- 31118-PL-212 UNIT 9 ELEVATIONS
- 31118-PL-213 UNITS 10_13 FLOOR PLANS
- 31118-PL-214 UNITS 10_11 OFFICE FLOOR PLANS
- 31118-PL-215 UNITS 12_13 OFFICE FLOOR PLANS
- 31118-PL-216 UNITS 10_13 ELEVATIONS
- 31118-PL-217 ILLUSTRATIVE ELEVATIONS
- 31118-PL-218A ILLUSTRATIVE REAR ELEVATIONS UNITS
- 31118-PL-220A SITE CONSTRAINTS PLAN
- 31118-PL-221 BYFLEET ROAD STREETScape VIEW

Reason: To ensure high quality design and to comply with Policy EE1 of the Runnymede 2030 Local Plan and guidance in the NPPF.

3. Use of buildings

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order amending or re-enacting that Order with or without modifications) no more than 10% of the floorspace of the units hereby approved shall be used for Use Class B2 (General Industrial) or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order, without the prior approval in writing of the Local Planning Authority.

Reason: In the interest of the character of the area and residential amenities and to comply with Policy IE1 of the Runnymede 2030 Local Plan and guidance within the NPPF

4. Flood mitigation measures

The development shall be carried out in accordance with the submitted flood risk assessment (BWB Consulting dated May 2021, reference LWB-BWB-ZZ-XX-RP YE-0001_FRA) and supplementary letter from BWB dated 31 January 2022, reference IR/RG/BMS2399, and the following mitigation measures it details:

- Compensatory storage shall be provided on a level for level basis, as shown on plan named "Rive Ditch Floodplain Compensation" prepared by BWB Consulting, reference LWB-BWB-ZZ-XX-DR-YE-0004, Rev P02 contained within BWB letter dated 31 January 2022, reference IR/RG/BMS2399.

These mitigation measures shall be fully implemented prior to occupation and subsequently in accordance with the scheme's timing/phasing arrangements. The measures detailed above shall be retained and maintained thereafter throughout the lifetime of the development.

Reason: To prevent flooding elsewhere by ensuring that compensatory storage of flood water is provided. This is in accordance with National Planning Policy Framework (NPPF) Paragraph 167 which states that development should only be allowed in areas at risk of flooding where any residual risk can be safely managed.

5. M25 viaduct

No vehicles or plant shall be operated or parked/unloaded/loaded, or materials, liquids or fuels, or waste to be stored or stockpiled under the M25 viaduct at any time, other than those being used during authorised works on or to the M25.

Reason: To ensure that the M25 continues to be an effective part of the national system of routes for through traffic in accordance with section 10 of the Highways Act 1980 and to satisfy the reasonable requirements of road safety.

6. Phasing of development

Prior to the commencement of Phase 2 (units 10-13) of the development hereby approved an Air Quality Assessment shall be submitted to and approved in writing by the Local Planning Authority to demonstrate that Phase 2 shall have a “negligible impact” on the Addlestone Air Quality Management Area.

Reason: To prevent exposure to poor air quality that would otherwise have an adverse impact on health and quality of life of people working in the development and to protect neighbouring amenities and to comply with Policies EE1 and EE2 of the Runnymede 2030 Local Plan and guidance within the NPPF.

7. Construction transport management plan

No development shall commence until a Construction Transport Management Plan, to include details of:

- (a) parking for vehicles of site personnel, operatives and visitors
- (b) loading and unloading of plant and materials
- (c) storage of plant and materials
- (d) programme of works (including measures for traffic management)
- (e) provision of boundary hoarding behind any visibility zones
- (f) measures to prevent the deposit of materials on the highway
- (g) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
- (h) on-site turning for construction vehicles
- (i) details of waste management arrangements;
- (j) a plan showing the safeguarded area around and under the M25 Viaduct where no vehicles or plant shall be operated or parked/unloaded/loaded, or materials, liquids or fuels, or waste to be stored or stockpiled at any time;
- (k) contact details of personnel responsible for the construction works.

has been submitted to and approved in writing by the Local Planning Authority.

Only the approved details shall be implemented during the construction of the development.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policy SD4 of the Runnymede 2030 Local Plan.

8. Construction Environmental Management Plan (CEMP)

Prior to the commencement of development a Construction Environmental Management Plan (CEMP) shall be submitted to and approved in writing by the Local Planning Authority. The CEMP should include adequate details including;

- Risk assessment of potentially damaging construction activities
- Practical measures to avoid and reduce impacts during construction
- Location and timing of works to avoid harm to biodiversity features having regards to species specific requirements and the detail of above recommended LEMP report
- Responsible persons and line of communication
- Use of protected fences, exclusion barriers and warning signs

This should relate to the site as whole irrespective of implementation

Reason: In order to avoid adverse impacts on the adjacent Wey Navigation Site of Nature Conservation Importance, the Rive ditch, retained ecological features and protected species present within and adjacent to the development site and to enhance the appearance and biodiversity of the surrounding area and to comply with Policies EE1, EE9 and EE11 of the Runnymede 2030 Local Plan and guidance within the NPPF.

9. Reptile Mitigation Strategy

Prior to the commencement of development a Reptile Mitigation Strategy shall be submitted to and approved in writing by the Local Planning Authority. The strategy will need to be prepared by a suitably qualified ecologist and appropriate to the local context. The reptile mitigation strategy should include, but not be limited to following:

- a) Location and map of the proposed reptile mitigation
- b) Assessment of the habitats present, including their ecological function to reptiles
- c) Assessment of the baseline site reptile population size.
- d) Analysis of post-development and during construction habitat carrying capacity.
- e) Details of management measures that are required.
- f) Work schedule (including an annual work plan capable of being rolled forward over a ten year period)
- g) Details of the body or organisation responsible for implementation of the reptile mitigation strategy
- h) Ongoing monitoring and remedial measures
- i) Legal and funding mechanisms by which the long-term implementation of the reptile mitigation strategy will be secured by the applicant with the management body(ies) responsible for its delivery.
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed, and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.

Reason: In order to avoid adverse impacts on protected species present within and adjacent to the development site and to comply with Policies EE1, EE9 and EE11 of the Runnymede 2030 Local Plan and guidance within the NPPF.

10. Landscape and Ecological Management Plan (LEMP)

Prior to the commencement of development a Landscape and Ecological Management Plan (LEMP) shall be submitted to and approved in writing by the Local Planning Authority. The LEMP should have regard to both habitat and species impact avoidance and mitigation measures, and should include adequate details of the following;

- a) Description and evaluation of features to be managed and created including measures to compensate for loss of [specific habitat of concern]

- b) Numbers and locations of bat and bird boxes, including provision integral to the design of the new buildings.
- c) Aims and objectives of management
- d) Appropriate management options for achieving aims and objectives
- e) Prescriptions for management actions
- f) Preparation of a work schedule for securing biodiversity enhancements in perpetuity
- g) Details of the body or organisation responsible for implementation of the LEMP
- h) Ongoing monitoring and remedial measures
- i) Legal and funding mechanisms

Demonstration of no net loss of biodiversity and proposals for biodiversity net gain should be clearly recorded and reported through use of an appropriate metric such as the DEFRA Biodiversity Metric 4.0. Natural England advise that any net gain should be fully secured and funded for the lifetime of the development. An appropriate metric should be applied and the results used to inform the above referenced LEMP, with results clearly documented and submitted for approval in writing by the Council. This shall demonstrate how this will be achieved on a phased basis.

Reason: To enhance the appearance and biodiversity of the surrounding area and to comply with Policies EE1, EE9 and EE11 of the Runnymede 2030 Local Plan and guidance within the NPPF.

11. Buffer zone

Prior to commence of any development a scheme for the provision and management of an 8 metre wide buffer zone alongside the watercourse shall be submitted to, and approved in writing by, the Local Planning Authority.

Thereafter, the development shall be carried out in accordance with the approved scheme. Any subsequent variations shall be agreed in writing by the local planning authority, in which case the development shall be carried out in accordance with the amended scheme. The buffer zone scheme shall be free from built development including lighting, domestic gardens and formal landscaping.

The scheme shall include:

- plans showing the extent and layout of the buffer zone.
- details of any proposed planting scheme. This should include selected tree thinning and a native planting scheme that aims to create a mosaic of different habitats. No cultivars or varieties should be used within the buffer zone and we request that these are removed and replaced with locally native species of UK genetic provenance.
- details of how the non-native invasive Japanese knotweed and Himalayan balsam will be eradicated from the site.
- details demonstrating how the buffer zone will be protected during development and managed over the longer term including adequate financial provision and named body responsible for management plus production of detailed management plan.
- Details demonstrating how the buffer zone will be protected during development, for instance by temporary fencing.
- Details of any proposed fencing or lighting. The buffer zone should be free from lighting that could impact on the behaviour of nocturnal animals such as bats. To reduce light spill into the river corridor outside the buffer zone, all artificial lighting

should be directional and focused with cowlings. For more information see the Institution of Lighting Professionals guidance:
<https://www.theilp.org.uk/documents/obtrusive-light/>
<https://www.theilp.org.uk/documents/guidance-note-8-bats-and-artificial-lighting/>

Reason: Land alongside watercourses is particularly valuable for wildlife and it is essential this is protected. This approach is supported by paragraphs 174 and 180 of the National Planning Policy Framework (NPPF) which recognise that the planning system should conserve and enhance the environment by minimising impacts on and providing net gains for biodiversity. If significant harm resulting from a development cannot be avoided, adequately mitigated, or as a last resort compensated for, planning permission should be refused. This condition is also supported by legislation set out in the Natural Environment and Rural Communities Act 2006 and Article 10 of the Habitats Directive which stresses the importance of natural networks of linked corridors to allow movement of species between suitable habitats and promote the expansion of biodiversity. It is also supported by Runnymede's Local Plan Policy EE12.

12. Tree Protection

Prior to the commencement of any works hereby approved, including demolition, and before any equipment, machinery or materials are brought on to the site tree protective measures shall be installed in accordance with the approved submitted plan 1919/18/02F and Arboricultural Impact Assessment by BB Trees Ltd Document reference: 501-20 Revision 0 . The works shall be carried out in accordance with the approved protection plan and method statement. Once in place, photographic evidence of the protective measures shall be submitted to the Local Planning Authority (LPA) for approval. The protective measures shall remain in place until all works are complete and all machinery and materials have finally left site. Nothing shall be stored or placed in any area fenced in accordance with this condition, nor shall any fires be started, no tipping, refuelling, disposal of solvents or cement mixing carried out and ground levels within those areas shall not be altered, nor shall any excavation or vehicular access, other than that detailed within the approved plans, be made without the written consent of the LPA.

There shall be no burning within six metres of the canopy of any retained tree(s). Where the approved protective measures and methods are not employed or are inadequately employed or any other requirements of this condition are not adhered to, remediation measures, to a specification agreed in writing by the LPA, shall take place prior to first occupation of the development, unless the LPA gives written consent to any variation.

Reason: To protect the trees to be retained and enhance the appearance of the surrounding area, to ensure that replacement trees, shrubs and plants are provided and to protect the appearance of the surrounding area and to comply with Policies EE1, EE9, and EE11 of the Runnymede 2030 Local Plan and guidance in the NPPF.

13. SuDS

The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

a) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+20% allowance for climate change) storm events, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. Associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 6.8 l/s. Evidence should be provided which confirms where possible areas of the site have been drained via gravity.

b) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.).

c) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.

d) Details of drainage management responsibilities and maintenance regimes for the drainage system.

Reason: To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site.

14. Archaeological Work

No development shall take place until the applicant has secured the implementation of a programme of archaeological work in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the Planning Authority.

Reason: To allow archaeological information to be recorded and to comply with Policy EE7 of the Runnymede 2030 Local Plan and guidance within the NPPF.

15. Bat Roost Assessment

Prior to the felling or works to T53 a bat roost assessment shall take place by a qualified ecologist with the results clearly documented and submitted in writing to the Council.

Reason: To protect the habitat of bats and the ecological value on the site and to comply with Policy EE9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

16. Hard and Soft Landscaping

(a) Full details of both hard and soft landscaping works shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the above ground construction of the development hereby permitted and these works shall be carried out as approved prior to the first occupation of the development. This scheme shall include indications of all changes to levels, hard surfaces, walls, fences, access features, minor structures, the existing trees and hedges to be retained, together with detailed measures to be taken to ensure that retained trees and their roots are not damaged as well as a programme of works for the land containing the retained trees along the boundary with Wick Road extending from Blays Lane to the boundary with Brook Lodge. This programme should be designed to enhance the existing tree population with native canopy species, ensure a long-term gain of tree canopy cover, enhance the wildlife/biodiversity potential and replace non-native shrubs and understorey with native understorey species. The details to be submitted shall include all vegetation to be removed and the number, size, species and locations of both the individual canopy trees to be planted and understorey planting.

(b) All hard and soft landscaping works shall be carried out in accordance with the approved details. Arboricultural work to existing trees shall be carried out prior to the commencement of any other development, otherwise all remaining landscaping work and new planting shall be carried out prior to the occupation of any part of the development or in accordance with the timetable agreed with the Local Planning Authority.

No tree to be retained in accordance with the approved plans (hereafter known as retained trees and including offsite trees) shall be cut down, uprooted or destroyed and no works to the above or below ground parts of the trees in excess of that which is hereby approved shall be carried out without the written approval of the Local Planning Authority until the expiration of five years from the date of completion of the development. If, within this time, a retained tree is pruned not in accordance with BS3998, removed, uprooted, damaged in any way, destroyed or dies, replacement trees shall be planted at the same place, sufficient to replace the lost value of the tree as calculated using an amenity tree valuation system, unless otherwise agreed in writing by the Local Planning Authority. The number, size, species, location and timing of the replacement planting shall be as specified by the Local Planning Authority.

Any trees or plants (including retained trees) which within a period of five years of the commencement of any works in pursuance of the development is pruned not in accordance with BS3998, die, are removed, or become seriously damaged or defective, shall be replaced as soon as practicable with others of similar size and species, following consultation with the Local Planning Authority, unless the Local Planning Authority gives written consent to any variation.

Reason: To mitigate the impact of a new parking area being created within land identified as a biodiversity opportunity area, to protect the trees to be retained and enhance the appearance of the surrounding area, to ensure that replacement trees, shrubs and plants are provided and to protect the appearance of the surrounding area and to preserve and enhance the character and appearance of the surrounding area and to comply with Policies EE1, EE9, and EE11 of the Runnymede 2030 Local Plan and guidance in the NPPF

17. External Materials

No development above slab level shall commence (on a phased basis or otherwise) until details of the materials to be used on the external surfaces of the works hereby approved, and no variations in such materials when approved shall be made without the prior approval, in writing, of the Planning Authority. Development shall be carried out in accordance with the approved details.

Reason: In the interests of the visual amenities of the area and the character an appearance of the area and to comply with Policy EE1 of the Runnymede 2030 Local Plan and guidance in the NPPF.

18. Finishing Materials

No development above slab level (on a phased basis or otherwise) shall commence until a specification of all the finishing materials to be used in any hard surfacing on the application site have been submitted to and approved in writing by the Local Planning Authority and thereafter undertaken in accordance with the approved scheme.

Reason: In the interests of the visual amenities of the area and the character and appearance of the area and to comply with Policy EE1 of the Runnymede 2030 Local Plan and guidance in the NPPF.

19. Modified access

No part of the development shall be first occupied (on a phased basis or otherwise) and/or open to trade unless and until the proposed modified vehicular access to Byfleet Road has been constructed and provided with visibility zones in accordance with the approved plans and thereafter the visibility zones shall be kept permanently clear of any obstruction over 1000 mm high.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policy SD4 of the Runnymede 2030 Local Plan and guidance within the NPPF.

20. Parking and turning

The development hereby approved (on a phased basis or otherwise) shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles and cycles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. all cycle parking shall be secure, covered and lit. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policy SD4 of the Runnymede 2030 Local Plan and guidance within the NPPF.

21. EVC Charging points

Prior to the occupation of the development, a scheme setting out the delivery of the proposed electric vehicle charging points (EVCPs), including details of when they will be delivered and how they will be managed, shall be submitted to and approved in writing by the Local Planning Authority. The approved EVCPs, consisting of an overall of 10% active and 90% passive charging points, (delivered on a phased basis) with 10% active prior to first occupation shall be installed and shall be maintained in accordance with the approved details thereafter.

Reason: in the interest of sustainable development and to satisfy the Runnymede Local Plan (2030) policies Policy SD3: Active & Sustainable Travel, Policy SD4: Highway Design Considerations, Policy SD5: Infrastructure Provision & Timing, Policy SD7: Sustainable Design.

22. SuDS Verification

Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the Drainage System is designed to the National Non-Statutory Technical Standards for SuDS.

23. Foul Water drainage

No development (on a phased basis or otherwise) shall be occupied until confirmation has been provided that either:

- Capacity exists off site to serve the development, or
- A development and infrastructure phasing plan has been agreed with the Local Authority in consultation with Thames Water. Where a development and infrastructure phasing plan is agreed, no occupation shall take place other than in accordance with the agreed development and infrastructure phasing plan, or
- All wastewater network upgrades required to accommodate the additional flows from the development have been completed.

Reason: Network reinforcement works may be required to accommodate the proposed development. Any reinforcement works identified will be necessary in order to avoid sewage flooding and/or potential pollution incidents. The developer can request information to support the discharge of this condition by visiting the Thames Water website at thameswater.co.uk/preplanning.

24. Boundary Treat Including Acoustic Barrier

Before the development hereby approved is first occupied all boundary treatments, including a proposed 2.4 metre high acoustic barrier (of at least 10kg/m² with no gaps or holes in the barrier – particularly at the interface between the barrier posts and the panels) as identified in the mitigation section of the Vanguardia Noise Assessment report (November 22 revision 5) shall be installed in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority. Should the development be bought forward in phases an updated noise report and mitigation strategy shall be submitted to and approved in writing by the Local Planning Authority and the necessary mitigation installed prior to the occupation of the buildings hereby permitted. The boundary treatments shall thereafter be maintained in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of the visual amenities of the area and the character and appearance of the area and to minimise noise impacts and to comply with Policy EE1 and EE2 of the Runnymede 2030 Local Plan and guidance within the NPPF.

25. Sensitive Lighting Scheme

Before any external lighting is installed at the site, details (which shall include a lighting plan including lux plots illustrating light levels at W1 and the canal surface at a variety of elevations as required in the ILP guidance note 8) shall be submitted to and approved in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details and be retained as such thereafter.

Reason: To protect the amenities of occupiers of nearby properties and to protect wildlife and to comply with Policies EE2 and EE9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

26. Heating and cooling technologies

Prior to works above slab level of the development hereby permitted details of modern heating and cooling technologies to be utilised within the development shall be submitted and approved by the Local Planning Authority. Development shall be carried out in accordance with the approved details and be retained as such thereafter.

Reason: In order to achieve water efficiency and sustainable development and to comply with Policy SD7 of the Runnymede 2030 Local Plan and guidance within the NPPF.

27. Renewable energy (details required)

Prior to the first occupation of the development (on a phased basis or otherwise) hereby approved, details of the chosen renewable energy/low carbon technology to be used, along with calculations demonstrating that 10% of the predicted energy consumption would be met through renewable energy/low carbon technologies shall be submitted to and approved in writing by the Local Planning Authority (LPA)..

Development shall be carried out in accordance with the approved details and thereafter retained, maintained and operational unless otherwise agreed in writing by the LPA.

In the event of air or ground source heat pumps being the chosen renewable energy measure, details shall be submitted to and approved in writing by the LPA prior to installation. Details shall include acoustic data to demonstrate that there will be no increase in the background noise level and that there will be no tonal noise emitted from the unit, as well as details of the location of the unit(s) and the distance to the closest dwelling.

Reason: To ensure that a minimum of 10% of the energy requirement of the development is produced by on-site renewable energy sources/low carbon technology and to protect the amenities of occupiers of nearby properties and to comply with Policies SD8 and EE1 of the Runnymede 2030 Local Plan and guidance within the NPPF.

28. Waste storage

Prior to the first use of the development hereby approved, details of the proposed arrangements for the storage, recycling and disposal of refuse from the premises shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and be retained as such thereafter.

Reason: To ensure high quality design, and to protect the environment of the area from nuisance by reason of smell, insects or rodent pests and to comply with Policies EE1 and EE2 of the Runnymede 2030 Local Plan and guidance within the NPPF.

29. Land Affected by Potential Contamination

Unless otherwise agreed by the Local Planning Authority, development other than that required to be carried out as part of an approved scheme of remediation must not commence until Conditions (i) to (iv) or otherwise agreed remedial measures have been complied with. If unexpected contamination is found after development has begun, development must be halted on that part of the site affected by the unexpected contamination to the extent specified by the local planning authority in writing until Condition (iv) has been complied with in relation to that contamination.

(i) Site Characterisation

No development must take place until an assessment of the nature and extent of contamination on the site has been submitted to and approved in writing by the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and shall assess any contamination on the site whether or not it originates on the site. The report of the findings must include:

(a) a survey of the extent, scale and nature of contamination;

(b) an assessment of the potential risks to:

- human health
- property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes
- adjoining land
- ground waters and surface waters
- ecological systems
- archaeological sites and ancient monuments

(ii) Submission of Remediation Scheme

If found to be required no development shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health,

buildings and other property and the natural and historical environment has been submitted to and approved in writing by the local planning authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, an appraisal and remedial options, proposal of the preferred option(s), a timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

(iii) Implementation of Approved Remediation Scheme

If found to be required, the remediation scheme shall be implemented in accordance with the approved timetable of works. Upon completion of measures identified in the approved remediation scheme, a verification report (validation report) that demonstrates the effectiveness of the remediation carried out must be submitted to the local planning authority.

(iv) Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified, it must be reported in writing immediately to the local planning authority and once the Local Planning Authority has identified the part of the site affected by the unexpected contamination, development must be halted on that part of the site. An assessment must be undertaken in accordance with the requirements of Condition (i) or otherwise agreed and where remediation is necessary, a remediation scheme, together with a timetable for its implementation must be submitted to and approved in writing by the Local Planning Authority in accordance with the requirements of Condition (ii) in the form of a Remediation Strategy which follows the .gov.uk LCRM approach. The measures in the approved remediation scheme must then be implemented in accordance with the approved timetable. Following completion of measures identified in the approved remediation scheme, a validation (verification) plan and report must be submitted to and approved in writing by the Local Planning Authority in accordance with Condition (iii).

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off-site receptors in accordance with guidance in the NPPF.

30. Noise Impact Assessment

A noise impact assessment for each unit shall be carried out and submitted to the Local Planning Authority within 3 months of occupation of the relevant unit demonstrating a Rating Level equivalent to or below the representative background sound levels provided in Table 4 of the Vanguardia Noise Assessment report (November 22 revision 5) and where necessary, to identify what further mitigation would be required to achieve this.

Reason: In the interests of the visual amenities of the area and the character and appearance of the area and to minimise noise impacts and to comply with Policy EE1 and EE2 of the Runnymede 2030 Local Plan and guidance within the NPPF.

Informatives:

1 Summary of Reasons to Grant Consent

The decision has been taken in compliance with the requirement in the NPPF to foster the delivery of sustainable development in a positive and proactive manner.

- 2 If proposed site works affect an Ordinary Watercourse, Surrey County Council as the Lead Local Flood Authority should be contacted to obtain prior written Consent. More details are available on our website.
- 3 If proposed works result in infiltration of surface water to ground within a Source Protection Zone the Environment Agency will require proof of surface water treatment to achieve water quality standards.
- 4 The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover or to install dropped kerbs Please see www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle_crossovers-or-dropped-kerbs.
- 5 The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme> . The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice
- 6 **Mud/debris on the highway**
The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
- 7 **Damage to the highway**
Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
- 8 It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to:
<http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
- 9 **Highway impact of contractors vehicles**
The applicant is expected to ensure the safe operation of all construction traffic to prevent unnecessary disturbance obstruction and inconvenience to other highway users. Care should be taken to ensure that the waiting, parking, loading and unloading of construction vehicles does not hinder the free flow of any carriageway, footway, bridleway, footpath, cycle route, right of way or private driveway or entrance. The developer is also expected to require their contractors to sign up to the "Considerate Constructors Scheme" Code of Practice, (www.ccscheme.org.uk) and to follow this throughout the period of construction within the

site, and within adjacent areas such as on the adjoining public highway and other areas of public realm.

- 10 Statutory utility works
The developer would be expected to agree a programme of implementation of all necessary statutory utility works associated with the development, including liaison between Surrey County Council Streetworks Team, the relevant Utility Companies and the Developer to ensure that where possible the works take the route of least disruption and occurs at least disruptive times to highway users.
- 11 A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.
- 12 The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures. <https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>. Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB
- 13 The applicant is advised to open a dialogue with UK Power Networks to discuss working arrangements under the HSE Guidance note GS6 "Avoidance of danger from overhead electric power lines" and the Energy Networks Association Technical Specification (Table 11.1 and sections 43-8). Furthermore the developers should be aware of the requirements under the HSE guidance on Avoiding Danger from Underground Services <https://www.hse.gov.uk/electricity/information/excavations.htm> pre/during/post construction.
- 14 The applicant is advised to contact UK Power Networks General Enquiry team on 0800 029 4285 to obtain further information on mitigation and guidance when working around the electrical apparatus and prior to any building works in order for all parties to manage and co-ordinate the works safely and effectively during the construction phase. This would be the most sensible course of action for all parties and ensure compliance with the relevant HSE legislation and manage any risks during the construction phase accordingly.
- 15 Many trees contain wildlife such as bats and nesting birds that are protected by law. The approval given by this notice does not override the protection afforded to these species and their habitats. You must take any necessary steps to ensure that the work you are carrying out will not harm or disturb any protected species or their habitat. If it may do so you must also obtain permission from Natural England prior to carrying out the work. For more information on protected species please go to www.naturalengland.gov.uk.
- 16 The applicant is advised that biodiversity net gain is provided across the whole site. This is supported by Runnymede's Local Plan Policy EE9 which states 'The Council will seek net gains in biodiversity, through creation/expansion, restoration, enhancement and management of habitats and features to improve the status of priority habitats and species.'

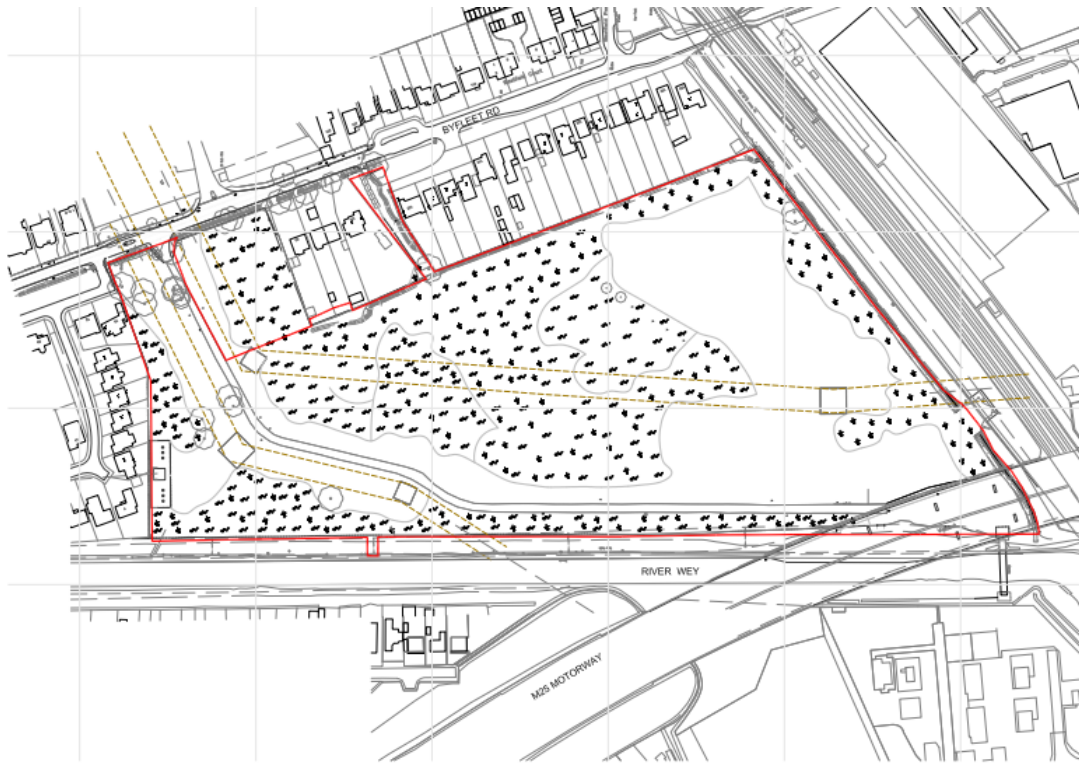
- 17 Network Rail requests the applicant / developer engages with Network Rail's Asset Protection and Optimisation (ASPRO) team via AssetProtectionWessex@networkrail.co.uk prior to works commencing. Our Asset Protection will review the details of the proposal to ensure that it can be successfully implemented without any risk to the operational railway. The applicant /developer may be required to enter into an Asset Protection Agreement to get the required resource and expertise on-board to enable approval of detailed works. More information can also be obtained from our website <https://www.networkrail.co.uk/running-the-railway/looking-afterthe-railway/asset-protection-and-optimisation/>.
- 18 The applicant is advised to liaise with the Basingstoke Canal Authority to agree the measures needed to ensure the towpath conforms to the 2012, Department for Transport, Local transport Note 1/12, Shared Use Routes for Pedestrians and Cyclists.
- 19 With reference to the condition on EVC charging points Active Electric Vehicle Charging point shall have a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply).

Recommendation Part B:

The HoP be authorised to refuse planning permission should the S106 not progress to his satisfaction or if any significant material considerations arise prior to the issuing of the decision notice that in the opinion of the HoP would warrant refusal of the application. Reasons for refusal relating to any such matter are delegated to the HoP.

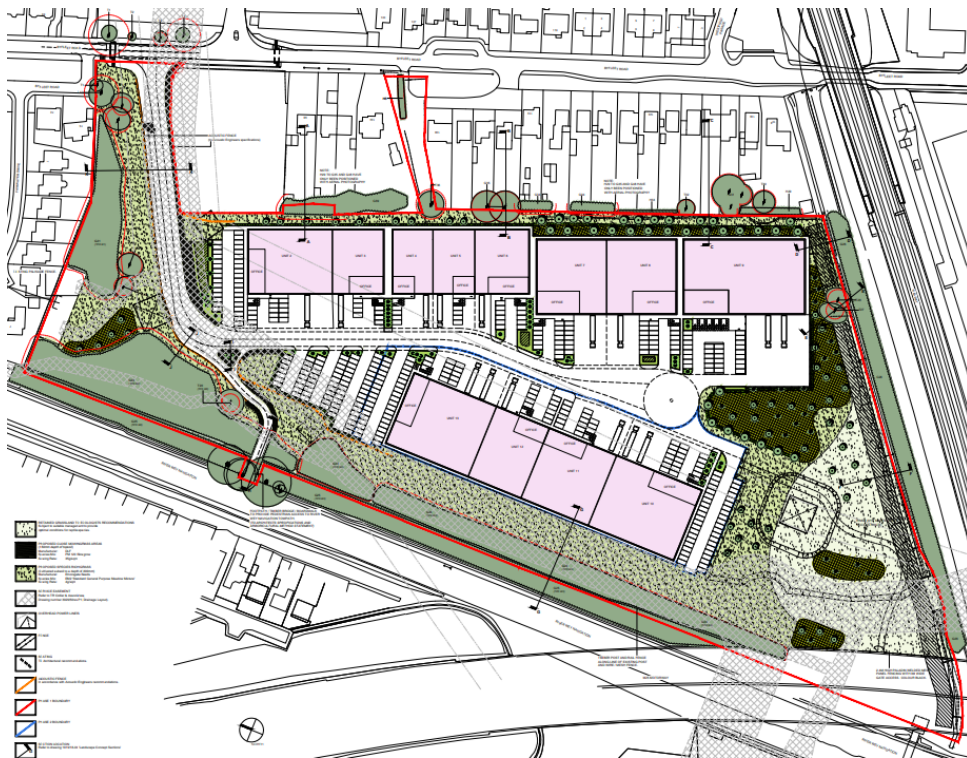
Appendices – Land West of Byfleet Road – RU.21/0207

Location Plan

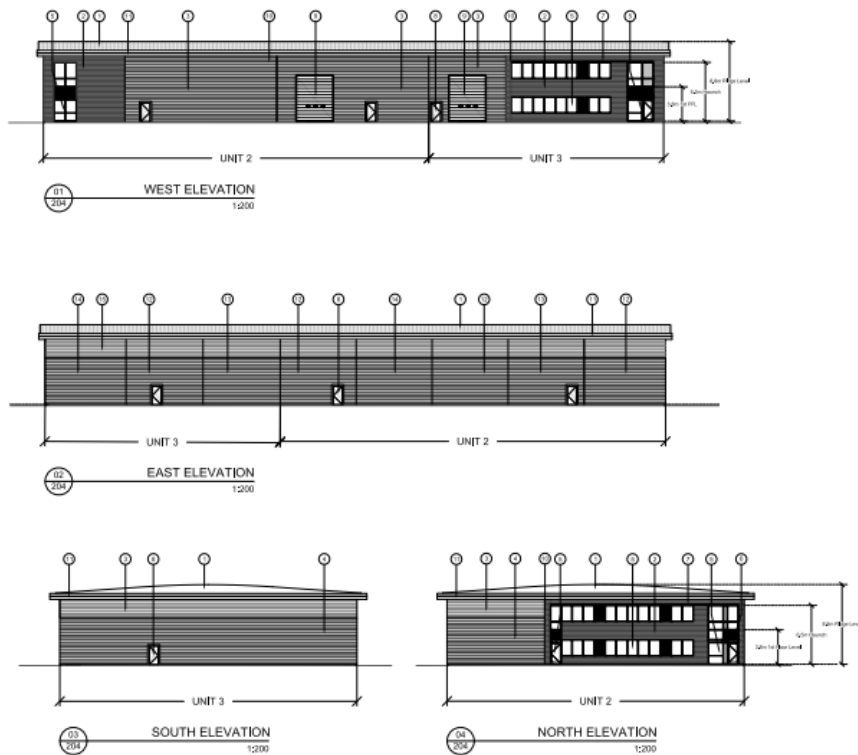


Proposed Site Plan

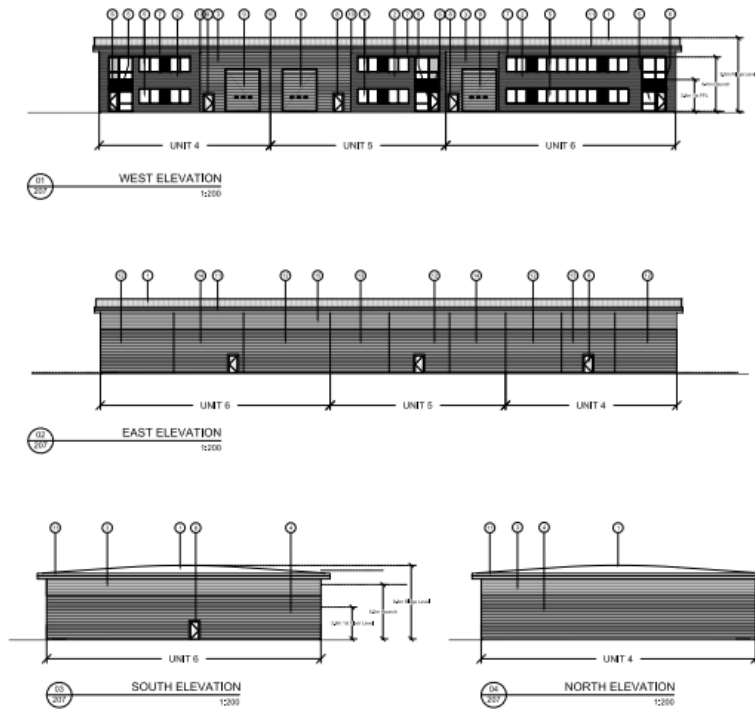




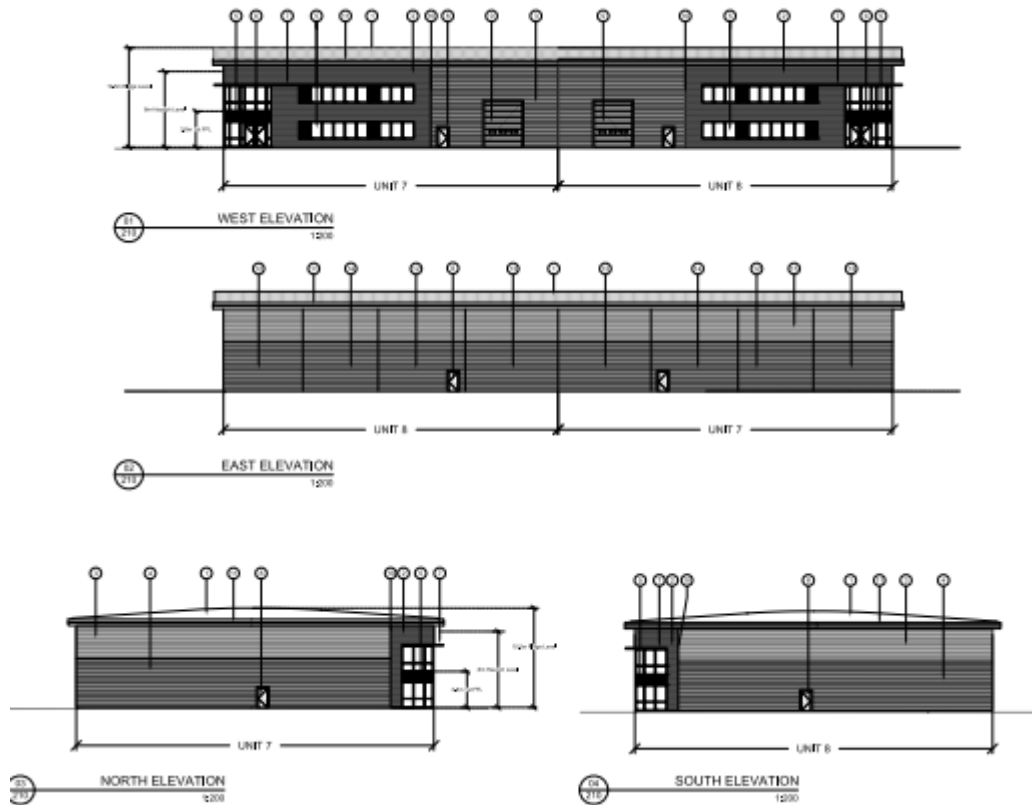
Landscape Concept Plan



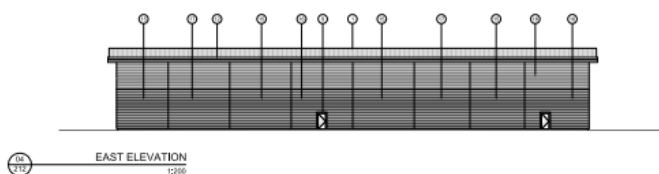
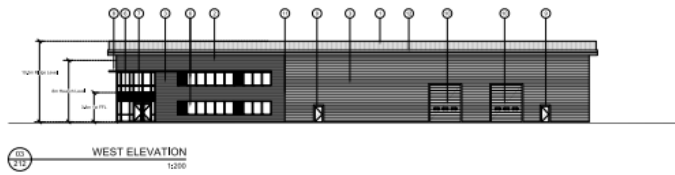
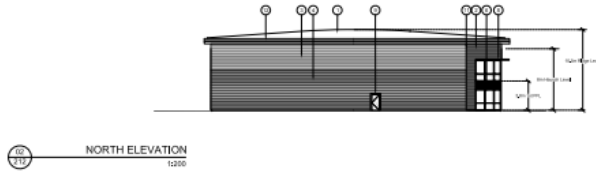
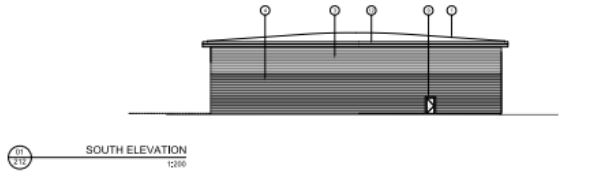
Units 2 & 3 Elevations



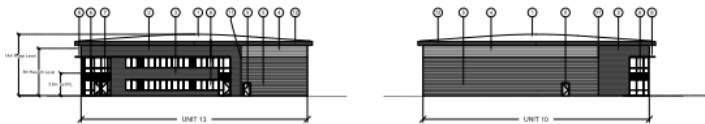
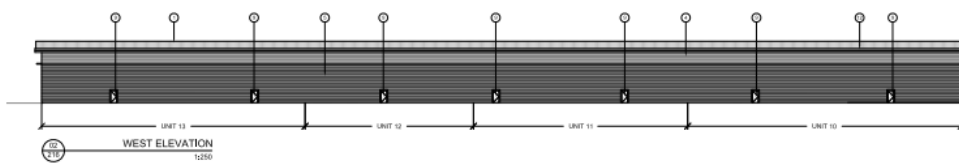
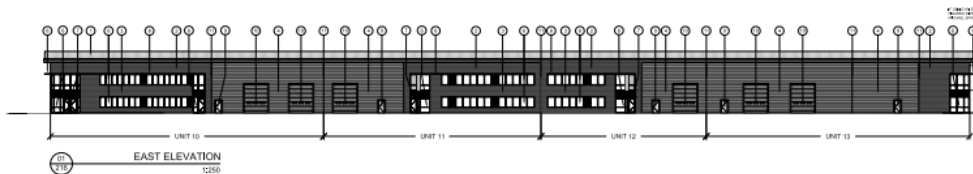
Units 4-6 Elevations



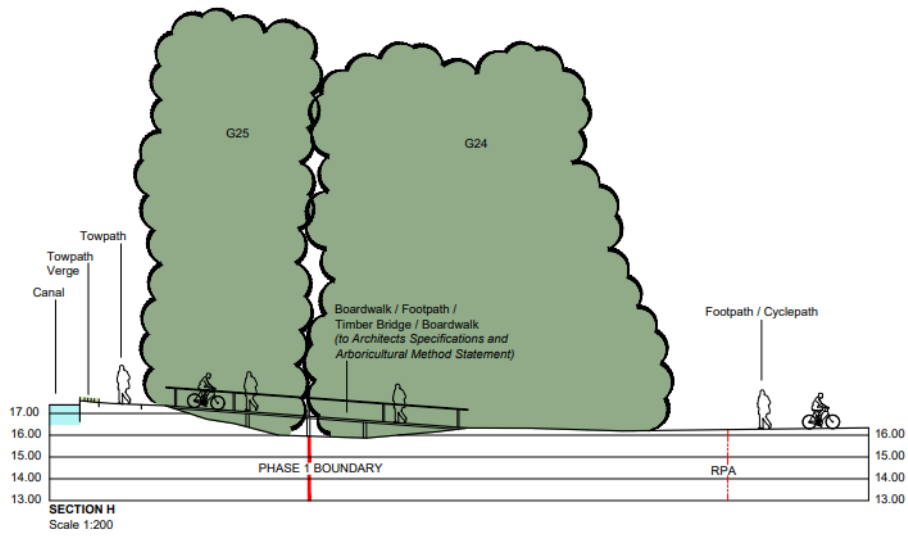
Units 7-8 Elevations



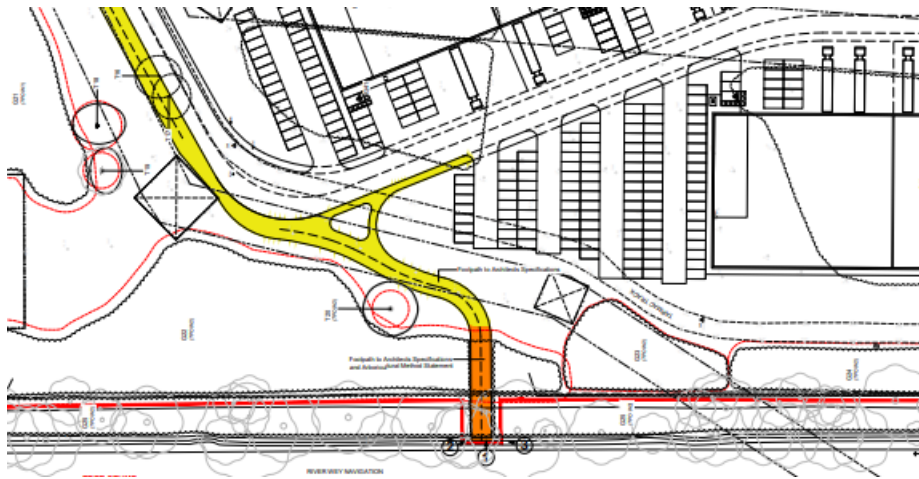
Unit 9 Elevations



Units 10-13 Elevations



Proposed footpath link





FOR LOCATION PURPOSES ONLY

Runnymede Borough Council
Runnymede Civic Centre
Station Road
Addlestone
Surrey KT15 2AH

Date:11/04/2024

Longcross South



© Crown copyright and database rights 2024 Ordnance Survey 100006086

Scale: 1:10,000



RU.22/0393



COMMITTEE AGENDA REFERENCE: 5B

APPLICATION REF:	RU.22/0393
LOCATION	Longcross South, Longcross Road and Kitsmead Lane, Longcross, KT16 0EE
PROPOSAL	Outline planning application with all matters reserved (except for means of site access with Longcross Road and Kitsmead Lane), for a mixed use Garden Village development comprising: residential development (Use Classes C3), care home/extra care accommodation (Use Class C2), land reserved for travelling showpeople plots (sui generis), retail, food and drink (Use Classes E and F.2), public house (sui generis), community facilities (Use Classes E, F1 and F2), employment use (Use Class E), a primary school including early years provision (Use Class F1), public open space including allotments, sports pitches and ancillary facilities (Use Class F2), Suitable Alternative Natural Greenspace (SANG) (Use Class F2), landscaping and associated infrastructure and works including enabling demolition and ground works (Environmental Statement submitted)
TYPE	Outline Application
EXPIRY DATE	24/04/2024 (EoT)
WARD	Longcross, Lyne and Chertsey South
CASE OFFICER	Melissa Gale
REASON FOR COMMITTEE DETERMINATION	Major application
<i>If you have questions about this report please contact Ashley Smith, Victoria Gibson or the case officer.</i>	

1. SUMMARY OF RECOMMENDATION

It is recommended the Planning Committee authorises the HoP:

- A To approve the application subject to the negotiation and completion of a S106 agreement and relevant planning conditions to his satisfaction (as detailed in part 9 of the report and indicatively set out at appendix one)
OR
- B To refuse planning permission at the discretion of the HoP should the S106 not progress to his satisfaction or if any other material planning matters arise prior to the issuing of the decision that in the opinion of the HoP would warrant the refusal of the application

2. DETAILS OF THE SITE AND ITS SURROUNDINGS

- 2.1 The application site comprises a total area of 79.52 hectares and forms a large proportion of the land forming the Longcross Garden Village (LGV) site allocation south of the M3. The site is allocated under policy SD9 of the Local Plan for residential development including employment and associated community uses. The wider LGV allocation covers a total site area of some 137 hectares to the north and south of the M3. The site was formerly used by the Defence & Research Agency (DERA) as a military tank testing facility together with land north of the M3. Following the adoption of the 2030 Local Plan the site now lies within the urban area. The Garden village site allocation also includes a parcel of land to the east of Kitsmead Lane for 'publicly accessible open space' and allocated SANG (Chertsey Common) which remains within the green belt.
- 2.2 The land north of the M3 has the benefit of planning permission. The residential part has been completed comprising 186 dwellings, a 'Discovery Building' (mixed use), open space, play space and SANG. Planning permission is also underway for a Data Centre complex. The remaining western half of the northern site is occupied by Netflix as film studios utilising former military buildings together with new stage and associated workshop buildings and structures. To the north is Longcross railway station.
- 2.3 The application site, commonly referred to as Longcross South, is bordered to the north by the M3 motorway and to the east by Kitsmead Lane, Chertsey Common SANG (part implemented) and existing residential dwellings (Hurst Lodge, Kitsmead House, 1-3 Kitsmead Lane and Chevythorn). To the south, the site is bounded by Longcross Road (B386) together with Holly Close and Tanglewood Close. Longcross Church (which is Grade 2 listed together with lychgate with attached churchyard wall) and several other dwellings are also located immediately south of the application site with access directly on to Longcross Road. To the west of the site lies Albury Close, and the former Longcross Barracks which is vacant. Immediately south of Longcross Road and forming a significant proportion of its frontage is Longcross House and its curtilage, together with Lily Pond Farm and Flutters Hill House further east. Other dwellings forming Longcross village are located close to the south-east corner of the site and the vicinity of the junctions of Longcross Road, Kitsmead Lane and Accommodation Road. The site also includes the M3 bridge which links the application site with Longcross North.
- 2.4 Further to the west and south lies Chobham Common which is designated as a National Nature Reserve and Site of Special Scientific Interest (SSSI), as well as forming a component part of the Thames Basin Heaths Special Protection Area (SPA), and Thursley, Ash Pirbright and Chobham Special Area of Conservation (SAC). The westernmost part of the Longcross South site lies within 400m of the SPA where residential development is restricted through joint agreement between Natural England and TBHSPA Local Authorities. The curtilage of Longcross Church on Longcross Road has been designated a Site of Nature Conservation Importance (SNCI).
- 2.5 The application site interior comprises a series of largely single or two-storey, utilitarian buildings (including two taller hangar buildings (equivalent to 2.5-3-storey), ex-military vehicle testing structures and extensive areas of hardstanding and woodland associated with its former use as a military vehicle testing site. A concrete test track runs around the site perimeter, with extensive tree cover surrounding this track on all sides. There is a two-storey control tower, close to the M3 bridge leading from Longcross North. Within the confines of this test track are miscellaneous military vehicle testing facilities, including extensive runs of concrete rumble strips, a large concrete skid pan (approx. 100m radius) and a tank wading pool. Most notable is a multi-gradient vehicle drop (MGVD) which comprises three very steep

test gradients for military vehicles falling directly northwards from the highest part of the site (at circa 65.0m AOD, compared to 30.0m-50.0m AOD for much of the site). The site is generally level at around 50m AOD, rising to the high point at approximately 60m AOD. However, there is a gentle fall from north to south and west to east. The carriageway of the M3 motorway is at a similar level to the northern part of the application site, but is screened by woodland vegetation.

- 2.6 This high point lies just to the north of a bronze-aged Barrow, a Scheduled Monument. The land surrounding the Barrow is identified as an 'Area of High Archaeological Potential'. East of the barrow is Barrow Hills House, a former officers' mess now largely vacant but partly used for filming purposes as a film set. The House and its south garden terrace is Grade 2 Listed. Former private gardens and a former 9-hole golf course lie within the immediate setting of the House, but distant views of the building and its terrace are now largely obscured by tree cover. The majority of tree cover along the southern and eastern edges of the site (up to the western edge of Kitsmead Lane) is protected by TPO No.6 (made Sept 1951) and an Area Tree Preservation Order was recently confirmed in March 2022, (TPO 461) covering the whole southern planning application site area. The site includes large areas of hardstanding associated with its former military use, together with woodland and scrub, along with some areas of grassland. Areas of the site have also been identified as Priority Habitat, primarily 'deciduous woodland'.
- 2.7 The application site is currently used for film production as Longcross South Studios and includes a number of temporary structures including large film stage buildings, supporting structures and portacabins. The use of the site for filming has intensified more recently and is the subject of a temporary planning permission.
- 2.8 Part of the application site (approximately 2.8ha) lies on the east side of Kitsmead Lane and comprises agricultural land. It is bordered to the north by the wooded curtilage of a residential dwelling ("Chevythorn") and to the east and south by the Chertsey Common SANG which is to be delivered in two phases pursuant to planning permission ref: RU.13/0857, the first phase already completed and open to the public as a requirement of the approved development at Longcross North. The second phase has recently received planning permission under reference RU.23/0118. Further to the north is Trumps Farm closed Landfill site and neighbouring Anaerobic Digestion and Wood Drying and Pelleting Facility. A corner of the parcel of land on the eastern side of Kitsmead Lane falls within part of a wider 'Mineral Safeguarding Area'.
- 2.9 Access into the site is currently provided via an existing vehicular road bridge over the M3 motorway (Accommodation bridge) connecting the northern and southern site. This leads onto Chobham Lane via the existing roundabout junction. On the northern side of the M3 access is also available to Longcross railway station. There is also existing vehicular access from Longcross Road to the south, currently used by the film studios. There are currently no Public Rights of Way within the site. Existing footpaths and bridleways to the south of the site extend from Longcross Road to connect existing networks within Chobham Common to the south and west. To the east existing footpath extends from Kitsmead Lane alongside Chertsey Common SANG extending northeast to Bridge Road.

3. APPLICATION DETAILS

- 3.1 This is an outline planning application, with all matters reserved for future consideration with the exception of the means of site access with Longcross Road and Kitsmead Lane. Details of access have been provided for the creation of 5 new access junctions serving

the site. They comprise 2 from the B386 Longcross Road, and 3 from Kitsmead Lane which includes access to the sports pitches and allotments on the east side of Kitsmead Lane. In addition, the existing vehicular access from Longcross Road (to the south of Barrow Hills House) would be retained and modified. In addition to the proposed new access points, the submitted plans also illustrate access available from Chobham Road to the north and over the existing M3 Accommodation bridge, as well as illustrating indicatively the potential for a new access from Kitsmead Lane to serve the proposed Travelling showperson plots, but does not form part of the full details currently under consideration.

- 3.2 Outline planning permission is sought which allows for a decision on the general principles of how the site can be developed. Several parameter plans have been submitted for approval which would inform the future reserved matters application(s). Details of access have been provided to demonstrate access routes into the site to serve the proposed development and how these fit into surrounding access/road network. The application is also supported by an Access and Movement Parameter Plan which illustrates indicatively the principal highway routes proposed within the site and how this links with the proposed vehicular access points. The parameter plan also illustrates indicative pedestrian links. Whilst this provides details as to how the internal access arrangements would work, the full details of the internal roads, routes for pedestrians and cyclists would form part of future reserved matters applications.
- 3.3 The matters of 'appearance', 'landscaping', 'layout', 'scale' and access (with exception of means of site access with Longcross Road and Kitsmead Lane) are reserved for future consideration. These are defined as:
 - 'Appearance' – the aspects of a building or place within the development which determine the visual impression the building or place makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture.
 - 'Landscaping' – the treatment of land (other than buildings) for the purpose of enhancing or protecting the amenities of the site and the area in which it is situated and includes: (a) screening by fences, walls or other means; (b) the planting of trees, hedges, shrubs or grass; (c) the formation of banks, terraces or other earthworks; (d) the laying out or provision of gardens, courts, squares, water features, sculpture or public art; and (e) the provision of other amenity features;
 - 'Layout' – the way in which buildings, routes and open spaces within the development are provided, situated and orientated in relation to each other and to buildings and spaces outside the development.
 - 'Scale' – the height, width and length of each building proposed within the development in relation to its surroundings.
 - 'Access' - the accessibility to and within the site, for vehicles, cycles and pedestrians in terms of the positioning and treatment of access and circulation routes and how these fit into the surrounding access network
- 3.4 Outline permission is sought for a mixed use Garden Village development comprising: residential development (Use Classes C3), care home/extra care accommodation (Use Class C2), land reserved for travelling showpeople plots (sui generis), retail, food and drink (Use Classes E and F.2), public house (sui generis), community facilities (Use Classes E, F1 and F2), employment use (Use Class E), a primary school including early years provision (Use Class F1), public open space including allotments, sports pitches and ancillary facilities (Use Class F2), Suitable Alternative Natural Greenspace (SANG) (Use

Class F2), landscaping and associated infrastructure and works including enabling demolition and ground works.

3.5 As this is submitted as an outline application, permission is being sought for the principle of the quantum of development proposed, whilst seeking to fix certain 'parameters' which will inform future reserved matters applications. Therefore, the detailed design of the development is not being sought under this application. The extent and types of land use within the site such as residential and the location of the mixed-use village centre and green/blue infrastructure are to be fixed within the submitted parameter plans. The details of the layout of the development i.e. the roads and siting of the buildings will form future reserved matters applications. The application is supported by an illustrative masterplan, this helps inform the Environmental Statement and to demonstrate how the development could come forward to accommodate the quantum of development proposed. It is therefore the parameter plans which will inform future reserve matters.

3.6 Parameter Plans

The application is accompanied by several parameters plans which are discussed further within the planning considerations section below. These plans set to define and fix certain parameters of the development and provide a framework to guide future reserved matters applications. The parameter plans submitted for approval include:

- Land Use
- Green Infrastructure
- Access and Movement
- Demolition Plan

3.7 Illustrative Masterplan

An illustrative masterplan has also been submitted to support the application. This plan is not for approval, but illustrates one way in which the site could be delivered. It takes account of the land uses specified within the parameter plans, and reflects the vision and design strategies and principles set out within the Design and Access Statement and supporting documents.

3.8 Whilst the quantum of development is not defined within the application description, the application is accompanied by an Environment Statement which assesses the following mix of development. This is therefore considered to be the maximum quantum/amount of development that could be considered under the current application having regard to the supporting documents but would need to be reflected within the planning decision (conditions or s106)

- Up to 1,700 dwellings (Class C3)
- Up to 9,556 sq.m of non-residential uses including:
 - Up to 1,000 sq.m retail, food and drink (Class E and F2)
 - Up to 770 sq.m for a public house (sui-generis use)
 - Up to 1,500 sq.m flexible community facilities (class E, F1 and F2)
 - Up to 300 sq.m employment uses (class E)
 - A 2 FE Primary School including early years provision (use Class F1)
 - A 60 bed car home (3,700 sq.m) (Class C2)
- Public open space (to include allotments, sports provision and ancillary facilities) (Class F2), SANG at Chertsey Common, with associated landscaping and infrastructure works.

- 3.9 In response to comments received during the course of the application process, minor amendments have been made to the development parameter plans and relevant supporting documents, in May 2023 and November 2023. The main amendments are summarised below:

Amendments to masterplan and parameter plans include:

- Increase in area of the school site which allows for early years provision and outdoor sports provision, and change to footpath connections around school.
- Increase to area allocated for Travelling show person plots, to provide minimum of ¼ acre for each plot.
- Revised pub location and re-siting of residential parcel from eastern to western side of Barrowshill to preserve views from south of the site.

In addition, amendments and updates to other supporting information to take account of comments raised during the course of the application.

As a result, the parameter plans have been updated to reflect the above changes and an ES addendum provided to include climate change and to consider the effects of the revisions made. This is summarised within a covering letter dated 11 May 2023 and 2nd November 2023.

- 3.10 The application is accompanied by a Design and Access Statement which provides further detail on the type, quantum and indicative phasing of the development proposed including design guidance and principles to guide subsequent reserved matters applications. This has been updated to taken account of revisions made. Whilst the application description does not define a specific number of dwellings proposed, the supporting information and illustrate masterplan proposes up to 1700 (use class C3 dwellings), together with a care home/extra care accommodation (use Class C2) and Travelling show person plots. The application proposes 35% affordable housing with a mix of tenure and size including First Homes, Affordable and Social Rent and Shared Ownership.
- 3.11 The application documents advise that the construction programme for the Development is anticipated to span approximately 10 years and is expected to be built out in a five core phases in addition to the care home, school land and Travelling show people plots where delivery will be secured by s106 or Condition to reflect appropriate trigger points. Each phase will include demolition, clearance, site enabling, levelling and construction works for approximately 300 to 400 dwellings and all associated new drainage and utility works. The mitigation measures proposed within the ES to minimise adverse effects during the construction phase would be implemented through a Construction Environmental Management Plan (CEMP).

3.12 Environmental Statement

The development falls within the category 'Urban Development Projects' (Schedule 2, 10 (b)) of the Town and Country Planning Environmental Impact Assessment) (England and Wales) Regulations (2017) (as amended). Having regard to the scale of the development, a Scoping Opinion was submitted on 9th May 2017 under the 2011 regulations. The application has been submitted with an Environmental Statement which covers various topic areas and includes a number of supporting documents. The Environmental Statement was prepared in accordance with the 2011 EIA Regulations due to the date of the EIA Scoping Opinion. However, following comments from the Council consultants additional information has been provided to reflect the requirements of the 2017

regulations having regard to the passage of time between submission of the scoping opinion and submission of the planning application. This includes the addition of a chapter in respect of climate change. Additional information and points of clarification have been submitted in the form of ES addendum(s) during the course of the application.

3.13 Consultation and Engagement

The applicant has been engaged in extensive pre-application discussion with both RBC and SCC, and through preparation and examination of the 2030 Local Plan. In addition, they have also undertaken various consultation events with the local community which is summarised within the Statement of Community Involvement submitted with the application.

3.14 The evolving masterplan has also been reviewed through an independent design review panel undertaken by Design South East in 2018 and in 2020 prior to submission, and in November 2022 post submission.

4. RELEVANT PLANNING HISTORY

4.1 The following history is considered relevant to this application:

	There is a long planning history relating to the application site, in particular in respect of land north of the M3 motorway (Longcross North) which has been subject to the phased implementation by the applicants of a mixed residential and commercial planning permission granted in 2014 (see below). It is already approaching full residential occupancy of 186 units (Phase 1 and 2), The mixed use Discovery Building (formerly Focal Building) is completed albeit ground floor currently unoccupied and the Data Centre complex is under construction. The key relevant application site history is therefore set out below as relevant specifically to Longcross North, Longcross South and, where applicable, both land areas combined:
--	---

Reference	Details
	Longcross North:
RU.12/0946	Replacement of main point of vehicular access to the former DERA North Site, including a roundabout, carriageway, hoarding, wall, landscaping and associated infrastructure and engineering operations from Chobham Lane. Granted December 2012.
RU.13/0856 (Whole site redevelopment)	Hybrid planning application for the demolition of existing buildings and redevelopment of the site to provide; up to 79,025sqm (GEA) of Class B1 employment uses (including parking); up to 36,000sqm (GEA) of sui generis Data Centres use (including ancillary facilities and parking); up to 200 dwellings, including a detailed first phase comprising 108 dwellings (comprising 13 x two bed, 26 x three bed, 21 x four bed and 13 x five bed dwellings; 8 x one bed apartments and 23 x two bed apartments; and 2 x one bed FOGs and 2 x two bed FOGs); roadways driveways and pavements; fencing and walling; up to 6,300sqm (GEA) of ancillary uses, including Class A1 - A5 uses (i.e. retail uses, cafe/restaurants and a public house up to 1,550sqm GEA), Class D1 uses (i.e. childcare facilities up to 600sqm GEA); Class D2 uses (i.e. Health and Leisure (up to 1900sqm GEA); the creation of Publicly Accessible Open

	<p>Space (PAOS), ecological habitats, general amenity areas (including informal and formal open spaces), equipped play areas and landscaped areas; new vehicular accesses from the existing public highway network; vehicle and cycle parking; bin stores; landscape compound; car parking (for railway station); electricity sub-stations; lighting; drainage and associated infrastructure works, including sustainable drainage systems (SUDS); a foul pumping station; an acoustic fence and associated engineering and service operations.</p> <p>Amended under RU.16/0584 (removal of Condition 32) and RU.20/0729 (Variation of Condition 9)</p> <p>Current s73 application under RU.24/0210 for the Removal of condition 9 (the requirement for demolition of upper western plateau buildings) and Variation of Condition 47 (requirements of Environmental Statement) of RU.13/0856 (as amended by RU.16/0584 as amended by RU.20/0729) to enable the retention of the existing buildings on the upper western plateau. Under consideration</p>
RU.17/1191 (Discovery Building)	<p>Reserved Matters application for the development of a 3 storey building (Focal Building) totalling 1,265sqm (GEA), including up to 1,263sqm of A1-A5 uses, up to 1,263sqm of B1 employment uses (including marketing suite), up to 600sqm of D1 uses and up to 838sqm of D2 uses; general amenity areas; vehicle parking; cycle parking; associated planting and structural landscaping; works associated with the main pond; fencing and walling; pavements and footpaths; bin store; external lighting; drainage and associated infrastructure works (including SuDS). This application forms part of Phase 2 of planning permission RU.13/0856 (as revised under RU.16/0584) (Hybrid planning permission for the demolition of existing buildings and redevelopment of the site including mixed uses, accesses, landscaping, infrastructure and utility works).</p> <p>Granted 8th December 2017.</p>
RU.23/1834 (Discovery Building)	<p>Change of use of the Discovery Building ground floor from Class E(a) (for the display or retail sale of goods, other than hot food, principally to visiting members of the public) limited only to convenience/supermarket goods), E(b) (for the sale of food and drink principally to visiting members of the public where consumption of that food and drink is mostly undertaken on the premises), public house/drinking establishment, sui generis premises for the sale of food and drink with a takeaway element that might exceed the allowances of E(b) to Class E(g) (i) (office) use</p> <p>Under consideration</p>
RU.20/1206	<p>Hybrid planning application: full planning application for a re-configured discovery building car park (to that approved under RU.17/1191); retention of the stage 2 building and associated hardstanding; Outline planning permission sought for proposed sports provision, public open space and associated landscaping; vehicular access, drop-off and car parking to the railway station; and associated engineering works (all matters reserved) and proposed security fence (all matters reserved except layout).</p> <p>Granted 8th July 2021.</p>
RU.17/1295 (Phase 2	<p>Phase 2 reserved matters application for the development of one part 4/ part 5 storey residential building, one 5 storey residential building and one 4 storey residential building comprising 78 dwellings (comprising 18 x one bed</p>

Residential)	apartments and 60 x two bed apartments); general amenity areas; vehicle parking; cycle parking; associated planting and structural landscaping; fencing and walling; pavements and footpaths; bin stores; external lighting; drainage and associated infrastructure works (including SUDS). The application forms part of phase 2 of planning permission RU.13/0856 (as revised under RU.16/0584) (Hybrid planning permission for the demolition of existing buildings and redevelopment of the site including mixed uses, accesses, landscaping, infrastructure and utility works)(Amended Plans). Granted 15 th February 2018
RU.22/0512 (Station Access)	Reserved Matters Application pursuant to application RU.20/1206 for Proposed sports provision, public open space including the creation of pedestrian routes and associated landscaping, access from Chieftain Road to Longcross Train Station, Station car parking and drop off provision, surface and foul water drainage and other associated engineering works. Refused – Appeal Approved 20 th February 2024
RU.21/0780 (Data Centre)	Phase 3 Reserved Matters application for the development of a data centre campus comprising: a) A building(s) for data storage and processing, associated cooling infrastructure, ancillary office and technical space and roof mounted PV cells; b) Energy Centre Building; c) Stand-By Generators and fuel storage; d) HV Sub-Station; e) visitor reception centre; 3 f) hard and soft landscaping and g) new roads, paths and yards and the provision of parking for cycles, cars and commercial vehicles, and requiring: h) site preparation and earthworks, i) drainage and associated infrastructure works (including SUDS), j) the erection of walls (including retaining walls) and fences, k) the installation of external lighting and necessary physical security systems, and l) other enabling works required during the construction and operation of the data centre campus The application forms part of phase 3 of planning permission RU.13/0856 (as revised under RU.16/0584) (Hybrid planning permission for the demolition of existing buildings and redevelopment of the Longcross North site) Granted - November 2021 Amendments approved under Section 73 application RU.22/0686 - Granted July 2022
RU.19/1851 (Netflix)	The erection of three replacement film studio workshops; a management office and associated car parking area; and a security hut for a temporary period of five years. Granted 5 th March 2020. Amendments approved under Section 73 application RU.23/0651 to amend date referenced within conditions - Granted 25 th July 2023
RU.21/1556 (Netflix)	Construction of two demountable stages and associated works for temporary period. Granted 22 nd December 2021
RU.23/1547 (Netflix)	Full planning application for two x (2,107sqm) stage buildings (retention of the two stage buildings previously granted temporary planning permission under ref. RU.21/1556) Under consideration
	Longcross South:

<p>RU.23/0118 (Film Studios)</p>	<p>Temporary planning permission for the use of the land for film studio purposes including the erection of studio stages and workshops, erection of cabins and marquees within dedicated zones, and the use of the land for associated filming purposes and as studio backlot (part retrospective)</p> <p>Granted 02/02/2024 (temporary permission until 31st January 2028)</p>
<p>RU.22/1508 (Film Studios)</p>	<p>Two Film Studio Sound Stages (for a temporary period of 5 years) (retrospective). Known as Meganova C and D and located to the north of the central 'skid pan' area. Granted 24 March 2023 for a temporary period expiring on 1st January 2028.</p>
<p>RU.21/1806 (Barrowhills)</p>	<p>Proposed siting of temporary structures and the use of land for filming including: 2 no. temporary structures on existing car park and 1 no. temporary structure on former tennis court, erection of 1 no. marquee and retention of existing marquee and siting of 9no. attached portacabin units together with temporary surfacing for car parking and associated temporary internal access. Granted consent 21/01/2022 (relates to land to the south of Barrow Hills House)</p>
<p>RU.23/0795 (Barrowhills)</p>	<p>Variation of Condition 2 (Temporary Permission) of planning approval RU.21/1806 (Proposed siting of temporary structures and the use of land for filming including: 2 no. temporary structures on existing car park and 1 no. temporary structure on former tennis court, erection of 1 no. marquee and retention of existing marquee and siting of 9no. attached portacabin units together with temporary surfacing for car parking and associated temporary internal access) to extend the temporary permission by up to 3 years.</p> <p>Granted consent 15/11/2023 (As above - relates to land to the south of Barrow Hills House)</p>
<p>RU.04/1297 (CLUED)</p>	<p>Certificate of Existing Lawfulness for a mixed lawful use involving military related and non-military commercial uses which were carried out by or on behalf of the Crown for the following:</p> <p>(1) the testing & evaluation of tracked and wheeled military vehicles;</p> <p>(2) driver training courses using military vehicles;</p> <p>(3) subject to (i) and (ii) below and between the hours of 8am to midnight and for a maximum of 4 occasions between the hours of midnight to 8am. Monday to Friday for film and media work including the filming of television shows</p> <p>(4) Subject to (i) below and between the hours of 8am to 6pm Monday to Friday for:</p> <p style="padding-left: 40px;">(a) civilian commercial automotive vehicle testing and other specialist tasks relating to the testing of new & used motor vehicles, heavy goods vehicles, emergency service vehicles and motorcycles;</p> <p style="padding-left: 40px;">(b) automotive club events including static exhibitions and shows of all makes and models of vehicles;</p> <p style="padding-left: 40px;">(c) corporate driving events including recreation, entertainment and corporate hospitality events, and</p> <p style="padding-left: 40px;">(d) driver training courses using civilian vehicles</p> <p>(5) Subject to (i) below and between the hours of 08.00 to 18.00 for a maximum of ten motor rally events per annum to be held no more frequently than one event every four weeks and each event to last no longer than two</p>

	<p>days including setting up and testing the facilities.</p> <p>(i) the qualifying noise level for vehicles not to exceed 100dB(A) measured at 0.5m from the source of the noise, and</p> <p>(b) Any use of motor vehicles between the hours of 18.00 and 08.00 not to amount to a common law or statutory nuisance and any noise, vibration, light, smoke or fumes caused by any of the above events and the use of any motor vehicle to be limited to such emissions as are reasonably associated with the traffic of a normal urban carriageway between such hours.</p> <p>Grant Certificate 27/05/2005 This covers a large proportion of land but not all of the southern site and identified buildings</p>
RU.02/1414 (CLUED)	<p>Certificate of Existing Lawfulness for:</p> <ul style="list-style-type: none"> • The use of the northern site/land and specified buildings for B1 (Business) use • The use as a test track for the testing of military vehicles (southern site with exception of area around Burrow Hill House) and specified buildings • The use as a golf course within Class D2 (assembly & leisure) and 3 specified buildings (land south of Barrows Hill House) • Use as a hotel within Class C1 (Hotels) and 3 specified buildings (Burrows Hill House and immediate surroundings) <p>Use of the site for B1 (Business) use with minor ancillary uses including B8 (storage/ distribution), A3 (Food & Drink), D1 (non institutional uses), D2 (assembly & leisure) and sui generis hostel use (Longcross Barracks deleted from application) (Additional supporting information received 1st August 2003)</p> <p>Granted 26/09/2003</p>
	SANG (Chertsey Common):
RU.13/0857	<p>Hybrid planning application for the change of use from agriculture to publicly accessible open space (PAOS) (Sui Generis use), together with associated development, car park, footpaths and landscaping, including a detailed first phase of development comprising road access to an onsite car park with 12 spaces, an 800 mm hoggin path, dog proof fencing, gates, benches, signs and landscape planting, including trees and scrub and a wildflower grassland within a 5.11 ha area</p> <p>Granted 03/04/2014</p>
RU.22/1348 (Phase 2 SANG)	<p>Application for the approval of Reserved Matters (Access, Appearance, Landscaping, Layout and Scale) pursuant to hybrid planning permission (ref: RU.13/0857) for the change of use from agriculture to form the Phase 2 publicly accessible open space to be used as Suitable Alternative Natural Greenspace (SANG) at Chertsey Common, together with associated development for car park extension, footpaths and associated landscaping.</p> <p>Granted 12/07/2022</p>

5 SUMMARY OF MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

- 5.1 National Planning Policy Framework and Guidance.
- 5.2 The Runnymede 2030 Local Plan was adopted on 16 July 2020 and the policies have to be read as a whole. Any specific key policies will be referred to in the planning considerations, however the following policies are considered of relevance to the current applications:

Runnymede Local Plan 2030:

The site as allocated within the local plan as Longcross Garden Village under Policy SD9.

Other relevant policies include:

SD1 (Spatial Development Strategy); SD2 (Site Allocations); SD3 (Active and Sustainable Travel); SD4 (Highway Design Considerations); SD5 (Infrastructure Provision and Timing); SD7 (Sustainable Design); SD8 (Renewable and Low Carbon Energy); SD9 (Longcross Garden Village); SL1 (Health and Wellbeing); SL19 (Housing Mix and Size Requirements); SL20 (Affordable Housing); SL22 (Meeting the Needs of Gypsies, Travellers and Travelling Showpeople); SL23 (Accommodating Older Persons and Students); SL24 (Self and Custom Build Housing); SL26 (New Open Space); SL28 (Playing Pitches); EE1 (Townscape and Landscape Quality); EE2 (Environmental Protection); EE3 (Strategic Heritage Policy); EE4 (Listed Buildings); EE7 (Scheduled Monuments, County Sites of Archaeological Importance(CSAIs) and Areas of High Archaeological Potential (AHAPs)); EE8 (Local listed and other Non-Designated Heritage Assets); EE9 (Biodiversity, Geodiversity and Nature Conservation); EE10 (Thames Basin Heaths Special Protection Area); EE11 (Green Infrastructure); EE12 (Blue Infrastructure); EE13 (Managing Flood Risk); IE1 (Employment allocations); IE2 (Strategic Employment Areas); IE3 (Catering for Modern Business Needs); IE4 (The Visitor Economy).

Surrey Waste Local Plan (2019-2033) (adopted 8th December 2020) - The Surrey Waste Local Plan sets out how and where different types of waste will be managed in Surrey in the future. It sets out the planning policy framework for the development of waste management facilities and is used in determining planning applications. Policy 11b Allocation of a site for a Household Waste Materials Recycling Facility at Trumps Farm

- 5.3 **SPDs** which might be a material consideration in determination:

- Infrastructure Delivery and Prioritisation SPD (November 2020)
- Runnymede Design SPD (July 2021)
- Green and Blue Infrastructure SPD (November 2021)
- Thames Basin Heaths Special Protection Area SPD April 2021
- Affordable Housing SPD (April 2022)
- Runnymede Parking Guidance (SPD) (November 2022)

Other guidance documents:

- First Homes Interim Policy Statement (January 2022)

6. CONSULTATIONS CARRIED OUT

Consultees responses

Consultee	Comments
Natural England	<p>No objection</p> <p>Subject to appropriate mitigation and contributions being secured. The mitigation measures relate to the delivery and management of SANG, adherence to the Longcross South Framework Construction Environmental Management Plan, and securing contributions to SAMM. In addition, reference to general advice is provided on the protected species and other environmental issues.</p>
National Highways	<p>No objection</p> <p>Recommends Condition to secure a Construction Environmental Management Plan (CEMP) to mitigate any adverse impact from construction phase on the strategic road network. Advises, “in the case of this development proposal, interest is in the M25 and M3 motorways and the ‘Accommodation Bridge’ situated directly above the M3 motorway. We have resolved all outstanding technical matters with the applicant and are content that the proposal would not result in unacceptable road safety issues or congestion issues on the SRN, either with or without the ‘Accommodation Bridge’ in use”.</p>
Historic England	<p>No objection</p> <p>Acknowledges that the area of the proposed development contains a number of designated and undesignated heritage assets. Response focuses on the Scheduled Monument only and advice should be sought from Runnymede Conservation Officers and Surrey Archaeology in respect of the Listed Building and undesignated heritage assets across the site.</p> <p>The inclusion of additional housing to the south-west of the Scheduled Monument may cause additional harm that was not apparent in the masterplans originally submitted. Given available information at the present time, the level of harm is likely to be on the lower end of less than substantial harm. It is also noted that some heritage benefits are proposed in order to provide balance to the harm. This mitigation can be obtained with an appropriately worded condition addressing reserved Matters. Mitigation will be required to remove potential harmful impacts (in the form of erosion by human interaction) and to provide opportunities to preserve and enhance the significance of the nationally important heritage asset. This include protection measures, management of the monument and the provision of interpretation.</p>
Environment Agency	<p>No objection</p> <p>Subject to imposition of recommended planning conditions. The Flood Risk Assessment and Outline Drainage Strategy submitted in support of this planning application provides confidence that it will be possible to suitably manage the risk posed to controlled waters by this development. Further detailed information will however be required before built development is undertaken.</p>

LLFA	<p>No objection</p> <p>Subject to conditions, to ensure the SuDS Scheme is properly implemented and maintained.</p>
Sport England	<p>No objection</p> <p>Request conditions to secure ground conditions assessment and suitable quality pitches and condition, restriction on use, a management and maintenance scheme and community use scheme for the school provision. In addition, they would welcome further information on a number of points: how the mix of provision has been informed by local context/needs; playing pitches location east of Kitsmead Lane is not ideal, and not easily accessible measures are encouraged to improve accessibility and connectivity to the site by walking and cycling. Following consultation on the updated illustrative masterplan they have advised if sports lighting could be considered as this would provide opportunities for community use into evening. Request that any sports facilities are designed in accordance with Sport England design guidance.</p>
Surrey CHA	<p>Recommends Refusal for the following reason:</p> <p>“It has not been demonstrated that the proposed bus service provision, as part of the Public Transport Strategy’ is robust enough to support the proposed development in the long term.</p> <p>There are significant concerns over the predicted costs of the bus services, predicted patronage from the development, the financial support throughout the build out and ongoing financial support and lack of a Transport Review Group with supporting Community Trust (or similar mechanism for generating finances), for ongoing support of service provision following commencement of development. The proposed use is therefore contrary to the sustainable transport objectives of the National Planning Policy Framework (2023), the Runnymede Local Plan-Longcross GV policy (SD9), and the Surrey County Council Local Transport Plan (LTP4).”</p>
RBC – Drainage	Refer to LLFA for comments
RBC – Trees	<p>No objection, if the number of entrances are necessary.</p> <p>The site is well treed on its boundaries consequently almost all new entry points will require removal of some trees. The selection of the proposed sites for the entrances does seem to have given some consideration to the depth and quality of the tree cover to be removed. However, the internal layout, which these entrances serve, will be determined in the reserved matters and that is driven by many factors not least the internal tree cover. So, when considering the exact siting of these new entrances is not possible to confirm that these proposed entrances are the best that can be achieved in terms of tree loss, just that the proposed entrances in all probability do not require more tree loss than other possible locations. I would however expect that when considering the reserved matters substantial tree planting and other arboricultural benefits will be secured to mitigate the tree loss required for this part and the whole development.</p>
RBC –	No objection

Contaminated Land	Conditions recommended. The report submitted with the application recommends further Contaminated Land site investigation works be carried out as per conditions. These should be carried out prior to development to inform ground works procedures and remediation completed prior to occupation.
RBC – Environmental Protection	<p>Comments provided</p> <p>Consideration to protection of gardens from noise and mitigation measures in final design consideration. Measures to reduce noise impact during construction. Suggest pre completion testing to ensure mitigation measures successfully achieve internal and external noise targets.</p> <p>Officer note: As this is an outline planning application, this will be a matter for RMA consideration. If a scheme cannot show a satisfactory layout from an amenity perspective this may result in a lower quantum of development being achieved at RM stage.</p>
RBC – Energy Officer	<p>Comments provided</p> <p>The approach outlined in the Statement is supported, namely a fabric-first approach through energy efficient design and materials, before efficient building services, and then low carbon/renewable energy are used to reduce emissions further. Need to ensure any reserved matters application demonstrates compliance with the committed principles in the Energy & Sustainability Statement sufficiently. The applicant proposes a fully electric strategy to coincide with the Government’s direction of travel with on-going decarbonisation of the electricity grid. the Energy Statement should continue to demonstrate how opportunities for renewable/low carbon energy have been maximised. No consideration in strategy of energy storage. Has the applicant demonstrated how green roofs could be incorporated into the design? Consideration could be given to the use of a scheme such as BREEAM. Charging points for electric bikes should be considered. Would have liked to see more detail on proposed ‘site-wide’ approaches to renewable and low carbon energy provision.</p>
RBC – Historic Buildings and Conservation Advice	<p>No objection - comments provided</p> <p>The proposals are not considered to have an adverse impact on the setting of significance of Longcross Church.</p> <p>The element of the proposals which may have the potential to impact the significance of Barrow Hill, through change within its setting, is the proposed courtyard-style development to the immediate north-west of the heritage asset. Built structures have occupied this area in various forms since the construction of Barrow Hills, and so the principle of development is established. Developed designs, and future applications, should be supported by a heritage statement.</p>
RBC – Affordable Housing	<p>Comments provided</p> <p>Affordable Housing Note confirms policy compliance. Need to ensure steady supply of Affordable Homes through the phasing and not lots all at once. Would like to see increase in number of adapted homes for</p>

	<p>nomination. Need to ensure social rent is included and not just affordable rent. Would like to seek a small block of 1 bed/studio flats as supported housing</p>
<p>Surrey Wildlife Trust</p>	<p>Comments provided</p> <p>We would advise that the LPA is confident that all of the recommendations and requirements of Natural England are understood and followed. The application has the feasibility to provide a net increase in biodiversity units. However, it must be noted that trading rules have been failed due to the strategy not providing sufficient compensation for 'Woodland and forest – Felled'. We would advise that if the application is granted, then prior to the commencement of each individual phase, a biodiversity gain plan is submitted. Each phase should be required to submit an Overall Biodiversity Gain Plan and a Phase Biodiversity Gain Plan and Woodland Management Plan which demonstrates sufficient compensation and mitigation for the baseline habitat felled woodland. We would advise that all linked SANG Management Plans have specific regard for the lowland dry acid grassland requirements. Additional protected species surveys are required.</p>
<p>Surrey Archaeology</p>	<p>No objection</p> <p>Recommend that any detailed planning application(s) to follow should be accompanied by the results of a targeted trial trench evaluation and a programme of Historic Building recording supplementing the Level 1 survey.</p> <p>This will ensure that any buried archaeological assets are identified and recorded and that an appropriate record is made of the identified undesignated heritage assets in advance of their loss.</p>
<p>Surrey Minerals and Waste</p>	<p>No objection</p> <p>Subject to RBC being satisfied in respect of impact and appropriate mitigation to prevent unnecessary amenity impacts to sensitive receptors; includes adequate facilities for waste storage and recycling; and condition to secure a waste management plan.</p>
<p>Surrey CC- Trumps Farm Landfill site (adjacent landowner)</p>	<p>Comments provided – in context of adjacent landowner</p> <ol style="list-style-type: none"> 1. Proximity of the proposed residential from the Trumps Farm closed landfill site. The impact of potential landfill gas migration should be explicitly evaluated in the full application. 2. Surrey County Council would like to retain access to the 2 x groundwater monitoring boreholes located on site until such a time as they are removed as part of the development. 3. The proposed access from the northern end of Kitsmead Lane is very close to the shared entrance for Severn Trent Green Power, ADF Facilities and Morris Leslie Plant Hire Ltd, the combination of which generate a significant number of HGV movements, producing an elevated risk of conflict. 4. Kitsmead Lane is narrow in parts and there is regular existing conflict between HGV's, cars and cyclists using the road. The indicative routing for HGV's will cause the level of conflict to increase unless the existing road is

	<p>widened.</p> <p>5. HGV's turning left from Kitsmead Lane onto Longcross Road need to swing out onto the opposite carriageway to make the corner. The junction could be improved to make this turn safer or HGV's could be routed along Chobham Lane and Longcross Road via the roundabout.</p>
Surrey CC Education	<p>No objection</p> <p>Given the anticipated scale of the proposed development, Surrey County Council would request on-site early years provision, and a contribution towards additional early years places close to the development site, should the on-site provision not be sufficient to accommodate all the early years children yielded from this development.</p> <p>For this development, Surrey County Council would request provision of a new 2FE primary school and all necessary supporting infrastructure, all constructed to an agreed specification, which is able to provide full time places for 420 children. The location of the primary school will need to be determined prior to the granting of detailed planning permission, which will also determine whether any additional contribution is required should the anticipated yield from the development exceed the proposed 420 places.</p> <p>Surrey County Council would request a contribution towards secondary education infrastructure in the Runnymede planning area.</p>
Surrey Countryside Access Team	<p>No objection</p> <p>Recommend consideration of link to rights of way in vicinity (link FP45 to the east with FP47 or BW66 in the south). Would invite contribution towards enhancing paths for benefit of residents of new development via s106.</p>
NHS Surrey Heartlands ICB	<p>Comments provided</p> <p>Provision to be secured through S106. The population increase created will have an impact on the provision of primary care in the vicinity of the subject site and a site-specific contribution (either onsite or financial contribution in lieu) will therefore be sought to mitigate the impact. The ICB welcomes the developer's flexibility regarding the means of mitigation, as the ICB must ensure that the mitigation reflects the needs of the healthcare services at the time when the development is implemented.</p>
Police – Surrey and Sussex	<p>Comments provided</p> <p>To mitigate against the impact of housing growth Surrey Police have calculated the capital 'costs' of securing the necessary infrastructure to serve the proposed development. This is to support staff and fleet/vehicle costs associated, provision of 3 no. ANPR cameras and provision of space/policing office within the development/community building. To be secured through S106.</p>
Thames Water	<p>Comments provided</p> <p>Thames Water are currently working with the developer to identify and deliver the off-site foul water infrastructure needs to serve the development. Upgrades to the waste water network will be required. Conditions are recommended to reflect this. In respect of Surface Water,</p>

	no objection if the developer follows the sequential approach to disposal of surface water. The application indicates Surface Water will not discharge to the public network and therefore no objection. LLFA approval should be sought.
Affinity Water	<p>Comments provided</p> <p>Affinity Water will supply drinking water to the development in the event that it is constructed. The construction works and operation of the proposed development site should be done in accordance with the relevant British Standards and Best Management Practices, thereby significantly reducing the groundwater pollution risk. Being within a water stressed area, we expect that the development includes water efficient fixtures and fittings. Measures such as rainwater harvesting and grey water recycling help the environment by reducing pressure for abstractions.</p>
UK Power Networks	Standing advice provided
SSE Power Distribution	Confirms no comment to make
Cadent Gas	No objection - informative
Network Rail	<p>Comments provided</p> <p>Network Rail request that the development provides a contribution to mitigate the addition usage, ensuring that the station can continue to operate effectively and provide a good standard for rail passengers. Network Rail agree that the station improvements identified by Southwestern Railway is necessary to mitigate the impact of the proposed development. Southwestern Railway have identified following improvements:</p> <ol style="list-style-type: none"> 1.Onward Travel Information (Touch Screen Displays) – an approx. cost of £16,086. 2 Access for All (Compliant Bridge & Lifts) - an approx. cost of £4,000,000. 3.The provision of new toilet facilities at Longcross - an approx. cost of £150,000. 4.Water supply & drainage survey - an approx. cost of £15,000. <p>Officer note: Network Rail have since clarified they would be seeking £1m from the developer as a proportionate contribution towards the total 'Access for all' bridge and lifts.</p>
South Western Railway	Refer to comments from Network Rail
The Showmans Guild	<p>Comments provided</p> <p>A minimum ½ acre per plot is required for each travelling show person plot Each plot requires both living area and working/storage area.</p> <p>A shared working/storage area would not work. Separation needs to be provided for each plot for living purposes and for safe-working legislation. Space needed for parking and turning large articulated vehicles. A working area possibly including a workshop/covered area and space to build up</p>

	rides. Need suitable site access with direct access from adjacent main road, and internal roads for access and turning. Plot shapes should be regular. Plots could vary in size. Security consideration when considering location/layout. Consider proximity to residential to avoid any potential disturbance from out of hours movement. Access to schools, shops and medical facilities.
Enterprise M3 LEP	No response
Historic Buildings and Places (Ancient Monument Society)	No response
RSPB	No response
Chobham Parish Council	<p>Objections Raised</p> <p>Concern impact development would have on Chobham's infrastructure, impact of traffic generated and potential adverse effects on Chobham Common. Roads are unsuitable for the heavier traffic volumes and HGV's, Staple Hill and Windsor Road junction presents highway safety risks, Lack of primarily healthcare and other social infrastructure creating strain on existing services. Insufficient community engagement. Measures to prevent neighbouring roads (Burma Road and Chobham Common car parks) being used for construction traffic. Should not impact negatively water and waste water services. Limited information regarding public transport linkages and appear to encourage car dependency. Proximity of development to Chobham Common would breach integrity of the TBHSPA. Concern over high rate of biodiversity loss and extinction and recent clearance of some green areas</p>
Virginia Water Neighbourhood Forum	No response
Sunningdale Parish Council	No response
Windlesham Parish Council	Concerns raised that insufficient consideration has been given to bigger picture, and whilst outside of the Borough, the development is likely to have a significant impact on more rural communities within the Parish. Concerns that adequate infrastructure to support the development including M3 junction 3 improvements, post primary school provision and GP medical provision has not been considered.
Surrey Heath Borough Council	<p>No formal objection in principle but makes following comment:</p> <p>Ensure that there would be no significant harm to highway network during construction period with a robust construction logistics/method plan agreed prior to the commencement of any works in conjunction with the highway authority. The site lies in close proximity to the Thames Basin Heath SPA and any mitigation measures recommended by Natural England shall be secured.</p>

Royal Borough of Windsor and Maidenhead	Comments provided Consider impact on then local highway network including the routing of vehicles during construction phase to ensure minimum disruption. Appropriate mitigation measures need to be secured in respect of sites proximity to Thames Basin Heaths SPA
Woking Borough Council	No objection
The British Horse Society	Objections raised Development should take opportunity to positively benefit equestrian access as mitigation for the additional traffic arising. If approved conditions recommended to secure equestrian provision through site and connectivity to adjacent bridleways including controlled crossings.
Chobham Common Preservation Committee	No response

6.2 The Secretary of State has also been notified of the application in line with the requirements of the Town and Country Planning (Consultation) (England) Direction 2024 and the Town and Country Planning (Environmental Impact Assessment) Regulations 2017. Confirmation has been received that they have no comment to make on the environmental statement.

Representations and comments from interested parties

6.3 489 Neighbouring properties were consulted in addition to being advertised on the Council's website and in the local press. In addition, a number of site notices were erected around the site boundaries and in the vicinity of the proposed development.

Following the receipt of additional supporting information and amendments to the parameter plans the application has been re-advertised during the course of the application, this has included letters of neighbouring properties, Site Notices and re-advertised within the local press:

A total of 45 letters of representation have been received from 32 separate properties with regard to the original scheme, and a further 9 letters following re-consultation during the course of the application. In total 54 letters have been received from 34 separate properties, which can be summarised as follows:

Highways/Travel

- Existing dangerous road junctions
- Local roads unsuitable for large increase in vehicles
- Road network cannot support increase in HGV's
- Trumps Farm site generates outrageous amount of daily HGV traffic which already struggle to pass each other
- Wellington Avenue and Trumps Green Road have been adversely affected by increase in HGV's

- Local schools would be impacted by increase in traffic and HGV's
- Risk to pedestrians and cyclists
- Outdated methodology for determining the level of pedestrian and cyclist infrastructure provision
- Increase in traffic, including deliveries
- Pressure on local parking
- Impact and additional pressure/strain on local roads
- Danger for pedestrians and cyclists - Roads already bus and narrow, Pavements narrow or lacking - passage to amenities unsafe or not suitable
- Lack of off-road cycle infrastructure outside the site boundary
- Junctions along A320 corridor already over capacity, will be exacerbated by Local Plan allocations, cause more gridlock
- Inadequacies of Transport Assessment
- Lack of assessment of junctions along the A320 corridor – resulting in a fundamental flaw in the EIA process and Environmental Statement submitted with the application
- lack of clarity regarding mitigation measures
- A320 works dependant on third-party land which questions deliverability of the mitigation measures and capacity of the highway network to accommodate the Local Plan allocations.
- Viability may restrict funds available for A320 mitigation works.
- Shame new junction onto M3 using MOD land is not considered
- New roads should connect development with the A30 and better disperse traffic
- Toucan crossing inadequate, highway safety concern crossing Kitsmead Lane between SANG/pitches and main site

Public transport

- Poor public transport requires multiple vehicles per dwelling
- Limited public transport services
- Bus stops are a mile away – too far to walk to.
- Bus stops should be located in a lay by to improve flow

Highway - other

- Absence of sustainable transport opportunities – site too remote
- Location of car park access to sports field and allotments directly opposite a residential driveway
- Increase in traffic congestion and pollution
- Distance to local facilities including Virginia Water
- Sustainability and location of development - Isolation, remote and disconnected from any other main centres or existing facilities, residents likely to use cars and not sustainable transport modes
- Reduced speed limit along B386 is welcomed

Character

- Impact on character of the area
- Not in keeping/ negative visual impact
- Overdevelopment, Inappropriate layout and siting - proposed density

- Effect on local character, of 1750 new dwellings and associated facilities and with application for further 700 homes further up Trumps Green Road and a small scale clinical waste thermal treatment facility close to site.
- Affect green countryside space – change character, destroy tranquil character and countryside views

Environmental

- Impact on wildlife and natural habitat/ flora and fauna
- Increase noise and pollution
- Impact on air quality
- Increase in litter and crime
- Loss of trees will mean increase in pollution from M3
- Pollution from the M3 link to health problems
- Impact of additional cars and people
- Adjacent to/negative impact on Chobham Common, including views – little and insufficient mitigation to protected area.
- Impact on adjacent environment of important categories (SSSI, NNR, SAC and SPA)
- Construction impact on local area from construction waste and dust, vibration, traffic and noise pollution
- Football pitches, parking and sports pavilion next to existing SANG would erode status of existing SANG
- Insufficient parking for increased visitors to SANG
- Existing grass verges destroyed by vehicles unable to park at Chertsey Common
- Light and noise impact on surrounding sensitive ecological area
- Impact on quality of life of the residents
- Loss and interference with local open spaces impact on mental health
- Limited assessment of viability of Decentralised Energy Networks
- Framework Construction Environmental Plan is lacking in detail
- Not clear if the green spaces will be accessible to the public
- Unnecessary light pollution if floodlights proposed to sports pitches and wildlife impacts
- No quality of life or Environmental Impact to establish cumulative impact of all environmentally detrimental applications in surrounding area

Infrastructure

- Too much pressure on local amenities including public services, schools, GP surgery, dentists, public transport, hospitals, dentists, utility supplies, drainage and water, and other infrastructure
- Lack of infrastructure/inadequate existing infrastructure
- Too many homes, and local surroundings are at capacity
- No secondary school provision, this is more pressing need
- Neighbouring Virginia Water – too small to cope
- Doesn't take account of impact of additional visitors drawn to the planned amenities
- Impact from several proposed developments in local area
- No thought to damage the development will leave if it is not finished.
- Environmental and social impact on existing and new residents
- Scale of development is half of the housing need in Borough by 2030 in one area

- Has already been large number of housing developments in this area.
- We have more traveller plots, sites and pitches in local area relative to surrounding Boroughs in London and South East. Focus should be on genuinely affordable housing for key workers.
- Lack detail how travelling showpeople plots will be managed and who authorised to use them. No plan for integration into wider Garden Village community.
- Runnymede Council should provide a detailed and independent run survey of analysis/report relating to Traffic Data, Wildlife and local environment, ecosystems and Travelling showpeople, not by the applicant.
- Without suitable alternative pedestrian network, would exacerbate problems of trespass incidents to Longcross Estate

Other

- Low quality and problems of existing development at Upper Longcross
- Issues should be addressed with current northern site before more building being allowed
- Significant management costs to residents of Upper Longcross and various issues experienced to date.
- Current development at Upper Longcross still does not have the local amenities (shop, sports pitches) promised in original application
- Appears to be no proposed regulation of the Travelling showperson plots
- Impact on neighbouring amenity and highway from land reserved for Travelling showpeople, due to disturbance at all times with large and heavy vehicles and equipment
- Any buildings at playing pitches potential to attract antisocial behaviour, litter etc
- Unfair that applicant carry out consultation on major proposals during holidays, and should be for longer than a month
- Pre-submission consultation not engaged closely or thoroughly enough with residents
- Description does not make clear the number of homes proposed

7. Habitats Regulations Assessment - Appropriate Assessment

- 7.1 To the south and west of the site beyond Longcross Road is Chobham Common which is a National Nature Reserve (NNR), a Site of Special Scientific Interest (SSSI) and forms a component part of the Thames Basin Heaths Special Protection Area (TBHSPA) and part of the Thursley, Ash, Pirbright and Chobham Special Area of Conservation (SAC). Together the SPA and SAC make up the Natura 2000 network of protected sites under the European Habitats Directive, incorporating the designations under the Birds Directive. The Thames Basin Heaths Special Protection Area (TBHSPA) was designated to protect internationally important numbers of populations of three rare bird species namely the Dartford Warbler, Woodlark and Nightjars. The Thursley, Ash, Pirbright and Chobham Special Area of Conservation (SAC) is designated for its internationally important habitat complexes, including depressions on peat substrates, dry heaths and wet heaths.
- 7.2 The provisions of the Habitats Directive have been transposed into UK law by the Conservation of Habitats and Species Regulations 2017 (as amended) (known as the 'Habitat Regulations'). The Habitats Regulations place a particular responsibility on a decision maker. The Local Planning Authority (as Competent Authority) must ascertain that

the project under consideration will not have an adverse effect on the integrity of the site(s), either alone or in combination with other plans and projects, either directly or indirectly, before granting permission. The applicant has submitted information to inform the Habitats Assessment. This has identified several potential impact pathways, notably in respect of recreational pressure from residents, urban edge effects such as fires, predation and domestic waste, surface and groundwater, noise, and air/dust pollution associated with demolition and construction and lighting. These are considered further below.

- 7.3 It is necessary for RBC as ‘competent authority’ under the Conservation of Habitats and Species Regulations 2017 (as amended) to undertake a HRA of the proposals to determine whether they are likely to have significant effect on the International sites either alone or in combination with other plans and projects, and if so to undertake an Appropriate Assessment to determine whether there will be an adverse effect on the integrity of the international Sites.
- 7.4 The allocation of the site within the Local Plan was informed by Habitats Regulations Assessment and other supporting documents, which established the principle of the development of the site. It was acknowledged that detailed proposals for development will be subject to an Appropriate Assessment, guided by Policy EE10 (Thames Basin Heaths SPA to determine whether there would be an adverse effect on the integrity of the International sites. It was recognised that due to the location of the development within the predicted zone of influence to the international designated sites forming the Thames Basin Heaths SPA and the Thursley, Ash, Pirbright and Chobham Special Area of Conservation (SAC), the development is likely to contribute to recreational pressure on the SPA. Therefore, a higher standard of SANG is required to avoid significant effects, to help manage the recreational pressures on the SPA. This is reflected of the SANG delivered pursuant to the existing residential development north of the M3 and is also proposed under the current application.
- 7.5 Natural England have provided input through the pre-application stage and during the course of the planning application with revisions made to the SANG layout and supporting management plan to take into account comments made. The application proposes both on-site SANG located centrally within the site at Barrowshill and linking through to the offsite SANG at Chertsey Common to the east of Kitsmead Road. The provision proposed is consistent with the Thames Basin Heaths SPA SPD. No residential development is proposed within 400m of the SPA. The application proposes the provision of SANG at the higher rate of 10.7ha per 1,000 population as agreed with Natural England.
- 7.6 The layout of the SANG and associated Delivery and Management plan has been revised during the course of the application to have regard to the comments of Natural England. Natural England have raised no objection to the application subject to securing mitigation measures in respect of the delivery of SANG in accordance with the Barrowhills and Chertsey Common Phase 2, Delivery and Management Plan and securing contributions to Strategic Access Management and Monitoring (SAMM). It is therefore considered that the SANG proposals and the contributions to the SAMM project are acceptable to avoid and mitigate the impacts of the development from recreational disturbance on the internationally important sites and thus avoid any likely significant effect on these international sites.
- 7.7 **Other Effects on the SPA and SAC**
- 7.8 Urban edge effects
Having regard to the proximity of the development to the SPA and SAC, consideration is also given to the potential urban edge impacts resulting from the development. This is

considered within the supporting information submitted with the application. No residential development is proposed within 400 metres of the SPA/SAC and the financial contributions to the SAMM project for the SPA wardening and management to help reduce the incidents of fire and fly-tipping. Therefore, this is not predicted to result in significant adverse effect on the SPA and SAC.

7.9 Water quality

In terms of surface and ground water drainage impacts, none of the site catchment areas have connections with Chobham Common. Therefore, there is no potential for an adverse impact as a result of the Development including contamination in groundwater or in surface water run off. Affinity water have indicated that water supply would be from off-site sources, outside the Bagshot formation which underlies the site and the SAC and therefore no potential adverse effect during the operational phase.

7.10 Noise

During the demolition and construction phases mitigation measures to be set out in the CEMP including measures to monitor noise levels will be required. The modelling showed that predicted noise levels are 50 dB LAeq,T or below during all activities and phases except site clearance and demolition during Phase 1, for which the predicted LAeq,T is still below 55 dB. With exception of loud but discontinuous noise events which would not exceed 85 dB (maximum, LAmax). At their very closest the temporary demolition/construction activities on the Site would lie in excess of 490 m from Chobham Common and would lie below the maximum thresholds. Noise levels are not predicted to have an adverse effect on breeding behaviour of birds and therefore no likely significant effect on the SPA.

7.11 Dust

It is recognised that Heathland habitats are vulnerable to the deposition of dust which may occur during construction stage. Dust impacts during demolition and construction can be suitably mitigated to reduce impact on air quality which includes CEMP and therefore it is concluded therefore no likely significant effect on the SPA.

7.12 Air Quality

Heathland Habitats are also sensitive to changes in air quality including atmospheric concentrations of oxides of Nitrogen (NOx) usually associated with traffic emissions and leading to localised effects, and nutrient deposition which may lead to changes in habitat composition and condition in the long term. The greatest impact on air quality due to emissions from vehicles and plant associated with the construction phase will be in the areas immediately adjacent to the site access. The likely impacts on air quality have been considered within the supporting documents. It is considered that the level of additional traffic is unlikely to cause any significant changes to local air quality. Anticipated traffic movements are given as a worst case scenario with numbers varying during the construction period, reducing over time. It is considered that having regard to current air quality, the proximity to sensitive receptors to the roads likely to be used by construction vehicles, and the level of additional traffic, the impacts are considered to be of negligible significance and is therefore unlikely to cause any significant changes to local air quality.

7.13 Lighting

Temporary lighting is expected during the construction phase, and artificial lighting has the potential to result in glare and light spill. However, given the distance to the SPA this is unlikely to impact on sensitive receptors. Whilst there is the potential for increase lighting levels, this is likely to reflect certain areas of construction and during working hours. Bird breeding period is during the summer months when lighting would not be required. During

the operational phase (occupation of the development), there will be changes resulting from artificial lighting, including street lighting, security and access lighting. Whilst this has potential to impact on ecological species within the site, the lighting strategy together with landscaping which will be secured by condition and through RMA details, will ensure suitable provision. Together with retention of trees and vegetation within and along the boundaries of the site and distance from Chobham Common is unlikely to result in harm to sensitive receptors.

7.14 Conclusion on Habitats Regulations Assessment

Natural England following the receipt of additional and updated information in respect of the management and maintenance of the proposed SANG, have raised no objection to the proposed development. Subject to appropriate mitigation and conditions being secured the application is not considered to have an adverse effect on the integrity of Thames Basin Heaths Special Protection Area (SPA) or damage or destroy the interest features for which Chobham Common Site of Special Scientific Interest (SSSI) has been notified. The mitigation requirements of Natural England are to be covered by conditions and/or included in the S106 agreement. This relates to the provision and management of the SANG, SAMM contributions and securing a Construction Environmental Management Plan (CEMP).

- 7.15 An appropriate assessment has been carried out to consider whether there will be adverse effects on the integrity of the International sites in light of their conservation objectives, taking into account the impact avoidance and mitigation measures proposed, and in light of the above information and information submitted to support the application and having given weight to the advice of Natural England, it is considered that providing the specified avoidance and mitigation measures are secured by the recommended S106 legal agreement and planning conditions the proposed development would avoid adverse effects which have been identified on the integrity of the International Sites when considered under Appropriate Assessment.

This report will now consider all of the planning issues relating to this application.

8. PLANNING CONSIDERATIONS

8.1 Planning Considerations

- 8.1.1 In the determination of this application regard must be had to the Development Plan and National policy within the NPPF. The application site was removed from the Green Belt upon adoption of the 2030 Local Plan, and is located within the urban area, forming a designated site within the Local Plan, where the principle of such development is considered to be acceptable subject to detailed consideration. The application site also includes an area of land to the east of Kitsmead Lane which whilst forming part of the Local Plan designation as forming public accessible open space remains within the Green Belt where strict policies apply. This must be considered in light of the presumption in favour of sustainable development advocated by the NPPF.

- 8.1.2 The key planning matters in respect of this application are considered to be as follows:

- Habitats Regulations Assessment - Appropriate Assessment (Section 7 above)
- Principle of Development and policy context
- Quantum of Development
- Indicative Masterplan and Proposed strategy for development
- Proposed Land Uses and Development Parameters for approval
- Phasing and Sequencing

- Scale/Density, Design and layout
- EIA
- Socio-economic effects/benefits
- Highways and movement:
 - On-site Site Access junction Details
 - Off site highway works
 - A320 works
 - Movement and sustainable transport
 - M3 Accommodation bridge
 - Internal access network
 - Wider pedestrian and cycle connectivity
 - Sustainable Travel measures
 - Public Transport strategy (Rail and Bus Service)
 - Highways summary/conclusion
- Landscape and Visual effects
- Ecology and Nature Conservation
- Protected Species
- Biodiversity Net Gain
- Trees
- SANG provision
- Open Space Strategy, Childrens' Play and Sports Provision
- Food Production
- Land Contamination
- Flood Risk, Drainage and water resources
- Air Quality
- Noise and Vibration
- Heritage
- Sustainability (Energy and Climate Change)
- Housing – supply/mix
 - Affordable Housing
 - Self build and custom build
 - Extra care
 - Travelling showpeople plots
- Education and Health
- Police and Crime prevention
- Utilities
 - Water
 - Electric
 - Gas
 - Telecommunications
- Minerals and Waste
- Residential amenities
- Land East of Kitsmead Lane
- Garden Village Principles/ Community Stewardship

8.2 Principle of Development and Policy Context

- 8.2.1 The application site forms a large proportion, but not all of the Longcross Garden Village site allocation within the Local plan. The site lies principally within the urban area following the adoption of the Local Plan which removed the site from the Green Belt. The exception being the land to the east of Kitsmead Lane which is designated as Publicly Accessible Open Space within the Local Plan and remains within the green belt. Through the Local Plan, Longcross has been identified as the focus for a major residential led development creating a new sustainable mixed use settlement and providing a range of housing types, local facilities and employment uses. The extent of the Garden Village settlement area allocated within the local plan covers a total area of 137 hectares. This reflects the area released from the green belt to form the village settlement and includes land to the north and south of the M3, and existing residential development, defined by the boundaries of Longcross Road and Kitsmead Lane.
- 8.2.2 The current application covers to total site area of 79.52 hectares comprising the main area of the southern site allocated under Policy SD9 of the Local Plan. The Local Plan allocates the site for the development of a Garden Village, including a minimum of 1,700 dwellings comprising a mixture of tenures and types, employment land, local facilities including a primary school and retail facilities, community facilities and leisure spaces. The Local Plan sets out a vision statement for the Garden Village:
- 'To deliver a development of the highest quality which encompasses garden village principles within a characteristically wooded Surrey setting; a development that creates a highly sustainable mixed use community with a wide variety of housing types and where residents will be able to access on-site services and facilities to fulfil many of their daily needs. The village will provide unique local employment opportunities through the Longcross Park Enterprise Zone and other on-site retail, community and leisure facilities. Furthermore, the development will maximise opportunities to promote non-car modes of travel through enhancement of the Longcross station rail service, anew bus service, electric vehicle charging and new pedestrian and cycle infrastructure to ensure sustainable connectivity with existing towns and villages. In combination with other opportunities for the longer term community stewardship of assets, extensive areas of interconnected green infrastructure, food production, net gains in biodiversity, climate resilient design and sustainable energy measures, Longcross Garden Village will be a 21stCentury village community that reflects the best that Surrey has to offer'.*
- 8.2.3 The extent of the Longcross Garden Village and the allocated development site is defined at figure 2 of Policy SD9 of the Local Plan. This includes allocated SANGS comprising Chertsey Common located to the east of Kitsmead Lane. The first phase of which has been delivered to facilitate the existing residential development north of the M3 (Longcross North), which was consented following planning permission in 2014, and also includes an area of open space/SANG which has been delivered north of the M3 to the east of the residential. The wider Garden Village site area includes the land to the north of the M3 which has been developed providing 186 residential dwellings, with the employed site which lies within the Local Enterprise zone occupied by Netflix as film studios together with a data centre complex which is currently under construction. It is defined by the boundaries of the railway to the north, Kitsmead Lane to the east and Longcross Road to the south, with the western boundary adjoining the Borough boundary with Surrey Heath Borough Council. The existing residential properties located off Kitsmead Lane and Longcross Road including Albury Close, Holly Close and Tanglewood Close lie outside the allocated development site but within the wider Longcross Garden Village. The current planning application encompasses a large proportion but not all of the allocated development site south of the M3. It does not

include land to the far west of the site allocation, part of which lies within the 400m buffer zone to the Thames Basin Heaths SPA.

8.2.4 The Inspectors report to the Runnymede 2030 Local Plan, in considering the Longcross GV site allocation considered that:

'while the site is located on the western fringe of the borough, away from the main urban concentrations along the A320 corridor, much of it is previously developed land; and it provides a unique opportunity to meet large scale development needs in a high-quality village setting that will form an integral part of the sustainable development of Runnymede.'

The Local Plan was adopted in July 2020. The principle for the redevelopment of the site for a mixed use Garden Village including residential development is support by Local plan policies, specially Policy SD9 (Longcross Garden Village).

8.2.5 Whilst this application does not include the full extent of the allocated development site south of the M3, as it excludes land the area to the west of accommodation bridge which comprises area of woodland and far western edge of the former test track, as well as Longcross Barracks which is in separate ownership, the application seeks to deliver the Policy requirements of the Garden Village. Furthermore, the majority of the excluded area falls within the 400m zone of influence of the Thames Basin Heaths SPA, where the principle of new residential development is not supported as set out within Policy EE10. The site allocation include land to the east of Kitsmead Lane allocated for Publicly Accessible Open Space. This area is proposed on the parameter plans submitted to deliver formal sports and other green infrastructure including sports pitches, allotments, sports pavilion and associated access and parking etc. The principle of development has therefore been established through planning policy. This is considered in further detail within the report below.

8.3 **Quantum of development**

8.3.1 The site allocation within Policy SD9 of the Local Plan seeks to secure a minimum of 1,700 net additional dwellings and specialist accommodation as set out under bullet point (e) and would deliver a significant proportion of the identified housing provision within the Borough over the Local Plan period. The application description does not specify the quantum of the various land uses including Class C3 residential development for which permission is sought. However, the Environmental Statement submitted with the outline applications assesses the following quantum of development, and this is reflected in the illustrative masterplan submitted to support the application:

- Up to 1,700 dwellings (Use Classes C3) ;
- Up to 9,556 sqm of non-residential uses including:
 - Up to 1,000 sqm retail, food and drink (Use Class E and F2);
 - Up to 770 sqm for a public house (sui-generis use)
 - Up to 1,500 sqm flexible community facilities (Use Class E, F1 and F2)
 - Up to 300 sqm employment use (Use Class E)
 - A 2 FE Primary School) including early years provision (Use Class F1)
 - A 60 bed care home (3,700 sqm) (Use Class C2); and
- Public open space (to include allotments, sports provision and ancillary facilities) (Use Class F2), SANG at Chertsey Common, with associated landscaping and infrastructure works.

Although it is noted that there have been slight revisions during the course of the application in response to comments from consultees and the illustrative masterplan, supporting documents and ES updated where required to reflect changes made. The above quantum of development is consistent with the land uses set out within Policy SD9, and if considered to be acceptable, having regard to the planning considerations set out below, would be secured through planning conditions.

- 8.3.2 Policy SD9 seeks to deliver a minimum of 1,700 net additional dwellings and specialist accommodation comprising a mix of housing tenures and types. This includes 35% affordable housing, at least 10 serviced plots for Travelling Show People, provision of an extra care facility. Whilst this is a minimum provision, the application seeks to provide up to 1,700 new dwellings, it is also acknowledged that the site allocation includes the land to the north of the M3 which comprises 178 completed dwellings pursuant to an outline planning application approved in 2014 (and subsequently amended through s73 applications) for up to 200 dwellings, and associated reserved matters applications. Furthermore the current application does not extend to the full area of the site allocation south of the M3, although noting the majority of the land excluded from current application lies within 400m of the TBHSPA for which the principle of new residential development is resisted. The quantum of development proposed would align with Policy SD9 of the Local Plan.

8.4 **Indicative Masterplan and Proposed strategy for development**

- 8.4.1 Whilst this is an outline planning application, the application is supported by a number of documents and illustrative masterplan which illustrates how the Garden Village could come forward for development through the RMAs. The application has been informed by pre-application discussions which has included input from Design South East through Design Review Panels (DRP) and focused workshops prior to the submission of the planning application, which has evolved the illustrate layout and strategy documents which underpin how the site is envisaged to come forward.
- 8.4.2 A DRP was undertaken in November 2022 after the submission of the planning application. In recognising how the scheme has evolved, the panel commended the landscape-led approach and the strong landscape character that is retained as a framework for the scheme. This is seen as a positive of the proposal and reflected within the Green Infrastructure parameter plan and 'Landscape and Open Space document' submitted to support the application. The comments expressed by the DRP largely refer to details of design and layout reflected in the illustrate masterplan. The masterplan is illustrative at this stage and shows one way the site could come forward for development. The DRP also acknowledged aspects of the scheme requiring further work or clarity, including sustainability strategy, movement and parking to avoid car dependency and stewardship. It was recognised that work had progressed on design guidance and coding that will steer future detailed work, but suggested that this aspect needs to be further strengthened with priority given to coding for aspects of the design that are most important to the place's character.
- 8.4.3 These aspects are considered further within the associated topic headings within the report below, but recognising this is an outline planning application seeking to establish the principle of the quantum of the development proposed. It is considered that these detailed aspects of design can be secured through conditions and/or considered

through detailed RMA's. Notwithstanding this, amendments have been made to the illustrate masterplan layout which have had regard to the comments of the DRP, and in particular with refence to the layout of the school and village centre and neighbouring access routes. This helps to support the ability to achieve a high quality garden village which responds to policy requirements and achieves the quantum of development proposed.

8.5 **Proposed Land Uses and Development Parameters for approval**

8.5.1 The following Parameter plans are submitted for approval:

- Land use
- Green Infrastructure
- Access and movement
- Building Demolition

The parameter plans are intended to fix certain elements or parameters of the development that will inform how future reserve matters come forward. These form the plans which are to be considered for approval under this application. All other plans and supporting documents help to demonstrate and inform how the quantum of development proposed can be design and built out having regard to policy requirements.

8.5.2 Land Use Parameter Plan

The land use plan identifies the distribution of land use across the site. It defines the maximum extent of the area within which the built development (buildings) can be delivered. It defines the areas forming the green and blue infrastructure (which includes public open space, SANG, play spaces, ecological areas, retained landscape features, community food production, drainage infrastructure and pedestrian and cycle links, and access). An acoustic barrier is proposed alongside the M3 motorway with a maximum height of 8m. An area to the west of the site is proposed to deliver the mixed use village centre, which will include Class E, F2, C2, C3 and sui-generis uses which includes commercial, retail and community uses as well as a primary school and early years provision. The plan illustrates a potential location for a proposed public house towards the south-east of the site. An area is illustrated as an indicative location for Travelling Showperson plots to the north-east corner of the site with potential access point onto Kitsmead Lane. The location of 5 vehicular access points forming part of this detailed consideration are illustrated together with the existing access points to the north and south of the site. Barrowhills House, a Grade 2 listed building and its immediate grounds are to be retained as existing with no development proposed. The parcel of land to the east of Kitsmead Lane is defined on the land use parameter plan for 'formal sports and other green infrastructure'. This includes sport pitches, allotments, pavilion, access, parking, pedestrian and cycle links, pumping station, drainage and associated infrastructure.

8.5.3 The location of the defined land uses is considered appropriate, and is informed principally by the retention of important tree belts and woodland areas, the site topography, scheduled monument and listed building.

8.5.4 Green Infrastructure Parameter Plan

This plan illustrates the land which will form the core green and blue infrastructure and includes: retained landscape features, ecological areas, suitable alternative natural green Space (SANG), play spaces, public open space, community food production, access, acoustic barrier, drainage infrastructure and pedestrian and cycle links. This does not preclude elements of green space within the development areas but defines the extent of the area which is reserved solely for the uses listed above (ie excludes built development). As such the detailed design of the 'development' areas would also include soft landscaping, open space and play spaces equipped play and sports pitches as reflected on the plan.

8.5.5 The plan also defines the area of land forming the proposed on-site SANG. This would be centrally located within the site centred around Barrow Hills house and incorporating the existing Scheduled Ancient Monument which forms the highest point of the site and adjacent multi gradient vehicle drop (MGVD) which is proposed to be landscaped and form a recreational feature within the SANG. It also includes areas of existing woodland and the open land south of Barrowhills formerly used by the military as a golf course. The SANG would form a link to Kitsmead Lane and adjacent Chertsey Common SANG. The area to form SANG will be secured by legal agreement. Areas of green infrastructure are illustrated around the perimeter road boundaries of the site which reflect existing tree coverage, and TPO trees along Kitsmead Lane and Longcross Road frontage. The plan also illustrates green corridors alongside main internal roads linking green spaces within the site and providing opportunities for green connections through the site. The plan also illustrates the provision of play space including NEAP, LEAP and LAPs (Neighbourhood Equipped Area for Play) (Local Equipped Area for Play) and (Local Area for Play) distributed throughout the site within both the development and green infrastructure areas. Sports pitches are illustrated both to the west of the site which will form part of the school grounds and on the land to the east of Kitsmead Lane. The Plan indicates an area for allotments and community gardens/orchards to the east of kitsmead Lane and to the east of the site adjacent to the village centre and school. It is expected that appropriate provision would be secured throughout the development reflective of the food production strategy and detailed through the reserved matters.

8.5.6 This reflects the proposed Landscape and Open Space strategy for the site which is underpinned by a site wide 'green grid'. This has regard to existing landscape features to be retained including existing vegetation, landform, heritage and ecology. This provides a central green space at the heart of the village, a network of connected open spaces including play areas, allotments, parks and sports facilities. This provides ease of access to open spaces for existing and future residents, enabling promotion of cycling and walking through the green grid and supporting health and wellbeing, as well as supporting connectivity of habitats.

8.5.7 Access and Movement parameter plan

The plan identifies the primary road network proposed within the site and connecting to the existing and proposed access points with the adjacent highway (Longcross Road, Kitsmead Lane and Accommodation bridge). This forms an internal loop within the site similar to the alignment and taking reference to the existing test track and illustrates how the proposed access points proposed to serve the development would connect to the internal road network proposed. It would also form a primary route which can be served by an internal hopper bus service with links to the railway station (refer to public transport section of report). Indicative pedestrian routes links are illustrated primarily around the SANG and green space and alongside the village centre and school to the

west.

8.5.8 Building Demolition parameter plan

There are large areas of existing hardstanding including roads and the former 'test track' which forms a loop around the perimeter of the site, in addition to various buildings relating to the sites former use. The demolition parameter plan illustrates all existing road and hardstanding to be removed with the exception of the internal loop road which extends through the existing woodland around Barrowhills which would be for further technical assessment, the majority of which would lie within the proposed SANG area. The plan also illustrates all existing buildings and structures to be removed. The exception being the Grade 2 Listed Barrowhill house and the Multi gradient vehicle drop (MGVD) which is to be retained and landscaped. The existing buildings, structures and features are not listed but would be viewed as non-heritage assets collectively having regard to their former military use. This is considered more fully within section 8.26 Heritage below.

8.5.9 Building Height

In addition, a Building Height Parameter Plan has been submitted to support the application but is agreed with the applicant that this is not to form an approved plan. The Building Height parameter plan helps inform the Environmental Statement and supporting documents to demonstrate that development up to the maximum height illustrated would be acceptable and would not result in significant harmful impacts. The plan illustrates a proposed height to up to 3 storey (maximum 13m) or up to 4 storey (maximum 17m) across the development area. It is acknowledged that this provides maximum height parameters. A detailed scheme may be designed to provide some buildings up to the maximum parameters but it will be expected that to ensure appropriate character throughout the site that there will be an appropriate variety of height within each character area and phase of development, with not all of the areas shown to be suitable for development up to the maximum heights indicated and this will need to be informed by detailed design at RMA stage and informed by Design principles and codes for the associated character areas and having regard to adjacent land use. Therefore, whilst this plan is helpful to assist in assessing the potential impact of the development having regard to absolute maximum heights specified, it is not intended to form a plan for approval. It is not to be approved as it is not considered that development covering the whole or much of the site to maximum parameters would create an attractive or high quality development and as such it is considered appropriate that height is considered on an individual basis as part of RM applications.

8.5.10 Maximum heights and variation in height is indicated within the DAS, and a Design Principles and Codes is to be secured by condition for the character area will provide greater certainty and detail of expectation of design including height, layout and materials within each area or phase of development to inform reserved matters applications.

8.6 **Phasing and Sequencing**

8.6.1 The developer has indicated a proposed build out period of approximately 10-12 years, with the development coming forward in phased areas. An indicative phasing plan has been submitted which illustrates phased areas but does not prescribe the associated sequencing. Notwithstanding this plan, officers consider it will be important to secure details of phasing through conditions and the S106 legal agreement. It is also

necessary through the S106 or conditions (to be set or negotiated by the HoP), the sequencing and delivery of key components of the scheme within an appropriate timeframe, including internal access road linking the M3 road bridge and Longcross Road, Travelling Show person plots and key infrastructure. This will be important to secure appropriate and sustainable access during the phased build out of the development.

8.7 **Scale/Density, Design and layout**

8.7.1 The application is accompanied by a Design and Access Statement (DAS) which considers the principles which underpin the illustrative masterplan including differing neighbourhoods/ character areas within the site and suggested design codes which would help inform how the layout and built form would evolve through RMAs. Whilst detailed design and layout are not under consideration at this stage the DAS helps to set out how the development responds to the site's context. It helps to illustrate how the quantum of development can be accommodated within the site whilst having regard to the garden village principles as reflected within site allocation SD9 of the Local Plan. Careful consideration has been given to develop several distinct character areas/neighbourhoods reflecting the location within the site with differing housing typologies and density. As part of the Local Plan site allocation, it was recognised that densities of around 30-50 dph are expected to be achieved within the sub areas of the village. Having regard to the size of the size and scale of development proposed it is expected that densities will vary across the site and this will be important in securing the most efficient use of the site whilst achieving high quality design and placemaking. This is reflected within the character/neighbourhood areas referenced within the DAS which reflect densities ranging from 25 to over 45dph, which will help variety of character within the village whilst accommodating the proposed quantum of dwelling (1700 dwelling, care home and travelling showpeople plots). Higher density is proposed within the village centre to the west of the site and along the northern boundary with lower density towards the south-east. This includes the following:

'Heart of Longcross' – the neighbourhood around the village centre which includes the primary school, extra-care home, shops, employment and community spaces. This includes varied building types and character at higher density located around a pedestrian village centre/high street and a focal village green.

'The Avenue' – higher density development along the northern boundary with the M3, predominantly apartments with some townhouses and semi-detached houses.

'Central Longcross' – located in the centre of the site and surrounded by mature trees, and provides links with village centre and primary school. Medium density proposed with range of dwelling types.

'Upper Kitsmead' – located in the north eastern corner, forms a transition between on-site SANG to the south and the M3 motorway to the north with high density to the west and includes a mix of density.

'Barrow Hills Green' – located to the south and east of the site, this character area adjoins and character is influenced by the relationship with the on-site SANG and parkland setting around Barrows Hill House. Proposed at a generally lower density and informal character forming a gateway into the Garden Village from the south-east.

It is expected that the development will include a range of heights within each neighbourhood area providing variety, whilst the tallest heights are proposed along the northern boundary with the motorway (up to 4 stories), with generally lower height along

the southern and eastern boundaries (generally 2 to 2.5 stories) where the dwellings sit within more woodland setting and adjacent to existing settlement of Longcross.

8.7.2 Further design guidance for each of the five proposed neighbourhood areas is provided within Part C of the DAS, providing a framework for development details at RMAs. Whilst the general principles set out within the document demonstrates the ability to deliver a development of high quality which encompasses garden village principles, it is considered that further work is required ensuring greater certainty and commitment of detail through Design codes which can be secured through condition which would fix the details which would inform future RMAs. The illustrative masterplan has been developed over a number of years with significant input from various stakeholders, including community engagement organised by the applicant and with external design advice. It represents one way in which the development could come forward in accordance with the parameter plans. The design principles and frameworks set out within the DAS and supporting documents, help to demonstrate how the quantum of development could come forward in delivering a Garden Village of high quality and in compliance with planning policies.

8.8 **Environmental Impact Assessment (EIA)**

8.8.1 The proposed development falls within the category 'Urban Development Projects' (Schedule 2, 10 (b)) of the Town and Country Planning Environmental Impact Assessment) (England and Wales) Regulations (2017) (as amended). Having regard to the scale of the development, a Scoping Opinion was submitted on 9th May 2017 under the 2011 regulations, and decision issued on 27th September 2017. The 2017 scoping opinion request was considered under the preceding 2011 EIA Regulations as being the relevant regulations at the time. It described the development as comprising an outline planning application for up to 1,400 dwellings, a primary school, 3,210 sq m of commercial space (restaurants, retail, public house), 930 sq m of community space, publicly accessible open space, landscaping, ecological habitats and access plus Suitable Alternative Natural Greenspace (SANG) would be provided on site and on Trumps Farm. Construction was anticipated to take 12 years. Overall, the Council remains satisfied that the development proposed in the current planning application is broadly similar in character and quantum to that forming the basis of the September 2017 Scoping Opinion and as such the format of the ES remains acceptable. The Environmental Statement was prepared in accordance with the 2011 EIA Regulations due to the date of the EIA Scoping Opinion. However, following comments from the Council consultants additional information has been provided to reflect the requirements of the 2017 regulations having regard to the passage of time between submission of the scoping opinion and submission of the planning application.

8.8.2 As set out within the governments EIA Guidance, 'the aim of Environmental Impact Assessment is to protect the environment by ensuring that a local planning authority when deciding whether to grant planning permission for a project, which is likely to have significant effects on the environment, does so in the full knowledge of the likely significant effects, and takes this into account in the decision making process.' It further advises that EIA should not be a barrier to growth.

8.8.3 The submitted Environmental Statement considers the 'likely significant effects' of the proposed development on the environment having regard to the EIA regulations. Development parameter plans detail the maximum limits, or controls, necessary to define and fix key aspects of the Development. This will ensure that any detailed proposals coming forward as Reserved Matters applications in the future under the

outline permission will lead to equal or lesser effects on the environment. This includes the location and types of land use (including maximum and minimum amounts), and the maximum heights of development.

- 8.8.4 The LPA has secured an independent review of the Environmental Statement. This initially raised a number of matters which sought further clarification. The applicants have provided updated information and points of clarification in response in the format of Environmental Statement Technical Addendum(s). Specifically in response to remaining clarification and potential Regulation 25 requests. The March 2023 ES Addendum provided a response to comments raised during a review of the ES and addressed minor amendments to the submitted parameter plans; as well as including the provision of a Climate Change assessment as an additional chapter to the ES. The March 2023 ES Addendum did not change any of the conclusions or findings of the December 2021 ES. Appendix A of the January 2024 ES Addendum has sought to address the request for further clarification raised by the Councils consultants Temple Group in September 2023 and additional points of clarification have been received in respect of air quality.

The final response received acknowledges that all responses are considered to be resolved and that with the mitigation measures proposed this would limit or remove any significant adverse environmental effects resulting from the proposed development. It is considered that the Environmental Statement together with the additional information submitted (including ES Addendum) complies with the above Regulations and provides sufficient information to assess the environmental impact of the proposed development.

8.9 **Socio-economic effects/benefits**

- 8.9.1 The use of the site for film studio purposes has intensified more recently and temporary planning permission was recently granted to regularise the current use and buildings on the site for a temporary period. A temporary permission was considered necessary in order not to prejudice the delivery of the Longcross Garden Village, given that the site forms the major site allocation for the delivery of residential dwellings within the Borough. Employment associated with the existing film studio use fluctuates, and this primarily relates to individual production companies utilising the site at any given time. As such the discontinuance of existing film studio uses on the site would not result in the loss of permanent employment. The use of LXS for film studio purposes was only ever intended to be an interim use. Furthermore, the use of the site is separate from and does not impact on the occupation of the employment land to the north of the M3 which is currently in film studio use occupied by Netflix. The construction of the development is predicted to create an average of 352 FTE jobs on site together with a further 341 jobs associated with the supply chain linkages. This would create some benefits on employment over construction period.
- 8.9.2 Following completion, the provision of up to 1,700 new homes will provide approximately 23% of the RBC area's housing target according to the adopted RBC Local Plan. This is a material consideration which is afforded very significant weight. The mixture of accommodation types and sizes including affordable housing provision would provide a significant benefit towards meeting the housing needs of the Borough. As well as the provision of new homes the application also includes a number of supporting uses including a new two-form entry primary school, a health centre/GP surgery, care home, local shops, cafes and restaurants, and community uses/buildings providing jobs and community needs. Whilst a secondary school cannot be accommodated on site, the relatively minor adverse impact resulting will be mitigated

through financial contribution to support off site provision.

8.10 Highways and Movement

8.10.1 On-site Site Access Junction Details

The application includes the full consideration of access in respect of the provision of 5 vehicular access points to serve the proposed development. This comprises the provision of 2 new access points from the B386 Longcross Road to the south, retention and upgrade of the existing vehicular access south of Barrowhills House, and 2 new access roads to the east of the site from the western side of Kitsmead Lane together with one off the eastern side of Kitsmead Lane to form access to the sports pitches/allotments on the east side of Kitsmead Lane. In addition, a further access is shown to the north of Kitsmead Lane to the proposed Travelling Showperson plots. This point of access is illustrated indicatively and therefore remains in outline with details to be determined at RMA.

8.10.2 Minor revisions have been made to the access points incorporating further detail as a result of discussions with Surrey County Highway Authority (CHA) and following Stage 1 Road Safety Audit (RSA). This will include adjustments to Longcross Road and Kitsmead Lane including associated road marking to accommodate the new and retained accesses into the site to serve the proposed development. These are illustrated within the plans submitted. The CHA are satisfied with the proposed site access arrangements, and have raised no objections in this respect. It is also noted that speed limits along the B386 Longcross Road and Kitsmead Lane have been reduced from 60 to 40 miles per hour since the application was first submitted.

8.10.3 The proposed access to serve the Travelling showpeople plots is illustrated indicatively, and therefore full details have not been provided at this stage. Details of the provision and layout of the Travelling showpeople plots will be secured at an early stage to ensure the siting including access arrangements can be suitably accommodated within the site. This may necessitate changes to the siting as shown on the illustrate layout if required to ensure suitable access and provision for the use proposed. The 6 points of vehicular access proposed to serve the garden village development enables a good dispersal of traffic across the highway network and also helps facilitate pedestrian and cycle links through the site and linking to the surrounding area. The application includes offsite highway works to improve wider pedestrian and cycle connectivity. The existing access over the M3 Accommodation bridge to the northern site is currently intended to provide a sustainable link which prohibits general vehicular traffic. This is described further below.

8.10.4 Given the existing tree belt which extends along both Longcross Road and Kitsmead Lane and is covered by a Tree Preservation Order, it is acknowledged that there will be some loss of trees to facilitate the development including the proposed points of access. Tree loss is considered more fully at 8.18 which acknowledges that the loss will relate to a small proportion of the overall road frontage.

8.10.5 Off-site highway works

The application is supported by a Transport Assessment (TA) which assesses the impact of the proposed development on the surrounding highway network. The methodology has been agreed with Surrey County Highways Authority and utilises the outcomes of traffic surveys, whilst taking account of growth rates and vehicular trip generation estimates.

The Transport Assessment has identified a number of off-site junctions where mitigation measures are recommended to off-set the impact of the proposed development. This includes measures to improve junction capacity and therefore to accommodate for the predicted increase in traffic movements. The schemes have been revised to reflect the advice of Surrey CHA and results of Stage 1 Road Safety Audits. It is recognised further amendments may be required to account for the detailed design as part of the highway approval process. The applicant has engaged in direct discussions with Surrey CHA both through pre-application and during the course of the application to ensure the resulting off-site highway works proposed afford suitable mitigation to ensure appropriate capacity, and the proposed development does not lead to severe adverse impact on the highway network. It is intended that the off-site mitigation measures will be delivered through a mix of s278 agreements and secured through S106.

The off-site junction improvement measures include the following junctions:

- Longcross Road/Accommodation Road
- Trumps Green/Wellington Avenue
- Trumps Green Road/Lyne Road Underbridge
- Chobham Lane/Kitsmead Lane (signals)
- Stonehill Road/Holloway Hill/Longcross Road (signals)

Surrey CHA is also seeking to secure through a Traffic Regulation Order to be funded by the Developer, an extension of the existing 30mph speed limit on Trumps Green Road supported by a scheme of traffic calming measures along Trumps Green Road between Lyne Road and Kitsmead Lane. Plans also include improved cycle and footways to improve wider connectivity for pedestrians and cyclists.

Whilst the TA identified 8 off site locations requiring junction capacity mitigation measures in addition to the A320 improvement scheme, the CHA has agreed that 3 of the junctions identified do not need mitigating, this relates to:

- Chobham Road/Chertsey Road/Windsor Road roundabout
- Chertsey Road/Chobham Lane/Longcross Road Roundabout
- Accommodation Road/Stonehill Road Priority Junction

The CHA advice is given on the basis that the measures to support a new 30mph speed limit on Trumps Green Road is of greater benefit to pedestrians and cyclists and will support the sustainable transport aims of Surrey County Council and Longcross Garden Village, providing equivalent and enhanced mitigation in lieu of the proposed minor junction capacity improvements identified.

8.10.6 Subject to a package of highway works and improvements secured by legal agreement, Surrey County Highways Authority are satisfied that the local highway network and access could safely accommodate the extra traffic.

8.10.7 A320 works

Policy SD9 (e) seeks to secure the delivery of on and off-site improvements to the local road network to mitigate significant impacts and contributions towards or the delivery of improvements to the A320. This is informed by the Infrastructure Delivery Plan (IDP) and Transport Assessment. Longcross Garden Village was identified through the Local Plan process as one of the sites dependant on the A320 improvement works. A

proportionate contribution towards the A320 HiF scheme is to be secured through a legal agreement, based on a rate per square metre floor area as reflected within the Infrastructure and Delivery Prioritisation SPD. Given the scale of development, this is expected to be circa £22m, which would be a significant proportion of the overall HiF funding. The TA also considered the impact of the development on the A320, which establishes that the development alongside other strategic locations is influenced by the mitigation improvements specifically at Junction 6a/6b (Holloway Hill/Guildford Road/Green Lane). Surrey CHA recommend a restriction on the number of properties to be occupied (250 dwellings) in advance of the completion of the associated A320 works and will be reflected within the s106 or through condition. Having regard to the timescales for the A320 works it is unlikely that housing delivery would be delayed as a result.

8.11 **Movement and Sustainable Transport**

8.11.1 Policy SD3 (Active and Sustainable Travel) supports developments which enhance accessibility and connectivity between people and places by active and sustainable forms of travel. This includes 'sites allocated (within the local plan) which integrate with or provide new accessible, safe and attractive active and sustainable travel networks and routes to service and employment centres and rail interchanges.' This is reflected further within Policy SD9 (Longcross Garden Village). The application is accompanied by a Transport Assessment which assess the impact of the development on the surrounding transport environment including road network and includes a Framework Travel Plan.

8.11.2 The site is currently in private ownership with no public access. The existing vehicular access into the site from Longcross Road would be retained and the junction modified to satisfy highway safety and capacity requirements. The existing internal road bridge (Accommodation bridge) which links the northern and southern site over the M3 would provide a direct link to the northern site as is discussed further below.

8.11.3 M3 Accommodation Bridge

The existing M3 Accommodation bridge provides a key point of access to link the northern and southern site including securing a direct route of access to Longcross railway station. This is important to link the wider Garden Village allocation, providing accesses between the various residential, employment, community uses and transport network both north and south of the bridge and linking with the wider community beyond the application site boundary. The bridge is therefore seen as an important part to delivering a sustainable Garden village. It is proposed that the bridge would be for non-car traffic modes, i.e. for buses, pedestrians and cyclists, promoting sustainable transport modes. This is to be secured through legal agreement and condition including associated improvements to the bridge to facilitate dual use and secure a safe and attractive link which encourages pedestrian and cycle use. It is considered that for the sustainability of the site the link to the bridge should be secured at a relatively early point in phasing.

8.11.4 Internal access network

The access and movement parameter plan illustrates the primary internal road network and this is further reflected within the illustrative masterplan. This illustrates a hierarchy of roads with the main avenues which are designed to accommodate buses, refuse and

emergency vehicles and provide structure for the formal pedestrian and cycle routes, through to secondary streets, residential streets and shared surfaced streets. The detailed design and layout will be considered through RMAs.

8.11.5 Supporting documents and illustrative masterplan illustrate a 'green grid' network providing separate routes through the site for pedestrians and cyclists and linking areas of open space, as well as a network of pedestrian footways and cycleways along primary roads. The pedestrian and cycle network is designed to accommodate both direct commuting routes through the site, including links to the local centres and railway station, as well as leisure routes. The Travel Plan illustrates that all of the site is within a 25 minute walking distance of the proposed local centre with the majority of the site within 25 minute walk to the railway station. Cycle routes and public transport provision would provide alternative access and support those in the far south eastern part of the site.

8.11.6 County Highways Authority have provided some initial comments on the proposed masterplan layout with reference to Surrey's Healthy Streets Design Guide which provides advice to inform the more detailed design. The application has been submitted in outline and therefore this advice can be considered through the associated detailed RMAs.

8.11.7 Wider pedestrian and cycle connectivity

A new 3m wide shared cycle and footway alongside the western edge of Kitsmead Lane is proposed which will better connect the development to Trumps Green and Virginia Water as well as providing added wider accessibility benefits for those visiting or passing through the site. This includes improving provision for pedestrians and cyclists in conjunction with the junction changes proposed at Chobham Lane/Kitsmead Lane and to the south of Trumps Green Road together with traffic calming measures to Trumps Green Road which will make this route more attractive for pedestrians and cyclists and easier to access neighbouring Virginia Water. Crossing points are proposed along Kitsmead Lane to facilitate access to Chertsey Common SANG, the sports pitches and allotments. This will include informal crossing points with refuge islands and a signalised crossing. A footpath is to be added alongside the old Kitsmead Road (close to Chertsey Common SANG), to provide a footway connection to the Accommodation Road/Longcross junction (Old School café).

8.11.8 The Travel Plan includes additional measures to support and encourage sustainable travel and to promote and facilitate cycling. This includes cycle parking facilities, including at the railway station, transport/mobility hubs with connection points for bicycle hire scheme between the local centre and Longcross railway station, provision for e-bikes and e-scooters, and signage through the new development.

8.11.9 Paragraphs 100 and 174 of the NPPF highlight the importance of public rights of way and access. There are no existing public rights of way (PROW) through the site, and therefore the redevelopment of the site will enable wider access and connectivity for the existing community of Longcross and beyond. This not only helps support access and use of the existing railway station but will also afford access to the proposed village centre, community facilities, school and open spaces proposed as part of the development. The application presents opportunities to link with existing off site PROW, and the details of the layout of footpaths through the site would be considered through

RMA. Notwithstanding this, in order to secure the necessary SANG mitigation pedestrian routes are proposed through the on site SANG and green corridors to the crossing points along Kitsmead Lane and the off-site SANG of Chertsey Common and public footpath 45 (FP45) to the east of the site. This will therefore link the proposed on site green spaces and SANG to those off site.

8.11.10 The County Access Officer has also identified the opportunities to also link FP45 with footpath (FP47) or bridleway (BW66) to the south of Longcross Road and extend into Chobham Common. Whilst it would be desirable to continue these public access routes through the garden village, in order to provide wider connectivity, this would however be contrary to the advice of Natural England. Natural England discourages improved connectivity to the TBH SPA from within the Longcross site as this would undermine the purpose of the SANG. Therefore, the illustrative masterplan demonstrates connectivity through the site, particularly linking the onsite SANG and areas of open space with green routes eastwards to connect to the Chertsey Common SANG and existing PROW network to the east, and with the exception of pedestrian access links to the south. The purpose of SANGs are to provide alternative recreational areas to move visitors away from SPA sites to safeguard the protected bird species and to reduce recreational pressure.

8.11.12 Sustainable Travel measures

Policy SD9 (Longcross Garden Village) requires the delivery of 'A range of sustainable transport choices which facilitate connections within the village and to other nearby settlements and which maximise opportunities for modal shift by optimising connectivity within the site by walking/cycling with improvements to the local road network to mitigate significant impacts'. The application is supported by a Framework Travel Plan which proposes a range of sustainable travel measures. The site will include EV charging points as confirmed within the Energy Statement to ensure compliance with requirements of Policy SD7 and SCC Parking Standards SPD. Although details will be secured through detailed reserved matters stage together with details of car parking provision. Surrey CHA have recommended a number of conditions should planning permission be granted in addition to public transport provision (rail and bus – discussed further below), which include a Travel Plan, sustainable travel vouchers to promote cycle and bus use, car club provision, pedestrian and cycle links over the M3 bridge and off site connections.

8.12 **Public Transport Strategy**

8.12.1 Policy SD9 (Longcross Garden Village), in seeking to securing sustainable transport choices includes the provision of 'funding towards provision of permanent bus services for the village which link with Longcross Railway Station and neighbouring settlements including Woking, as the nearest major local service centre'. The application is supported by a Public Transport Strategy which has been discussed at length with Surrey CHA. This includes rail and bus improvements in services and infrastructure, through a package of financial support to be secured through S106 agreement.

8.12.2 Rail Service

Longcross rail station is located to the north of the site on the northern side of the M3, with the majority of the new development predicted to be within 25-minute walking distance of the station. As such the location of the rail station is of significant benefit to the Garden Village and plays an important role in securing a sustainable Garden Village. The station currently benefits from a half hourly service on the Reading –

London Waterloo mainline, with local destinations including Virginia Water, Sunningdale, Egham and Staines. Connectivity to, access and service provision is therefore important to maximise use of the railway over private vehicles. Policy SD9 seeks to secure 'permanent upgrades to Longcross Station and working with partners and Network Rail; delivery of an increased stopping service on the Reading to Waterloo rail link'.

- 8.12.3 The planning consent for the land north of the M3 (Longcross North) secured contributions under the S106 legal agreement towards improvements to the station environment including station waiting area and platforms, in addition to financial support for an increased stopping service to secure the current half hourly service during the day. Longcross railway station is currently served by half-hourly services on Monday–Saturday, between 06:00-21:00, approximately. However, with timetables regularly reviewed there can be no guarantee of the long term provision of service level, as is the case with any rail service provision and this is generally linked with passenger use. The provision of up to 1700 new dwellings provides the opportunity to support passenger numbers utilising Longcross station and thereby help support the existing level of service and proposed increase to the hours of the half hourly service.
- 8.12.4 A reserved matters application was recently allowed on appeal which secured vehicular access, drop off and car parking to Longcross Railway Station. This permission ensures that sustainable and direct access can be secured from the site north across Accommodation Bridge to Longcross railway station, enabling direct access between the southern site and the Station for buses, cyclists and pedestrians. It also facilitates land at the station entrance for cycle parking and provision for e-bikes and scooters which will be secured through this planning application.
- 8.12.5 The current application proposes a contribution towards an increased stopping service, to assist with maintaining the existing half hourly service rail service and support for extending this earlier and later into the day, and this would be secured through legal agreement. Whilst there is no guarantee of service provision as this is reliant on the rail operators, the contribution will cover the predicted additional costs required to provide the increased service provision over a 10 year period, assuming increased patronage relative to buildout.
- 8.12.6 Both Network Rail and South Western Railway have reviewed the application and the contributions proposed to support rail services and infrastructure reflect comments received. The application also proposes financial contributions for station improvements. In order to promote inclusive travel a contribution towards the provision of 'step free access' across the platforms through the provision of lifts would be secured through S106. The provision of £1m towards a fully compliant pedestrian bridge and lift which is estimated to be in region of £3.5 - £4m in total is considered proportionate to the development proposed. In addition, the application proposes a contribution to secure Touch Screen Displays at the station which will provide a range of onward travel information. Network Rail have also requested that best endeavours are undertaken to provide toilet facilities at Longcross Rail Station. It is recognised that this will be impacted by land ownership, the availability and feasibility of utilities (namely water and drainage connections). A shuttle bus service is also proposed linking all parts of Longcross South with the railway station.
- 8.12.7 Bus Services

In addition to supporting upgrades to the railway station and towards supporting maintaining and enhancing frequency of service, contributions also proposed to support bus service provision. It is acknowledged that whilst the rail provision is a huge asset at Longcross it is also recognised that bus service links are also crucial to supplementing the rail service to link to local towns/facilities not served by rail and assist in connections between the site and rail station. Policy SD9 seeks to “Provide funding towards provision of permanent bus services for the village which link with Longcross Railway Station and neighbouring settlements including Woking, as the nearest major local service centre;”

- 8.12.8 The minimum level of service has been agreed between the applicant and CHA. Following substantial build out it will provide a 2 bus DRT Service, plus a separate north/south timetabled service connecting with Woking and Chobham plus a timetabled east/west service connecting with St Peter’s Hospital, Addlestone and Weybridge Business Park. The intention is to extend the existing bus services of the 73 and 461 service to serve the new development at Longcross. Also proposed is a DRT-based service to provide flexibility to cover core area serving immediate destinations around the Garden Village and serving broader destinations during evenings and weekends. The principle of providing an (up to) 3 bus DRT Service in the early stages of the development prior to substantial build out is also accepted. The applicant proposes funding to secure the extension of these existing services. In addition, a contribution is also proposed to secure upgrades to bus stops off-site along the routes served by the buses or the DRT.
- 8.12.9 However, whilst the level of service provision is generally agreed, there is currently a disagreement between the applicant and CHA on the amount of financial support required. This relates in part to the financial scenarios used in calculating the financial support required, i.e. whether an additional bus needs to be included to maintain the half hour level of service proposed on the 461 service which impacts on associated costs. The other key area of disagreement relates to the level of funding considered appropriate to secure bus service provision, in the scenario that existing services are not continued, thereby supporting a part replacement 73 and 461 service. Surrey CHA have concerns in respect of the lack of certainty over the future of these services as there is no guarantee that current local bus services will continue in the same format in the future. Therefore, the CHA are seeking to ensure that they are not unnecessarily burdened with funding these bus services. The CHA are seeking a funding mechanism which would secure sufficient future funding to operate bus services (includes a part 73 and 461 service), should the existing services not continue.
- 8.12.10 The CHA are currently raising an objection to the planning application as they do not consider that the proposed bus service provision, as part of the Public Transport Strategy, is robust enough to support the proposed development in the long term. They have concerns over the predicted costs of the bus services, predicted patronage from the development, the financial support throughout the build out and ongoing financial support and lack of a Transport Review Group with supporting Community Trust (or similar mechanism for generating finances), for ongoing support of service provision following commencement of development. The applicant has sought a legal opinion which concludes “*Funding towards a permanent bus service by extending the existing provision is in line with what policy requirement; securing the theoretical entirety of the in perpetuity costs of bus provision service, on the contingency (however remote) of existing services being withdrawn, is expressly not.*” They also make reference to the inspectors comments under the main modifications to the Local Plan which seeks to clarify that “bus links with Woking as the nearest major service centre will be provided, but that the developer is not expected to fund bus services in perpetuity.” The applicant

is continuing to liaise with the CHA to agree an appropriate public transport funding package which would be secured through a S106 agreement.

8.13 Highways Summary/conclusion

8.13.1 The County Highway Authority have only raised an objection to the application based purely on the Public Transport Strategy, and specifically in relation to the long-term support for the bus services. Their response is clear that the minimum level of service provision has been agreed with the applicant and consists of a half hourly service on all routes. It is the amount of the financial contributions to support the long term provision of the additional services, (providing for a replacement service in the scenario that the existing services were not to continue) that is in dispute. Surrey CHA have raised concerns regarding the uncertainty over future services and consider a 'worse case' scenario should be adopted. The Public Transport Strategy and associated level of financial support will be secured through the S106. Whilst officers acknowledge the views of the CHA and the importance of securing and maintain an appropriate level of bus service provision, subject to this being agreed through the S106 the principle of the proposed development is not in question.

8.13.2 The sole objection of the County Highway Authority relates to the long-term funding of the Public Transport Strategy (PTS). No objections have been raised to the proposed vehicular accesses serving the site (located off Kitsmead Lane and Longcross Road) for which planning permission is sought in full. Furthermore, no objections are raised to the impact of the development on the wider highway network subject to mitigation measures on key junctions and improved cycle and pedestrians links. The application proposes a range of sustainable travel measures which is consistent with Policy SD9 and SD3 of the Local Plan. The County Highway Authority have provided a fall back position advising of conditions and planning obligations should the LPA be minded to recommend the application favourably, and having regard to other matters which weigh in favour of the scheme which outweigh the deficiencies identified by the CHA to the PTS.

8.14 Landscape and Visual Effects

8.14.1 Paragraph 174 of the National Planning Policy Framework (NPPF) highlights the need to protect and enhance valued landscapes through the planning system. A Landscape and Visual Impact Assessment (LVIA) has been submitted to support the application and is contained within the Environmental Statement. It considers the impact of the proposed development on the surrounding landscape including views from Chobham Common. The site is relative well contained as a result of its topography and with good tree coverage around the sites boundaries and within the site. The perimeter tree belts form an important part of the landscape character both in the context of the site and to neighbouring roads, and helps screen the majority of the site. Much of the boundary screening would remain. The central high point of the site comprises the scheduled monument Bowl Barrow which is treed along with surrounding land which is to form part of the SANG and would be free from development, and the adjacent Barrows Hill House.

8.14.2 The LVIA considers the landscape and visual impact of the proposed development including during construction together with mitigation measures and the completed development. It recognises that whilst there would be some views of the development from outside the site including from neighbouring residential properties, it is recognised that tree belts and peripheral woodland belts would provide a visual screen. Views from the public footpaths within Chobham Common are limited to the tops of a cluster of

trees. During construction mitigation measures include tree protection, controls on construction lighting and appropriately located temporary hoarding. The assessment through the ES concludes that the development heights have been designed to mitigate significant adverse landscape and visual impacts from the surrounding area, with the majority of the existing tree belts being retained. It is considered that the Development would have little effect on views from Chobham Common and any glimpses of the Development would be very minor in the context of the overall view.

- 8.14.3 The Building Heights parameter plan indicates the higher (up to 4 storey) buildings would be located primarily on the lower land levels of site adjacent to the M3 motorway. The remaining site would have absolute maximum building heights of 13m, however in reality it is expected that most development would be significantly below this level for character and design reasons. The height of development would be informed through detailed design with expectation of variation in heights. The stated maximum heights with the supporting building height plan provided are useful for assessing the potential maximum impacts of the development from a landscape perspective (rather than a design and character perspective). Currently there are limited or partial glimpsed views through existing tree belts into the site. This would largely be maintained with increased views at access points, and greater visual impact from the M3 motorway as discussed further below. Whilst retained trees will ensure existing character is not undermined it is also important in establishing good placemaking to ensure the development is integrated and forms a connection with the existing community of Longcross and to ensure the development is not purely inwards facing. The new access points will provide punctuation and a visual connection into the village. Plans illustrate potential for a village green to the south-eastern corner alongside the proposed vehicular access, close to the junction with Longcross Road and Kitsmead Lane. This provides an opportunity to form a landscape feature at this southern entrance to the village, and whilst this would result in a visual change, with a combination of retention of key trees and woodland belt and new planting, there would not be harm to existing character.
- 8.14.4 An acoustic barrier and bund is proposed along the northern boundary of the site in order to reduce noise levels from the M3 motorway. This is proposed as a solid wooden fence approximately 4.7m to 8m in height above existing ground level. Whilst there are existing trees along this boundary, development would protrude above this and the greatest visual impact from the development site would be from the motorway. A similar barrier exists to protect existing residential development to the north of the M3 which is not uncommon for development alongside motorways. It will be important to secure details to ensure the siting minimises tree loss and where appropriate secures additional planting and appropriate design to reduce its visual prominence and have regard to the sites verdant setting.
- 8.14.5 The development has been designed to mitigate and reduce adverse landscape and visual impacts from the surrounding area, and this is reflected within the parameter plans submitted and secured through conditions. The development incorporates approximately 32 hectares of green infrastructure including on site SANG and links to existing off site SANG, together with formal and informal open spaces and playspace and areas for food production. This is reflected within the parameters plans and satisfied the requirements of SD9 (f) which seeks to ensure at least 40% of the site area is reserved for the delivery of green infrastructure.

8.14.6 The sports pitches, pavilion, and allotments on the east side of Kitsmead Lane, would result in the loss of an area of farmland and as such change in existing character. However, the development would maintain the existing open character and with appropriate tree retention and landscaping would not result in adverse visual impacts.

8.14.7 The ES and supporting information has identified a moderate adverse effects on landscape and views during the demolition and construction phase. This impact is temporary and can be minimised through mitigation which can be secured through conditions. Once the Development is complete and occupied there will be moderate beneficial effects for many views and some landscape aspects, with the remaining views experiencing minor adverse effects. No significant cumulative effects have been identified. It has been demonstrated that the quantum of development proposed can be accommodated within the site and having regard to the extensive areas of green infrastructure including tree coverage within and to the boundaries of the site, and informed by the illustrative masterplan and supporting documents, without significant harm to the wider landscape.

8.15 Ecology and Nature Conservation

8.15.1 Policy SD9 of the Runnymede Adopted Local Plan seeks to deliver at paragraph (f):
“A suite of connected green and blue infrastructure improvements across the range of different typologies with protection and enhancement of the natural environment through” a range of provision which includes:
“Having regard to the existing ecological qualities of the site including protected features and the objectives and targets for BOA units TBH01 and THB02 and delivering opportunities for net gains in biodiversity through priority habitat restoration/creation and priority species recovery through a range of green infrastructure typologies”.

The effect of the development on ecology and nature conservation is considered within the Environmental Statement and supporting documents which includes information submitted to support the Habitats Regulations. As set out above, in respect of an Appropriate Assessment under the Habitats Regulations, it has been demonstrated that there would be no significant effect on the integrity of the neighbouring Chobham Common which is a protected European Site (SPA and SAC) as a result of the development. Further information regard the proposed on site and off site SANG mitigation is provided at 8.19 below.

8.15.2 The construction of the development including demolition has the potential for loss of habitats and impact on protected species within the site. The application has been informed by various survey reports, and to ensure the application is informed by the most up to date information, an updated survey was undertaken during 2023 and updated report provided. Supporting information considered the impacts of habitats, vegetation and flora; amphibians; reptiles; breeding birds; bats; badgers; mammals; and invertebrates.

8.15.3 Existing habitats comprise a range of woodlands, with areas of hardstanding including perimeter test track road, and a central skid pan area of hardstanding. A large number of buildings are present on the site associated with both the former military use of the site as well as large stage buildings associated with the current film studio use (with temporary planning permission granted under RU.23/0118). Some of the existing habitats have developed at various times across the site as a result of disturbance from former uses. A number of grassland habitats are present within the site. Large areas of the site are designated as Priority Habitat, although updated condition survey of woodlands on site have indicated some would no longer be considered a priority

habitat. A large proportion of which would be retained within the on-site SANG and green spaces within and on the peripheral of the site. However, it is recognised that there would be loss of habitat to accommodate the scale of the development proposed and taking into account the limited use of the site in the years following occupation by the military (utilising pre-film use as the baseline for assessment). The majority of the semi-natural, broad-leaved woodland would be retained within the Green Grid and on site SANGS within the Site. However, approximately 6.1ha, of the seminatural, broad-leaved woodland would be lost as a result of the development. This would be an adverse impact of the development. However, the application also includes significant new planting and habitat enhancement, and despite this loss the application has been able to demonstrate the potential for a net gain in biodiversity of in excess of 10%, which is set out further in the report below.

8.15.4 The part of the site to the east of Kitsmead Lane comprises a small arable field. The site includes Habitats of Principle Importance comprising broadleaf woodland and lowland dry acid grassland. The areas of acid grassland located around the Barrowhills House parkland area which includes the former golf course, and support a rich and diverse flora with species that are threatened, rare and scarce including Heath Dog Violet. The Lowland Dry Acid Grassland habitat is considered to be of high conservation value that is of regional, possibly national, importance. Whilst some of this area will be retained, forming part of the on-site SANG it consequently will be subjected to increased recreational disturbance, there will also be a loss of acid grassland habitat, as reflected within the illustrative masterplan and proposed land use plans. The application proposes the translocation of acid grassland habitat from the site to the neighbouring Chertsey Common SANG. This strategy has been reviewed by Surrey Wildlife Trust and Natural England who have not raised any objections to this subject to conditions to ensure suitable management and protection during establishment of this area. Temporary fencing is proposed on the SANG to aid the establishment of the habitat and this will need to be reflected within the SANG management plan. Given the overall scale of SANG proposed, the restriction on access to part of the SANG is not considered as a barrier to delivery as the full extent of the SANG will not be required until the later phases of development enabling opportunity for habitat establishment. Furthermore, each phase or reserved matters application will need to demonstrate sufficient delivery of SANG for the quantum of residential development proposed within that phase. The loss of grassland habitat and flora is a negative adverse impact of the development which requires mitigation which is proposed as part of the application. This includes translocation of acid grassland within the Chertsey Common extended SANG, habitat enhancements and protection measures during construction works.

8.15.5 The application secures 40% of the site reserved for the delivery of Green Infrastructure and this is reflected within the Land Use Parameter Plan, which reflects the requirements of Policy SD9. Following completion of the development, the development includes both areas of retained, enhanced and creation of new habitats across the site. This includes habitat creation on Chertsey Common to include mitigation for great crested newts and ground-nesting birds, resulting from the loss of suitable habitats on the site, and resulting in an overall enhancement in habitat quality for these species.

8.16 **Protected Species**

8.16.1 Great Crested Newts (GCN)

Previous populations of GCN on site have been found to be small and recent survey suggests the population on site is declining. The application includes the loss of existing ponds/habitat and therefore a European Protected Species Licence (EPSL) for GCN will be required from Natural England to ensure the works are undertaken to avoid harm and provide appropriate replacement habitat. The mitigation strategy proposed under this

application is for the translocation of GCN and other amphibians to new ponds and terrestrial habitat that will be created in advance and will be managed in the long term within neighbouring Chertsey Common SANG, and would be secured through Condition. In addition, whilst not forming formal mitigation, there will also be opportunity for new ponds within the proposed development and areas of habitat retained within the proposed green grid.

8.16.2 Bats

The updated site surveys undertaken in 2023 identified a minimum of 4 species of bats roosting within 8 of the existing buildings on site. This includes the listed Barrowhills House (Building L87) which is to be retained, whilst other buildings are proposed to be removed. In addition, surveys of trees identified as high or moderate potential that are proposed to be felled have also been undertaken recently. This identified 3 roosts within existing trees to be removed (a day roost, maternity roost and hibernation roost). In addition, 58 trees of either high or moderate suitability are currently proposed to be felled to facilitate the proposed development. It is recognised that suitable trees have potential to be an important roost (foraging and commuting) resource for bats. The application has been submitted in outline and whilst it is recognised that in order to facilitate the development there will inevitably be a loss of suitable habitat. This will be reviewed through detailed design through RMAs and informed by updated surveys to accompany each phase or RMA. The loss of existing roosts will require appropriate mitigation, this will also form part of a protected species licence which will be required from Natural England. The ES provides details of proposed mitigation strategy which includes installation of bat boxes, bat lofts, sensitive lighting design and precautionary working approaches, including areas of tree and woodland retention and additional tree planting. It will be necessary to undertake further survey work to inform RMAs and prior to construction, having regard to the proposed build out period.

8.16.3 Badgers

Evidence of badger activity was found although no active setts were found to be present during the recent survey. This is in contrast to previous survey work which identified active setts on site, and therefore appropriate mitigation will be necessary to ensure the protection of Badgers utilising the site. Badgers often move between setts or create new setts and therefore further surveys are recommended pre-construction to ascertain the up to date presence and use of the site by badgers prior to works starting. This will also help to inform detailed mitigation measures as each phase or RMAs come forward. Precautionary measures are to be adopted during construction to avoid any impacts to badgers using the wider site which will be secured by condition (through the CEMP). The provision of the 'green grid' within the site including woodland areas will maintain foraging opportunities on site. A Badger Mitigation Strategy will be required for each phase of development to include updated survey information, sett and habitat impact assessment and mitigation strategy.

8.16.4 Dormouse

Whilst suitable habitat has been identified as present within the site within areas of woodland, no evidence of dormouse have been found during recent site surveys. This is also reflective of previous survey work undertaken. As such, this species is considered likely absent from the site.

8.16.5 Reptile

The areas of woodland provided suitable habitat for certain reptile species, patches of rough grassland, scrub and acid grassland within the site provided suitable habitat to support a reptile population and has good connectivity to other suitable offsite habitats. Whilst surveys undertaken have shown a decline on site, the ES sets out mitigation measures. It will be necessary to provide updated survey information together with

associated mitigation measures with reserved matters applications.

8.16.6 Invertebrates

A total of 59 species of invertebrate species were recorded on site in 2023. Having regard to the diversity of habitats present on site, the ES considers a significant effect likely on invertebrate assemblage due to habitat loss, if unmitigated. Mitigation measures include the retention, protection and enhancement of the main areas of woodland where deadwood (both standing and on the floor) is located, the translocation of the acid grassland into the nearby Chertsey Common Suitable Alternative Natural Greenspace (SANG) and the provision of extensive new habitat creation throughout the Green Grid and other areas of the site. Surrey Wildlife Trust have advised that the survey data provided does not provide survey data for early season and autumn and therefore are not confident that this provides a comprehensive demonstration that the baseline value of the application site for invertebrates is understood. The submitted ES states *“Increased lighting may result in adverse impacts on nocturnal invertebrates, particularly moths and beetles, by disrupting their feeding, breeding and movement”*. The absence of any nocturnal surveys to understand the baseline importance of the site for nocturnal invertebrates. However, the land use plan illustrates significant areas of the site which will remain undeveloped. The application has been submitted in outline and through detailed design of the reserved matters consideration will be given to the detailed layout and mitigation strategies including lighting.

8.16.7 Breeding Birds

The Ecology report considers overall, the site to be only of local interest for breeding birds. Although 10 of the 31 species recorded met at least one of a range of criterion relating to nature conservation value, all species were common and widespread species, and present in low numbers only. These species were mostly recorded along the boundaries of the survey area; adjacent to hedgerows and within the woodland. Notwithstanding this, the mitigation measures set out within the ES will be required.

8.17 **Biodiversity Net Gain**

8.17.1 Development should provide net gains for Biodiversity in line with paragraphs 174, 179 and 180 of the NPPF, and as promoted through policy EE9 and site allocation SD9 of the Local Plan which seeks to deliver a net gain in biodiversity.

The National Planning Policy Framework (2023) states *“Planning policies and decisions should contribute to and enhance the natural and local environment by... minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures”*.

The Runnymede Adopted 2030 Local Plan states *“The Council will seek net gains in biodiversity, through creation/expansion, restoration, enhancement and management of habitats and features to improve the status of priority habitats and species”*.

8.17.2 The Government has recently announced that the statutory requirement to deliver a minimum 10% Biodiversity Net Gain is to be mandatory for new major applications that are received after 12 February 2024. Whilst this is therefore not a statutory requirement for the current application, having regard to the importance of biodiversity reflected in local plan policies (EE9) and specifically referenced within Longcross Garden Village allocation SD9, and Green and Blue Infrastructure SPD, and the scale of the development proposed which is to be delivered over a number of years.

8.17.3 The applicant has submitted a Biodiversity Net Gain Assessment which is high level given that the application is submitted as in outline with all matters reserved. The

assessment seeks to provide sufficient information and evidence to demonstrate whether at the detailed design it is feasible that a biodiversity net gain (with trading rules and additionality satisfied) can be achieved given the presence of high and very high distinctiveness baseline habitats on the application site. This has been reviewed by Surrey Wildlife Trust and additional points of clarification and updates to the assessment have been provided to have regard to advice received.

8.17.4 The submitted assessment seeks to demonstrate a net gain is feasible comprising both on and off site enhancements and creation. The matrix submitted indicates that whilst there would be a slight reduction of just under 1% on-site net change, this is mitigated by an net increase of 23% off site change. It is therefore acknowledged that this relies on off-site enhancements made within the proposed extended SANG at Chertsey Common (Kitsmead Lane) and mitigation measures comprising Acid Grassland translocation to the SANG. Whilst the enhancements and habitat creation within the SANG is not required to support the function of the SANG, it is acknowledged that the application is heavily reliant on the off site strategy. It is also a consideration that the guidance on Biodiversity is that biodiversity net gain cannot be claimed if trading rules have not been satisfied. In this instance it is recognised that trading rules have been failed due to the strategy not providing sufficient compensation for 'Woodland and forest – felled', as this requires the same habitat to meet trading rules. The submitted assessment advises *“Given that transient nature, it is considered that this failure of the trading rules can be addressed through the Woodland Management Plan for the site, providing areas of felled woodland within the retained woodland on site, as part of a rotational management structure.”* Despite the limitation of the trading rules, the strategy for the planting and enhancement of woodland, as part of a Woodland Management Plan, provides a strategy which is considered to meet with the NPPF and Local Plan, and this view is supported by Surrey Wildlife Trust.

8.17.5 The application has been submitted in outline and as such the assessment has taken account of the indicative layout provided and therefore it is recognised that there will be variations with the final scheme. Nevertheless, if an allowance is made for flexibility in respect of trading rules, then the assessment does demonstrate how the proposed development could achieve a biodiversity net gain in excess of 10% compared with the pre-development baseline (prior to recent use by film studios). As this is an outline application which will be developed as phases, it will be necessary to ensure that with each phase of development a biodiversity gain. Having regard to the advice of Surrey Wildlife Trust, each phase should require the submission of an 'Overall Biodiversity Gain Plan and a Phase Biodiversity Gain Plan. In addition, having regard to the strategy for felled woodland, a Woodland management plan will also be required in order to demonstrate sufficient compensation and mitigation for the baseline felled woodland. Therefore, having regard to the above considerations, it has been demonstrated that the proposed development is capable of providing a net gain greater than 10% and would be compliant with LP policy, NPPF and Environment Act 2021 in this respect.

8.18 **Trees**

8.18.1 The application is accompanied by a full tree survey, detailing the key groups and individual tree specimens and their classification under BS5750. This also has regard to the existing Tree Preservation Order (TPO No.6) which applies to existing extensive tree cover along the site's southern and eastern borders (to Longcross Road and Kitsmead Lane). There is also an existing area Tree Preservation Order covering the

whole application site area (TPO 461) which was recently imposed following the intensification in use of the site for film production and associated buildings. The purpose of the order was to ensure the protection of existing trees by current users of the site relating to the temporary film studio use, and in advance of the consideration of the planning application. This ensures that those trees to be retained as part of the planning application for the redevelopment of the site are protected. The majority of tree groups within the site are category B or C with only a few category A trees, near Barrow Hills House within the proposed on site SANG. There are no aged or veteran trees within the site, nor any designated ancient woodland.

- 8.18.2 The applicant's arboriculturist has discussed the survey findings and the overall balance of proposed tree loss and retention, with the Borough Tree Officer. The development areas proposed in the submitted parameter plans have been produced having regard to these discussions at the pre-application stage. The illustrate masterplan and reflected within the land use and Green Infrastructure parameter plan maximises the use of existing areas of hardstanding and cleared areas of the site for the new development to minimise tree loss. It is also noted that some of the trees shown for removal on the submitted plans, have been removed over recent years as a result of the current temporary film related activity on the site and prior to the recent Tree Preservation Order protection. However, to achieve the proposed quantum of development including the site accesses, and in seeking to maximise the efficient use of the site to secure the delivery of a significant proportion of the Councils delivery of new homes over the next 10-15 years, there will inevitably be additional loss of trees.
- 8.18.3 It is recognized that there will be a significant loss of trees overall within the site. However, a large proportion would be of lower or of poor quality (category C or D), dead or otherwise compromised. Key groups and woodland areas within the site, and key individual trees have been identified for retention and have been illustrated within the masterplan to form key landscape features. This includes an extensive area of woodland centrally located within the site around the listed Barrowhills House and SAM which will form the on-site SANG and will form an important feature and green heart to the new village. This is also recognised and reflected within the parameter plans with key groups and woodland areas falling within the areas reserved for green and blue infrastructure. It is acknowledged that there may be some selected tree loss within these areas and similarly it is not held that all trees within the 'residential development' areas will be removed as there will be individual and groups of trees within these areas which are considered important to be retained and will form a key part of the landscape and character of the resulting development in conjunction with new tree planting. This will be considered through detailed RMAs, which will consider the detailed layout of the proposed development and impact on existing trees.
- 8.18.4 It is recognized that the new vehicular access points will result in the loss of existing trees having regard to the extensive tree coverage along the site boundaries, however this represents a relatively small proportion of the road frontage to both Longcross and Kitsmead Lane and as such is not considered that this loss would undermine the existing verdant character. It is also important to seek a balance between the retention of existing boundary trees and screening whilst also appropriately integrating the new development into the wider community of Longcross. Furthermore, the application affords the opportunity for new planting to offset the loss and provide opportunity for both formal planting including through woodland avenues and structural form and informal semi-natural parkland and woodland. The application also proposes

introduction of silvicultural management of the woodland providing footpaths, selective thinning and habitat promotion.

8.18.5 Whilst it is accepted there will be a loss of trees to accommodate the scale of the development proposed. The parameter plans and illustrative masterplan layout demonstrate how the development can come forward to minimise loss and maintain key tree belts and woodland areas within the site to ensure the existing woodland character is maintained and reflected within the new Garden Village. The site-wide landscaping strategy, as set out in the Landscape and Open Space Document addresses approaches to be taken in respect of all areas of open space, including on-site SANG, and development zones, and this is also reflected within the Green Infrastructure parameter plan. As noted in policy SD9, the generous approach to green infrastructure is a fundamental component of the garden village and this will be expected through the details submitted through reserved matters applications.

8.19 **SANG provision**

8.19.1 The application proposes both on-site SANG (14.658 ha) and off-site SANG, providing public open space. The on-site SANG has been designed to have regard to and protect existing on-site woodland and heritage assets encompassing the Scheduled Ancient Monument and setting of Barrowhills House and utilising this to create a green parkland setting at the core of the Garden Village. The on-site SANG would extend from this central area with a 'green' link to Kitsmead Lane and the planned extension of Chertsey Common SANG. This will be supported by a pedestrian crossing on Kitsmead Lane. The layout and siting has been designed to favour accessibility and use of the SANG both on and off site by future residents in preference to the neighbouring SPA (Chobham Common). Therefore whilst there is some desire to seek links between the new development and wider footpath network; the provision of the SANG and links to Kitsmead Lane would provide good connection to existing footpath FP45 to the west of Kitsmead Lane which extends to Lyne Close and Bridge Lane to the north east. However, connectivity to the south across Longcross Road to FP47 and BW66 would be discouraged as this would connect to Chobham Common SPA and as such undermine the provision of SANG.

8.19.2 The adjacent Chertsey Common SANG provides a total of 30.55 hectares of off-site SANG. Phase 1, comprising 5.1 ha has been delivered to off-set the recreational pressures of the existing residential development at Longcross North. Reserved matters approval has recently been granted for the remaining 25.52ha. The application also includes on-site SANG centred around the listed Barrow Hills House and Scheduled Monument, totalling 14.658 hectares. It will be necessary to ensure an appropriate quantum of SANG is delivered relative to the build out of the new dwellings and securing the Management and Maintenance of the SANG to ensure appropriate delivery having regard to the advice of Natural England. The total SANG provision, over both Longcross North and Longcross South is 48.198ha. This would provide SANG for the quantum of residential development proposed under this outline application (up to 1700 dwellings, 60 bed Class C2 care home and 10 Travelling Showperson plots) at the higher rate of 10.7ha per 1000 population, including the existing 186 dwellings at Longcross North. For most sites the provision of SANG is required at a standard of at least 8 hectares per 1,000 population, however, given the close proximity of the site to the SPA, Natural England have advised that a higher standard is required for this development. This would accord with the requirements of Policy EE10 which requires measures to avoid and mitigate the potential effects on the Special Protection Area.

8.19.3 Natural England have reviewed the application and following amendments to the SANG Management and Maintenance Plans they have raised no objection to the application. It will be necessary to secure the delivery of SANG relative to the delivery of the new dwellings together with financial contribution towards Strategic Access Management and Monitoring (SAMM) in line with the Thames Basin Heaths SPD. The provision of SANG and associated ongoing management and maintenance will be secured through mechanisms within the s106.

8.20 **Open Space Strategy, Childrens' Play and Sports Provision**

8.20.1 Policy SL26 sets out the requirements for new open space including provision for outdoor sports facilities (1.6ha per 1,000 population), provision for children and teenagers (0.8 ha per 1,000 population). The application is supported by a 'Landscape and Open Space Document which includes details of the proposed play strategy for the site which is also reflected within a separate 'open space strategy' plan which sets out how outdoor sports facilities including equipped play space and informal provision can be accommodated with the site. It also details provision for allotments together with alternative food production provision which are discussed further below. The submitted Green Infrastructure parameter plan proposes land to the east of Kitsmead Lane for formal sports and other green infrastructure, including the provision of sports pitches as well as illustrating provision of play areas (LAP, LEAP and NEAP).

8.20.2 The strategy proposes a centrally located NEAP (Neighbourhood Equipped Area for Play), within the central SANG which maximises accessibility, and is targeted primarily for older children. This is proposed in the form of an 'adventure playground' following the character of the woodland and landscape setting. The application also proposes four LEAPS (Local Equipped Area for Play) evenly spread across the development and several LAPs (Local Areas for Play) distributed across the development and also reflected on the 'Green Infrastructure Parameter Plan'. The play strategy includes the provision of informal playing space utilizing areas of amenity greenspace, distributed along the green grid.

8.20.3 The development also provides for outdoor sports facilities which includes playing pitches. For the quantum of development proposed, policy requirements equate to a total of 6.26ha including 4.5ha of playing pitches. The application proposes playing fields (senior and junior football pitch and 8 wicket cricket pitch and associated supporting facilities – pavilion building and parking) on the land east of Kitsmead Lane. In addition, a dual use of the primary school sports pitches is proposed. Whilst, the quantum of sports pitches at 3.76 hectares would fall slightly below the required area which is a shortfall that weighs against the scheme. Having regard to the wider provision of outdoor sport and green space within the development with the sports strategy which includes opportunities for on-site running routes, trim trail fitness and an outdoor gym it is considered collectively the development is able to demonstrate an appropriate quantum of outdoor sports facilities. The illustrative masterplan is considered to demonstrate an appropriate quantum and distribution of equipped and informal playing space in compliance with Policy SL26, with details to be secured through RMAs. However, the policy also allows for off-site contributions should the detailed design result in any shortfall.

8.20.4 In addition, the development proposes a range of green spaces, including SANG, areas of woodland, sports facilities, playspaces, village green, orchards, community gardens and allotments, providing a variety green spaces, with at least 40% reserved for the

delivery of Green Infrastructure reflecting Policy SD9. The application is submitted in outline and therefore the detailed layout and design are not being fixed at this stage. However, the supporting information helps to demonstrate that appropriate provision can be accommodated within the site, providing both equipped and informal playing space within the site to meet local plan policies and to promote opportunities for recreation and social interaction in accordance with Policy SL1 which supports health and wellbeing.

8.21 **Food Production**

8.21.1 Policy SL26 also seeks to secure opportunities for food production, requiring at least 20 standard size allotment plots (250sqm per plot size) per 1,000 population or where not possible, provision of an alternative such as community gardens or similar. Having regard to the quantum of development proposed this would equate to some 34 allotment plots comprising a total area of some 0.85ha. The application proposes a mixture of allotment provision (illustrated to be accommodated within parcel of land to the east of Kitsmead Lane) as well as opportunities for provision within the main development site. Taking into account the scale of the development proposed it is not considered that food production provision solely in the form of allotments would be appropriate. The open space strategy proposes a range of provisions for food production including traditional allotments, community allotments/ community growing gardens alongside the school and local centre, together with community orchards, as well as community courtyard gardens to be designed within the layout of some of the apartment development.

8.21.2 It is recognised that the area illustrated on the masterplan for the allotments east of Kitsmead Road is likely to provide potential for approximately 8-10 allotment plots. The shortfall (as required under Policy SL26) equating to approximately 27 plots would be provided as alternative food production provision within the main Garden Village site. Whilst a larger allotment provision would provide a more attractive community facility, it is also recognized that the space available also has to provide appropriate provision for sports pitches. The application as a whole is capable of providing a range of provisions for food production across the garden village which would enable compliance with Policy SL26 to be demonstrated. Each phase of development will be expected to demonstrate consideration and provision of food production appropriate to that phase.

8.22 **Land Contamination**

8.22.1 Given the potential for contamination to be present on site the application is supported by Ground Investigation reports in respect of contamination. This has included investigation work including boreholes, groundwater and soil gas monitoring. The results of the soil testing do not indicate a requirement for site wide remediation. However, further targeted ground investigation and an asbestos survey is recommended. The Council's Contaminated Land Officer has reviewed the application and proposes a condition to secure further investigation to inform required remediation measures. With appropriate remediation and protection measures as required, it is considered that the potential for presence of contamination should not form a barrier to development and any remediation of the land would be a benefit. Therefore with conditions in place the application is considered to be compliant with Policy EE2 in respect of land contamination.

8.23 **Flood Risk, Drainage and Water Resources**

- 8.23.1 The ES has considered the effects of development on water resources and flood risk, and with the recommended mitigation measures in place would not result in a significant adverse impact. This includes a CEMP which will include measures during construction to reduce any potential contamination or pollution, and provision of an appropriate wastewater drainage strategy and water efficiency measures.
- 8.23.2 Contamination
The Environment Agency considers that the former use of the site (as a military testing facility) and current film studio use presents a medium risk of contamination that has the potential to pollute controlled waters during construction phase. The Environment Agency have confirmed that the proposed development will be acceptable with planning conditions imposed. With the recommended conditions imposed they are satisfied this would ensure that that the development will not be put at unacceptable risk from, or be adversely affected by, unacceptable levels of water pollution. The Environment Agency confirm that the Flood Risk Assessment and Outline Drainage Strategy submitted to support the application provides confidence that it is possible to suitably manage the risk posed to controlled waters by the development. Further detailed information will be required before the built development is undertaken. It is considered that this can be secured through planning conditions in order to deal with the risks associated with contamination of the site, including remediation and verification strategy which would satisfy the requirements of the Environment Agency.
- 8.23.3 Drainage – surface water
In accordance with Policy EE13 all new development is required to ensure that sustainable drainage systems are used for the management of surface water unless demonstrated to be inappropriate. The site is covered by a combination of hardstanding, non-permeable buildings (approximately 40% coverage) and permeable amenity grassland, along with other permeable surfaces (approximately 60% coverage). There are no groundwater protection areas either within or adjacent to the site. The site lies within flood zone 1 which is identified as having a very low risk of river flooding. In addition, the site as a whole has not been identified as being at risk of flooding from surface water although several localised spots throughout the site have been identified at medium to high risk from surface water flooding. The drainage report attributes this to localised low points within the site along with poorly maintained, substandard and silted surface water drainage system, as generally the site is elevated above surrounding land. The redevelopment of the site with reprofiling of parts of the site and proposed drainage system will be designed to address this and minimise the risk of surface water flooding beyond the site.
- 8.23.4 To address any potential risks of surface water contamination or any detrimental impacts to existing drainage resulting from the construction works, a CEMP is proposed to incorporate measures to mitigate potential impacts including provision for temporary drainage systems bunding and ponds. The development will increase the amount of impermeable area across the application site and therefore attenuation is required to limit flows and manage surface water on site. The submitted Drainage Strategy proposes the use of sustainable drainage measures including use of swales, raingardens, and ponds providing on site attenuation to control the discharge into existing outfall points at a restricted rate. The SuDS features proposed will provide landscaped features within the site with potential for both wet and dry basins/ponds which can also assist with biodiversity. In addition, opportunities for on plot attenuation such as permeable paving and swales assist to minimise pollutants in the drainage system to safeguard water quality.
- 8.23.5 The LLFA are the statutory consultee in respect of surface water flood risk and surface water drainage. They initially raised an objection to the application on the basis of a

lack of information to demonstrate compliance with the requirements of the NPPF and PPG in relation to surface water drainage. The applicant has engaged in pre-application discussions with the LLFA, and additional information has been submitted to support the application in response to comments raised. Following a review of the updated Drainage Strategy Statement (July 2023) and Surface Water Outfall Technical Note (July 2023), the LLFA have confirmed that the proposed drainage scheme meets the requirements set out in the NPPF, its accompanying PPG and the Non-Statutory Technical Standards for sustainable drainage systems. They have confirmed no objection to the planning application, subject to Conditions to ensure that the SuDS scheme is properly implemented and maintained throughout the lifetime of the development, with associated conditions recommended.

8.23.6 Drainage – foul water

Thames Water have confirmed that the sewerage network in the area does not have additional capacity to serve the proposed development and significant upgrades to the waste water network will be required. They have been in discussions with the applicant to develop an appropriate foul water drainage strategy and agree a phasing plan required to ensure capacity is provided for all phases of the development. The upgrading of the existing network is expected to include the upgrade of the existing Kitsmead Lane pumping station to increase capacity.

8.23.7 Conclusion

The drainage details will be secured through condition having regard to the strategy submitted to inform this outline application. A 'Management and Maintenance Plan is to be secured by condition to follow the principles of the information submitted to support the application. The full details of the drainage design will be developed through RMAs which considers the layout and landscaping details of the scheme. The Environment Agency and LLFA have raised no objections to the proposed development subject to conditions. With conditions in place the development will not create issues of flood risk either on or off site, and will ensure compliance with current planning policy. Upgrade works will be required before connection to existing foul sewage networks which may necessitate private waste water management on site in the interim if the timescales do not align. No objections have been raised in principle by Affinity Water who are responsible for water supply. They would wish to see measures in place to reduce groundwater pollution risk (discussed above and controlled through conditions including CEMP), as well as water efficiency measures.

8.24 **Air Quality**

- 8.24.1 An Air Quality Assessment has been undertaken and has been considered as part of the Environmental Statement. It has regard to both demolition and construction of the development as well as post completion. It identifies the potential temporary impacts on local air quality as a result of dust arising during demolition, earthwork and construction activities and impact from construction traffic. This includes consideration of the adjacent Heathland habitats of Chobham Common which are vulnerable to deposition of dust with the potential to cause localised vegetation dieback and can be sensitive to changes in air quality. In addition, dust during construction phases has the potential to impact on surrounding residential properties. However, it is considered that the potential impact resulting from dust during construction can be significantly reduced through good site practices and suitably addressed through the implementation of mitigation measures including a Dust Management Plan proposed as part of a CEMP which is to be secured by condition. In terms of emissions from construction vehicles and plant this has been assessed through the Environmental Statement and includes

mitigation measures to ensure the development can be constructed without adverse air quality impact including measures proposed under the CEMP.

8.24.2 The potential impacts resulting from traffic generation following completion of the development would be addressed by a series of mitigation measures. This includes (but not limited to) off site highway works and proposed sustainable transport options and improvements, support for alternative travel modes including walking, cycling and use of public transport (bus and rail services), in order to reduce the number of vehicle trips to and from the site. The provision of Electric vehicle charging points will also support the shift to electric vehicles and assist with air quality. The Energy Strategy also seeks to minimise carbon emissions through design of the development and use of renewables. The implementation of the mitigation strategy is expected to reduce potential air pollution concentrations and mitigate any potential adverse air quality impact at the site and surrounding receptors. The results of the air quality assessment concludes that with mitigation measures in place, there will be no significant adverse impact to air quality as a result of the development. Therefore, subject to conditions, the application is considered to satisfy the requirements of Policy EE2 in respect of Air Quality.

8.25 **Noise and Vibration**

8.25.1 A noise and vibration chapter has been included within the Environmental Statement (ES) and an Acoustic Assessment has been submitted to support the application together with an updated report to respond to specific points raised, updated March 2023. This considers the suitability of the site for the uses proposed (namely residential and educational as being sensitive to noise). It is recognised that the site is impacted from noise from the M3 motorway which extends along the northern boundary of the site. The development includes proposals for an acoustic fence up to 8m in height along this northern boundary. The report shows that the majority of the site and private gardens would achieve the 55 dB noise target. There would however, based on the illustrative layout, be some gardens (approximately 4% equating to an estimated 48 out of 1279 gardens) which are located along the north-eastern, eastern and southern boundary which would exceed the target external noise criterion (55dB). However, this would be by a small margin of approximately 1-3dB, affecting a small proportion of properties and for most is limited to only part of the garden. This doesn't however take account of the apartments which would benefit from communal amenity space. Whilst the majority of the apartments are illustrated along the northern boundary, south of the M3, the noise assessment demonstrates that the layout can be designed to achieve external noise targets within the communal amenity spaces. It is also recognised that some of the dwellings would require updated double glazing and ventilation units to achieve appropriate internal noise targets.

8.25.2 Based on the layout of the indicative masterplan, the Showperson plots would also be subjected to noise levels above the target criterion. It is recognised that this is a negative of the scheme. Whilst the nature of the use would mean that they would not necessarily be occupied on a permanent basis given the travelling requirements of Showpeople, nevertheless there would be some harm from noise which is indicated to slightly exceed target levels, based on the indicative siting illustrated within the masterplan layout. As this is indicative, there will be opportunities to consider noise through the detailed layout of the development as part of reserved matters submissions.

- 8.25.3 For the vast majority of the site, standard thermal double-glazed units and acoustic through-frame vents are predicted to be sufficient to meet internal noise targets. For the first row of houses along the northern boundary of the site, a higher level of acoustic glazing and ventilation will be required, and mechanical ventilation with heat recovery (MVHR) may be necessary for apartments along the northern edge. The most noise sensitive boundary is along the northern boundary adjacent to the M3 which enables the use of passive measures to minimise solar gain (and reduce overheating) and therefore reducing reliance on open windows to reduce overheating.
- 8.25.4 However, it is acknowledged, to achieve the indicated quantum of development proposed that there will be areas of the site and thus proposed dwelling units which will not meet current noise target guidance. It is also acknowledged that whilst 'the majority of the site' will meet internal guidance with windows open (to allow for overheating), it is also noted that there will be parts that will not. The reliance on mechanical ventilation to meet noise guidance is not desirable and would be a shortcoming of the scheme which will need to be acknowledged and considered further through detailed RM applications. The current application is seeking outline permission for the quantum of development proposed and therefore the detailed siting and layout through RMAs will need to have particular regard to noise in the siting and layout of the proposed dwellings, to ensure a suitable environment for future residents can be secured. The school is shown indicatively within an area of the site which would meet noise targets. Proposed play spaces should also meet target external noise guidelines and whilst the illustrative layout demonstrates this would be achieved for the majority this would be considered through the detailed layout and design of the proposed development.
- 8.25.5 The application is proposed in outline, and whilst it is acknowledged in order to achieve the quantum of development proposed that there will some exceedance of target noise levels, within external garden areas, and some properties will require the use of alternative ventilation including mechanical ventilation to achieve desired internal noise targets. There are opportunities through detailed layout and design to minimise impacts through passive measures and siting of layout of development. Any exceedance would be limited as set out above. The detailed layout and design of the development through RMAs will need to have regard to noise impact to ensure the amenity of future residents are not unduly harmed through noise.
- 8.25.6 Potential noise impact during construction (and demolition) phase of development, including both existing residents in the vicinity and adjacent to the site as well as new residents as the development starts to become occupied and having regard to the build out period for the development. Whilst there will inevitably be some impact resulting from the construction works, the requirement for a CEMP will enable measures to be put in place to minimise impact on residential amenity as a result of the construction works.

8.26 **Heritage**

- 8.26.1 One of the principal objectives of the National Planning Policy Framework (NPPF) is the conservation of historic environment. The NPPF advises that 'Heritage assets are an irreplaceable resource and should be conserved appropriately to preserve their significance and to allow the continued enjoyment of existing and future generations (Paragraph 189)'. And furthermore 'where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal' (Paragraph 202). This is further reflected within Local Plan policies. Policy SD9 and EE4 seeks to protect and enhance

heritage assets and their settings. Local Plan policies also seek to preserve the character and significance of locally listed and other non-designated heritage assets and their settings (Policy EE8).

8.26.2 The heritage sensitivity of the site is acknowledged within the Environmental Statement which considers the designated and undesignated heritage assets and examines their significance and level of threat posed by the development proposals before suggesting appropriate ways to mitigate the effects. Surrey County Archaeology consider that this is based on a comprehensive archaeological desk-based assessment and built heritage assessment which identifies two main periods of archaeological interest in respect of Bronze Age activity with reference to the Scheduled Barrow and later use as a military testing centre from 1953.

8.26.3 Archaeology

The impact of the development on Cultural Heritage including existing designated and non-designated heritage assets has been considered within the Environmental Statement. Central within the site is a Bronze Age barrow designated as a Scheduled Monument (SAM) and is considered one the largest examples of its type in Surrey. Associated with this, the barrow and adjacent land is designated as 'Area of high archaeological potential'. Possible additional non-designated barrows may exist on site with potential for buried artifacts or features. Whilst potential for buried heritage assets may have been impacted by the former military use, further archaeology work will be required through the reserve matters stage.

8.26.4 The barrow is situated in a very prominent location, in an open and elevated position within the site albeit within a predominantly wooded setting. Historic England have been involved in pre-application discussions and as a result the area around the SAM is to be kept free from development. The area around the scheduled barrow is to form part of the central on site public open space and SANG which is reflected within the parameter's plans submitted which excludes this area from development. This approach is welcomed by Surrey Archaeology and Historic England who have advised that this 'enables the barrow to be respectfully appreciated as the significant and important burial feature it was designed to be'.

8.26.5 The illustrative masterplan layout and reflected within the land use parameter plans which include the removal of residential development from the eastern side of Barrowhills Green has led to the addition of housing to the south-west of the Scheduled Monument which has the potential to encroach on the setting of the monument. Given the level of information currently available Historic England suggest the level of harm is likely to be on the lower end of less than substantial. The removal of the housing from the eastern side of Barrowhills Green was undertaken to secure an open vista from the sites south-eastern entrance towards the open space and listed Barrows Hills House. Historic England acknowledge that that the redevelopment of the site will still have some impact on the monument which will need to be considered and mitigated. There are also opportunities to preserve and enhance this highly designated heritage asset, in order secure its longevity and enjoyment by future generations. Whilst increased use and public access of the site will also increase the risk of erosion or damage to the barrow, Historic England recognise that measures can be designed into the scheme to limit potential impacts together with management and maintenance of the barrow.

8.26.6 Policy EE7 requires development to conserve and where appropriate enhance the significance, historic features and importance of Scheduled Monuments and County Sites of Archaeological Importance and their settings. The application has been

designed will minimise the impact of development on the SAM and its setting. The policy further advises that 'proposals which improve public access to, or the understanding of, a Scheduled Monument or county Sites of Archaeological Importance in a manner consistent with its conservation, will be supported'. Currently the site is not publicly accessible, the application will secure and support public access to the site and in addition, a scheme for the interpretation of designated and non-designated heritage assets and a public art strategy is proposed to be secured by condition which provides an opportunity for the sites unique history is captured and communicated as part of the redevelopment of the site. Whilst the detailed design and layout will need to have regard to the SAM and its setting, it is considered that the scheme presents wider public benefits which would outweigh the less than substantial harms that would result.

8.26.7 Evidence suggests that other barrows may have been present in the area and a programme of archaeological investigation will be required to assess the potential for significant archaeology elsewhere on the site. It is acknowledged that much of the site has been disturbed by activity associated with its former military use and investigations will need to be targeted to areas with no previous impact. Surrey County Archaeology are satisfied that this can be undertaken following the grant of planning permission.

8.26.8 Historic England have raised no objection in principle to the development but also acknowledge that additional steps could be made and addressed as reserved matters specifically in respect of protection measures, management of the monument and the provision of interpretation providing opportunities to reveal and enhance the significance of the Scheduled Monument, in line with national policies.

8.26.9 Listed Building

The built heritage assets, which include the mid-nineteenth century Barrow Hills house and its early twentieth century garden terrace, both Grade II listed, and located to the east of the SAM. Policy EE4 'supports appropriate development which seeks to maintain, sustain and enhance the significance and special architectural and historic interest of Listed Buildings'. The application proposes the retention of the listed Barrows Hill House as existing. The surrounding land will form part of the open space and on-site SANG which will ensure the setting of the Listed building is maintained with potential for enhancements. A small area to the northwest of the listed building is shown for development. This relates to an existing area of hardstanding which includes two existing properties/buildings. It is therefore considered that development within this limited site area could be provided without harm to the setting of the listed building. It will be for the associated reserve matters application to ensure that the detailed design and form of development is acceptable.

8.26.10 Non-designated heritage Assets

The site also contains non-designated post 1953 military structures and features which have been subject to initial recording providing a basic visual record. Individually the majority of the structures are considered of local significance, however their significance lies in their group value and their association with the tank testing facility which enhances their significance to regional. Policy SD9 seeks to explore opportunities to retain and reference the sites historical links with the defence industry. However, because of their specialised nature, the majority of the structures are not considered suitable for re-use or incorporation within the development and are proposed to be removed. These include an amphibious testing pit, perimeter test track, and associated testing control tower, various storage and support buildings which are proposed to be

removed to facilitate the development. Surrey County Archaeology advise that further recording (to Historic England Level iii) would be appropriate in order to create an analytical record and that results in a final publication that sets out the results of the work and records the modern military history of the site.

- 8.26.11 The most significant structure is the Multi Gradient Vehicle Drop (MGVD) located just to the north of the scheduled barrow. It was formerly used for testing tanks on steep incline, and is recognised as forming a unique and interesting feature of the sites military history and as such an important non-heritage asset within the site. The top of the ramp is capped in concrete and forms the highest point of the site. The MGVD consists of four concrete ramps of increasing gradients for tanks to transverse and constructed into the existing hillside. The MGVD would be retained in situ and enhanced to form both a heritage and landscape feature within the site.
- 8.26.12 The loss of existing non-designated heritage assets is considered to lead to a less than substantial harm, which can be mitigated through recording and interpretation. Furthermore, the wider public benefits of the scheme would outweigh the less than substantial harm identified. Surrey County Archaeology is satisfied with the information submitted in support of the application and have raised no objections to the application subject to conditions to secure targeted trial trench evaluation and a programme of Historic Building recording. This will ensure that any buried archaeological assets are identified and recorded and that an appropriate record is made of the identified undesignated heritage assets in advance of their loss together with a scheme for interpretation including potential public art.

8.26.13 Public art strategy

Requirements for a strategy for the integration of public art into the village is set out within policy SD9(a). This affords the opportunity to enable the military history of the site, in conjunction with the more recent use for film production and historic bronze age use, to be capture and communicated into a strategy for the site. This will be secured by condition and prior to any removal of existing buildings and structures.

8.27 **Sustainability, Energy and Climate Change**

- 8.27.1 Policy SD8 (renewable and low carbon energy) in addition to securing a minimum 10% of the developments energy needs from renewable and/or low carbon technologies, also expects development proposing more than 50,000sqm additional floorspace to provide onsite, new decentralised networks of renewable or low carbon energy sources, unless it can be demonstrated that it is not practical, feasible or viable. The application is supported by an Energy and Sustainability Statement. This sets out a range of sustainability measures proposed. The energy strategy proposes a fabric first approach through energy efficient design and materials before low carbon/renewable energy to reduce emissions further. This reflects the requirements of Policy SD9 (Longcross Garden Village) which seeks to maximize opportunities for natural resources through a fabric first approach, potentially including passive solar design, passive cooling and implementing water efficiency measures. This is also reflected within Policy SD7 which seeks to maximise opportunities for passive solar gain and passive cooling through the orientation and layout of development. The application submission sets out that dwellings will be designed to accord with these principles. It will be necessary to ensure the details that come forward through reserved matters stage are consistent with this approach.
- 8.27.2 The energy and sustainability statement considers the feasibility of potential

technologies and concludes that Air Source Heat pumps and solar photovoltaics are the most viable technology for the development which will enable over 10% reduction of the developments energy demand from on-site renewables and low carbon sources in accordance with policy requirements.

- 8.27.3 A Decentralised Energy Networks Viability Study has been submitted, which specifies that decentralised energy networks have been considered but discounted as not being considered a feasible option for this site. The Energy & Sustainability Statement Addendum states that the proposed density and anchor loads of residential development will not be sufficient to support either a district heat network serving the whole site, or a network serving a proportion of the site and this would not be a commercially viable option. The benefits of gas CHP will reduce as a result of national emission factors. In respect of heating, the applicant proposes a fully electric strategy to coincide with the Government's direction of travel with on-going decarbonisation of the electricity grid. The Energy Statement demonstrates that heat pumps, coupled with excellent fabric standards and small solar PV arrays are anticipated to deliver the carbon savings required by the Future Homes Standard. Details will be considered through the reserved matters applications.
- 8.27.4 In addition, the development seeks to promote sustainable travel through safe and accessible pedestrian and cycling routes and a range of facilities and land uses on site. In addition, The public transport strategy and Travel Plan will incorporate a range of sustainable measures which support use of rail and bus services, car clubs and electrical vehicle charging points. Considered further within movement section above.

8.28 **Housing Supply/mix**

- 8.28.1 Policy SD9 seeks to deliver a minimum of 1,700 net additional dwellings and specialist accommodation comprising a mix of housing tenures and types across the Garden Village site allocation. The Environmental Statement and reflected within the supporting documents submitted propose a maximum 1,700 new residential dwellings together with up to 60 bed care home, and 10 Travelling Showperson plots.
- 8.28.2 The application has been submitted in outline with density, housing mix and tenure provided illustratively within the supporting illustrative masterplan. This indicates a range of dwelling typologies and sizes including apartments and houses ranging in size and tenure which will vary throughout the development from apartments (1, 2 and 3 bed) through to 5-bed homes. The indicative dwelling mix which informs the planning application has taken account of current policy requirements. The final housing mix will be confirmed at the detailed design stage but will need to demonstrate compliance with planning policy and reflecting housing need It is expected that the mix and housing type will vary throughout the development to reflect the varying character areas ie with northern 'The Avenue' character area expected to come forward as predominantly apartments, with dwellinghouses of varying sizes forming the predominant house type to the lower density south eastern 'Barrow Hills Green; character area. However, the RMAs will be expected to demonstrate general accordance with the housing mix set out within the Strategic Housing Market Assessment (SHMA) reflected within Policy SL19, to ensure each phase of development, individually and collectively addresses housing need requirements.

8.28.3 Affordable Housing

Policy SL20 (Affordable Housing) sets out the Councils requirements for affordable housing provision, requiring 35% of net additional dwellings to be provided as affordable housing. This is further reflected within Policy SD9(b) which also provides a split of tenure type and size which the development 'should generally follow'. The split set out within policy SD9 includes affordable rent, social rent and affordable home ownership. However, Since the adoption of the Local Plan the Government in May 2021 published a ministerial statement and reflected in changes to national planning policy in June 2021, which introduced the requirement to secure First Homes as part of the provision of affordable housing for sites delivering 10 or more net additional dwellings. The requirements for First homes has informed the Councils 'First Homes – Interim Policy Statement (January 2022)'. First Homes are the governments preferred discounted market tenure and should account for at least 25% of all affordable housing units delivered by developers through planning obligations. Therefore, given the requirement for a minimum of 25% of all affordable housing units to be First homes, this changes the tenure split requirement which is set out in Policy SD9 to the following:

Affordable Type/Tenure	SD9 Policy Requirements			Mix with First Homes	Quantity based on 1700 dwellings	
	% Split	Size				% Split
		1 bed	2 bed	3 bed		
Affordable Rent	60%	0%	65%	35%	53%	315
Social Rent	10%	20%	65%	15%		
Affordable Home Ownership	30% (at least half for shared ownership)	50%	50%	0%	22%	131
First Homes					25%	149
Total					35% Affordable	595

8.28.4 An updated Affordable Housing Strategy was provided in August 2023 to reflect the required tenure mix including first homes. It states the Applicants propose an Affordable Housing Strategy which is compliant with Local Plan Policy SL20 'Affordable Housing' and the First Homes Interim Policy Statement (January 2022). The overall provision for affordable housing reflects policy requirements and this will be secure through S106 agreement. It is recognized that with each reserved matters application or phase of development, there is likely to be some variation in the tenure mix that is being delivered to take account of the specific character and type of development to be delivered within each specific phase. Therefore, each phase or reserved matters application will need to demonstrate delivery towards the overall mix of affordable housing as required by planning policy and at a suitable pace of delivery to ensure a suitable quantum and mix is provided through each phase of delivery. This would be

reviewed and agreed at each reserved matters stage. Furthermore, this enables consideration of changes identified within any future update to the Strategic Housing Market Assessment (SHMA) over the build out period for the development in order to meet the identified housing needs.

8.28.5 The Council's Housing Officer would like to see some of the affordable dwellings which RBC are nominating to achieve compliance with Part M4(3) of the Building Regulations providing wheelchair adaptable/accessible units. In addition, they would wish to secure some homes at Social rent (or at a lower than the maximum 80% market eg. 65%) to make them genuinely affordable to those on the Housing Register. The provision of dwellings for social rent is reflected within the specific requirements of Policy SD9, and whilst this also now accounts for first homes, there remains the expectation that there would be a percentage of units for social rent. Housing have identified that they have a shortage of supported housing in the borough and would like the delivery of a small block of supported 1 bed or studio flats as part of the affordable offer. The application has been submitted in outline but does demonstrate a commitment to delivering a policy compliant scheme in respect of affordable housing type and tenure and mix of dwelling types, the provision and details of which will be secured through S106, with detailed design and layout considered through RMAs.

8.28.6 **Self Build and Custom Build Housing**

Policy SD9 (b) includes the provision of housing to be delivered by SMEs (Small and Medium Enterprises) and/or self-build plots in accordance with Policy SL24. Policy SL24 requires consideration to be given to custom and self-build plots as part of housing mix with serviced plot(s) being provided where there is an identified need and is viable and feasible to do so. At the time of completion of this report, the self-build register held by the Council pursuant to The Self-build and Custom House building Regulations 2016, currently contains 11 names on the waiting list. As this demand appears particularly low at present there needs to be ability to review demand and provision through the reserved matters stages and affords opportunity to add to the diversity of housing delivery options within the garden village. However, through the delivery of each phase and reserved matters application a review of demand and feasibility for delivery will be expected. This is considered important for a development of this scale and lengthy build-out period. As noted above, the development will also be providing for a range of affordable housing, and travelling showperson plots and overall this is considered compliant with the aim of policy SD9 in meeting the objectively assessed housing needs.

8.28.7 **Extra Care Housing**

Policy SD9(b) seeks to secure the provision of an extra care facility comprising in the order of 60 units to assist in meeting the specialist accommodation requirements for those with care needs including for an aging population. The land use parameter plan reflects both C2 and C3 residential uses. The Illustrative masterplan includes provision of a care home to the south of the village centre and village green, which reflects the requirements of Policy SL23 for proposed development for older persons to be readily accessible to public transport, shops, local services, community facilities and social networks. The application is submitted in outline and therefore details will be secured through RMAs.

8.28.8 **Travelling Showperson Plots**

The G&T Accommodation Assessment 2018 which supported the Local Plan, identified

the need for 21 additional Travelling Showperson plots over the period up to 2035. Policy SD9(b) requires the delivery of at least 10 serviced plots for Travelling Show People in accordance with Policy SL22 to assist in meeting current needs for sites. Policy SL22 define Travelling Showpeople as, “Members of a group organised for the purposes of holding fairs, circuses or shows (whether or not travelling together as such). This includes such persons who on the grounds of their own or their family’s or dependants’ more localised pattern of trading, educational or health needs or old age have ceased to travel temporarily, but excludes Gypsies and Travellers”.

- 8.28.9 The Councils ‘Design Guide’ includes at Appendix 5, guidance for designing Travelling Showpeople sites. This guidance was produced following consultation with the Showmens Guild. It advises that as a guide, plots should have an area of at least ¼ acre, with individual plots to be large enough to accommodate adequate space for car parking and storage of equipment, pitch boundary treatment, space for a mobile home and touring caravan, and area of private amenity space and amenity building. The application includes land reserved for up to 10 Travelling Show person plots which is illustrated indicatively within the land use parameter plans to the north-east corner of the site enabling opportunity for direct access onto Kitsmead Lane. A potential point of access is shown illustratively on the plans but does not form a full consideration under the current planning application.
- 8.28.10 To support the application a ‘Travelling Showperson proving layout’ has been provided to demonstrate one way in which the provision of at least 10 plots could be provided within the identified area. The area of land indicated for these plots has been enlarged since first submitted following concerns regarding the plot sizes. These have been enlarged to reflect a minimum ¼ acre plot as set out at Appendix 5 of the Design Guide SPD. These plots need to include provision for the storage and maintenance of show equipment and associated vehicles. The final design will need to ensure suitable access onto the highway network, as well as access for large vehicles to manoeuvre within the site, this is also reliant on resulting land levels. Whilst the proving layout does not provide full details of land levels which will be of importance as to whether the indicative siting is appropriate and achievable, officers have some initial concerns regarding the practicalities of the illustrate layout for the use proposed as well as the relationship with the site boundaries, including adjacent trees and neighboring properties.
- 8.28.11 The Showmen’s Guild have provided comments on the indicative layout illustrated including desire for a minimum ½ acre plot size to enable space for a living area (including chalet and tourer) and a working/storage area including space for storage of large vehicles and enabling space to build up and maintain the rides, and ensure compliance with HSE legislation. The minimum size for the plots suggested by the Showmens Guild would be much greater than that which informed the Design Guide. It is however recognised that the majority of the space requirements relate to the storage and maintenance of fairground equipment/rides and associated vehicles, and that the size requirements will vary dependent on the scale of these and also noting that the need for plots also includes those who have ceased to travel temporarily, therefore space required for storage and maintenance is likely to vary dependent on individual requirements.
- 8.28.12 The location is however indicative at this stage with flexibility available for delivery within the area annotated for ‘residential development’ within the land use parameter plan. Therefore, if the illustrative location is not considered suitable or amendment

required then this will be considered through detailed RMAs. Policy SL20 identifies the requirements for the imposition obligations to include an appropriate management agreement. This includes the phasing of site delivery and trigger points to secure early provision, proportionate to site delivery. The early submission of RMA for the showpeople plots will be necessary to ensure provision can be suitably accommodated within the site in advance or alongside the reserve matters applications for first phases of the dwellinghouses. The delivery of the Travelling showpeople plots is to be secure through S106 agreement.

8.29 **Education and Health**

8.29.1 Education

The new development will result in increased demand for school places. Surrey County Council anticipates that the quantum of development proposed would yield in the region of 850 school age children (across all age groups – early years, primary and secondary). The new development would not be large enough to sustain a secondary school on site, and this was recognised through the Local Plan site allocation process. A financial contribution will be required to support off site provision of secondary education infrastructure to support the provision of additional secondary places. Advice from Surrey County Council has been provided based on the predicted number of school children having regard to the indicative mix of housing proposed and policy requirements. The associated financial contributions would be reviewed and adjusted to reflect the final housing mix as agreed through RMAs.

8.29.2 The application includes the proposed provision of a 2-form entry (2FE) primary school together with early years provision, which accords with the requirements of Policy SD9. Provision is to be secured through s106 legal agreement including securing an appropriate phase/timescale for delivery in conjunction with the advice of Surrey County Council as Education Authority. The development is not considered to generate sufficient pupil numbers to support a 3FE school and therefore any additional provision required would be accommodated off site utilizing additional financial contributions.

8.29.3 The school provision is illustrated on the masterplan located close to the village centre, village green and community garden and appears well placed to serve the proposed development. Whilst illustrative to inform the outline planning application, the masterplan has been revised to have regard to recommendations from the Design Review Panel to have regard to accessible safe, direct and 'green' walking and cycling routes to the school. And to take account of the requirements of Surrey County Council. As a result, the illustrative masterplan provides a green corridor to the front of school which continues along the main internal access road, helping to support sustainable travel and health and wellbeing. The size of the school grounds has been enlarged since first submission providing a site area of at least 1.9 ha, which meets the requirements of SCC to accommodate facilities for a combined early years and primary provision. SCC are happy with the size and illustrative location of the school recognizing there will need to be dialogue with regards to the final design. The detailed design and layout would be secured through RMAs

8.29.4 The current strategy for the provision of outdoor sports relies on the dual use of the primary school playing fields to support the wider garden village development. This is currently shown to provide an artificial grass pitch (previously 3G proposed but revised to allow more flexibility of use to benefit school use), a mini soccer pitch, under-12 pitch and Multi Use Games Area (MUGA). A community use agreement will need to be

secured to understand and secure how sports facilities to be provided at the school can also be accessible and support the wider community. Dual use of school facilities are not uncommon and this will need to be worked up in conjunction with Surrey County Council to ensure such agreement does not undermine the function and has regard for the safeguard requirements of the school. This would form part of the S106.

8.29.5 Health care

The application has been reviewed by the NHS Surrey Heartlands Integrated Care Board (ICB) who are responsible for the provision of health services in the Surrey Heartlands area. They advise that having regard for to the proposed development and total population that is likely to be generated, collectively the GPs in the vicinity of the proposed development do not have capacity to absorb the additional population created by the proposed development. The ICB are seeking a site-specific contribution to mitigate the impact of the development. This would be either through the delivery of a 500 sq.m new healthcare facility provided on site, or through the provision of a financial contribution to support capacity off site alongside occupying part of a building. The ICB are regularly reviewing their estates strategy for the area and mindful of the build out period for the proposed development are seeking flexibility at this stage. This will be reflected and secured through the section 106.

8.30 **Police and Crime Prevention**

8.30.1 Surrey Police have advised that the large numbers of housing being developed across Surrey and more specifically within Runnymede will place a significant additional demand upon the police. In recognising that the proposed development will create a significant additional demand upon the police service that does not currently exist they are seeking a financial contribution to support the additional services in order to mitigate the impact of housing growth proposed by the development including provision of ANPR's in the locality of the site. In addition, having regard to the location of the development in terms of the nearest policing presence (with policing in Runnymede delivered from the civic offices in Addlestone), Surrey Police are also seeking the potential for provision of a policing presence within the community building of the Longcross garden village. This will be secured through the s106.

8.30.2 As this is an outline planning application the RMAs will be expected to demonstrate that safety and crime prevention has been considered and informs the layout and detailed design submitted through RMAs.

8.31 **Utilities**

8.31.1 The applicant has engaged in discussions with the various utility providers regarding electricity, gas, water and telecommunications, and this is reflected within the submitted utilities statement. Utility services continue to be delivered by the private sector. Developers may need to contribute directly to the private sector utility companies for connections or reinforcements to the network, but it is not the responsibility of the Borough Council. Utility providers have been engaged through the Local plan process in respect of site allocations and through consultation both pre and post application to enable any associated network reinforcements to be appropriately planned and timetabled. The applicant has been working with various utility providers in order to avoid potential limitations on occupation.

8.31.2 Water

Thames Water have identify the need for network reinforcement works. They have

advised that there is currently no capacity within the existing network and therefore network reinforcement will be required prior to any connection to the foul water system. Private wastewater management maybe need in the interim until the reinforcement works have been undertaken. Thames Water have confirmed that they have been in regular contact with the applicant to develop an appropriate foul water drainage strategy and to agree a phasing plan required to ensure capacity is provided for all phases of the development. This requires provision of a new pumping station close to the existing provision along Kitsmead Lane and connection to Chertsey Sewage Treatment Works.

8.31.3 In terms of water supply, Affinity Water has confirmed the need for off-site network reinforcement and have been developing a design for this. The utility statement considers a likelihood that in the region of 150 dwellings can be connected ahead of the off-site reinforcement being completed. This would therefore not prohibit the granting of outline planning permission. The Energy Statement which supports the application states that overall targets in water conservation will be achieved across the development in accordance with Policy SD7, as reiterated in Policy SD9. This will be a matter for consideration at detailed design stage through RMAs.

8.31.4 Electricity

The development is located on the border of two distribution network operators (Scottish & Southern Energy Network SSE and UK Power Networks. The supporting information identifies recent network improvement work undertaken by SSE to significantly increase capacity at the Primary sub-station within Longcross North which it reports will secure additional capacity for the whole of the Garden Village including Longcross South. This is expected to capture electricity demands for electrical vehicle charging infrastructure and additional requirements for electricity to allow the widespread deployment of heat pump led heating and hot water after 2025 in accordance with anticipated Future Homes Standard.

8.31.5 Gas

A point of connection has been identified with Cadent on the gas network which can supply the proposed development. The utilities statement recognizes that connection to the natural gas network, is a worst case scenario having regard to likely changes in Building Regulations to reduce use of gas. Energy use is considered further within the sustainability section of this report, but will also be addressed through detailed RMAs. For this outline application, the applicant has demonstrated the ability to secure energy sources to serve the proposed development.

8.31.6 Telecommunications (including 5G)

Openreach has confirm a full 'fibre to home' platform to supply the residential development proposed.

8.32 **Minerals and Waste**

8.32.1 The generation of waste has been considered through the demolition and construction phase as well as the operational phase of development. Supporting information illustrates no anticipated significant environmental effects resulting from either operational or construction waste across the proposed development. Policy 4 of the Surrey Waste Local Plan seeks to minimise waste arising from new development, this is further reflected within Policy SD7 (Sustainable Design) of the Local Plan which encourages the re-use of construction and demolition waste at source or recycled. Surrey as Waste and Minerals Planning Authority (WMPA) are seeking to ensure that

the development includes adequate facilities for waste storage and recycling. The most significant impacts will be during demolition/site clearance and earthworks, and construction activities. The reuse of earthworks/construction materials on site or reuse/recycling off site will help mitigate waste generation. It is proposed to reuse the majority of excavation waste on site.

8.32.2 Whilst the development would generally maintain land levels such that the site would maintain its overall topographical character, there would be areas of 'cut and fill' within the site in order to facilitate the proposed development. This is reflected within the existing and proposed site levels banding plans submitted to support the application. Reuse on site will assist in minimising off site waste and associated traffic movements. The application specifically identifies those areas in which no level changes are proposed which relates to the central SANG and area around Barrowhills house and Bowl Barrow Schedule Monument, and areas of existing trees to be retained. The changes are not considered to be significant and as such would not impact on the wider landscape views or visual impact of the development. Given the high level detail that is provided at this stage, details of existing and proposed levels will be expected to form part of reserve matters submission and will need to be carefully considered particularly in relation to impact on trees and habitat areas to be retained.

8.32.3 Whilst an adverse impact resulting from waste generated during demolition and construction phases has been identified, the harms identified are considered to be minor and could be mitigated by condition that seeks to secure measures to help minimize waste generation resulting from the development. Provision for the control and management of waste, during both construction and competition stage will be secured through condition and through detailed RMAs and this is also reflected within the measures outlined within the ES submitted.

8.32.4 Surrey MWPA have also identified that part of the site to the East of Kitsmead Lane to be used for sports pitches would extend to within a Minerals Safeguarding Area for concreting aggregate which the MWPA seeks to safeguard from development that would sterilize the underlying mineral resources. Given the small area affected and the use proposed, the MWPA have raised no objections as it is not considered that the proposed development would sterilize mineral resources or prejudice future mineral extraction.

8.32.5 **Trumps Farm**

To the east of the Kitsmead Lane is the operational Trumps Farm Anaerobic Digestion and Wood Drying and Pelleting facility. In addition, land adjacent to Trumps Farm is also allocated within the Surrey Waste Local Plan 2019 for waste management development, part of which benefits from consent for green waste bulking and transfer. Planning permission was granted in March 2023 for a Clinical Waste Energy Recovery Facility to the east of Kitsmead Lane, although this permission has not been implemented to date and a current planning application for an alternative use suggests that this use is not being pursued.

8.32.6 Surrey as Waste and Minerals Planning Authority (MWPA), whilst raising no objection to the application, do so subject to RBC being satisfied that the development incorporates appropriate mitigation to prevent unacceptable amenity impacts resulting from the existing and future waste management facilities and minerals workings in the area, having regard to the proximity of the application site to existing waste facilities at Trumps Farm. They are seeking to ensure the proposed development does not prejudice the operation of existing waste management facilities. Longcross Garden

Village forms a designated site within the Local Plan which was adopted in July 2020 and which is also a material consideration when considering proposals for the Trumps Farm waste and minerals site. Whilst it is recognized that the application will introduce new residential development as well as recreational uses in the locality, it is considered that there would also be good separation having regard to Kitsmead Lane and existing tree coverage/screening between the waste facilities and nearest residential properties. It is not considered that the existing waste and mineral site would form a barrier to the development of the Longcross Garden Village site.

8.33 **Residential Amenity**

- 8.33.1 Whilst the site is generally well contained there are a number of residential properties which lie adjacent or close to the application site boundaries. This includes residential roads/cul-de-sacs to the south, accessed off Longcross Road, including Longcross Road, Tanglewood Close, Holly Close and Albany and existing properties off Kitsmead Lane. Whilst it is recognised that the impact of traffic movement can have a wider impact on the highway network and residential amenity, this is considered further above with various off site highway works proposed to mitigate the impact of traffic generation.
- 8.33.2 The M3 affords good separation with the existing dwellings north of the M3, and as such the principle of delivering development of up to 4 stories and apartments along this northern edge could be accommodated within harm to residential amenity. The illustrative masterplan layout demonstrates that the quantum of development can be designed to have regard to existing neighbouring properties and maintain appropriate separation distances with the detailed design will be considered through RMAs which will need to ensure compliance with planning policy, including Policy EE1. There is no obvious reason why suitable residential amenity could not be achieved for existing and proposed dwellings by using good design. Furthermore, conditions including CEMP will help to minimise impact during construction works.

8.34 **Land East of Kitsmead Lane**

- 8.34.1 Whilst the main site area falls within the urban area, the parcel of land to the east of Kitsmead Lane remains within the Green Belt and is currently an arable field. The proposals for the use of this parcel of land as reflected within the parameter plans need to be considered against green belt policy. The land use parameter plans propose the use of the land for 'formal sports and other green infrastructure'. The site is proposed to provide sports pitches (to accommodate junior and senior football pitches and 8 wicket cricket pitch) and allotments, but also incorporates sports pavilion, site access, parking, pedestrian and cycle links to support this use, as well provision to accommodate a pumping station as part of the wider foul water upgrade provisions, drainage and associated infrastructure. The uses proposed are considered compliant with Policy SD9, this parcel also forms part of the Longcross Garden Village site allocation which proposes use for 'publicly accessible open space'.
- 8.34.2 The use of the land as allotments would constitute appropriate development within the green belt. Material changes in the use of land for outdoor sport and recreation are considered to be appropriate within the green belt provided the openness of the Green belt is maintained. This is also the case for the provision of appropriate facilities in connection with a change of use for outdoor sport, recreation and allotments, as long as

the facilities preserve the openness of the Green Belt and do not conflict with the purposes of including land within it. The sports pavilion would provide changing facilities to support the sports use of the site and would also include facilities to support the allotment use (i.e. toilet and tea making facilities). The building would be modest in size and limited to single storey although the details of the building (including height, design and scale etc) would be considered through RMAs and would need to demonstrate compliance with policy. However, the principle of the proposed uses and supporting building is considered to constitute appropriate development within the green belt that is capable of preserving openness subject to detailed design through RMAs. It is understood the pumping station and drainage works would primarily be below ground, but would be assessed through detailed RMAs.

8.34.3 The size of this parcel (approximately 2.85 hectares) presents some challenges in providing appropriate layout for the size of pitches proposed, allotments together with supporting pavilion and car parking (and potential requirements for foul water upgrade/provision). The space illustrated for allotments as shown on the illustrative masterplan layout suggests that the space could potentially cater for the equivalent of 8 to 10 full size plots. Whilst there is opportunity to accommodate other forms of food production such as community gardens and orchards as well as allotment provision within the main southern site as proposed as part of the open space strategy and reflected within the masterplan layout. There are some concerns that the limited quantum or area available may limit the desirability and practical function as allotments. This is acknowledged and there is an expectation that the rest of the development will provide a good range and siting of alternative provision including community growing gardens. In addition, the space needed to accommodate both senior and junior football pitch and cricket wicket will present challenges to afford space to site boundaries and between various uses, the allotments, pavilion and car park, and boundary trees and therefore there is likely to be compromises required when considering the detailed design and layout. Each phase of development will be expected to demonstrate consideration and provision of food production appropriate to that phase.

8.34.4 To the north the boundary abuts the residential curtilage of the neighboring property Chevythorn, a detached dwellinghouse located close to the road frontage and approximately 30m from the boundary of the application parcel east of Kitsmead Lane. It is recognized that the change in the use of the land and associated activity has the potential to impact on neighbouring residential amenity, particularly with reference to noise and disturbance. However, it is considered that an appropriate separation can be maintained to neighbouring properties and the layout designed to maintain existing boundary trees and screening. Details would be considered through reserved matters including siting of the pavilion and car parking together with any external lighting requirements which can be controlled through condition which would help ensure residential amenity is appropriately protected.

8.35 **Garden Village Principles/ Community Stewardship**

8.35.1 The long term management and maintenance reflected within an appropriate community stewardship strategy is an important component in securing a new community reflective of Garden Village principles and the Local Plan site allocation. This includes on-going stewardship of the Garden Village, its community assets including play areas, sports provision, open spaces and allotments, and community buildings. Whilst a Stewardship Strategy has been submitted to support the application. This details the intention to incorporate a Residents Management Company to take responsibility and ownership of the open spaces and community facilities with a

Residents Steering Group and community development worker. Officers have some reservations regarding the stewardship strategy as currently proposed. This will need to be developed further and will be secured through Condition and/or S106 obligations.

9. PLANNING OBLIGATIONS, CIL & CONDITIONS

9.1 CIL

The Runnymede CIL was adopted by the Council on 1st March 2021. However, the Longcross Garden Village site has been expressly excluded from CIL in favour of project-specific s106 planning obligations.

9.2 Section 106 Obligations

Planning obligations will continue to be negotiated in accordance with policy requirements and consultee responses. It is likely to take some time for the legal agreement to be fully resolved. It is anticipated that during this time some matters may switch between inclusion within either the s106 or through planning conditions, altering the mechanism in which to secure the associated matter. It is possible therefore that the below s106 headings could change either due to negotiation with consultees or at the agreement of the HoP. Draft Planning Obligations include but are not limited to:

- a. Policy compliant Affordable housing
- b. Travelling Show person plots
- c. Public Transport - Rail services and Rail Infrastructure Improvements
- d. Public Transport – Public Transport Strategy - Bus Services
- e. Travel Plan, Sustainable Travel Vouchers, car club, mobility hubs
- f. Off site Highway works
- g. A320 works contribution
- h. M3 Accommodation Bridge
- i. Community stewardship
- j. Education – on-site primarily school and early years provision, and financial contributions
- k. Police – provisions for on site sub-office and financial contributions
- l. Healthcare – either on site or financial contributions
- m. SANG and SAMM
- n. Blue and Green Infrastructure
- o. Outdoor sport, recreation and food production
- p. Retail/mixed use marketing

- q. Phasing
- r. Monitoring fee

9.3 **Conditions:**

Draft indicative conditions are included at **appendix one**. As set out above there are matters which are likely to move between S106 obligation and condition, and amended or further conditions that may well arise as a result of discussion with the applicant or consultees.

9.4 **Note on Conditions and Planning obligations**

Whilst the recommendation delegates the finalization of negotiation of conditions and obligations to the Head of Planning, the HoP reserves the right to return any of these matters to the committee at his discretion. This may be because he deems it to be appropriate, that he wishes to receive members sign off or endorsement on a particular issue, or that negotiations are not progressing to his satisfaction.

10. **EQUALITY AND HUMAN RIGHTS CONSIDERATIONS**

- 10.1 Consideration has been given to Articles 1 and 8 of the First Protocol of the European Convention on Human Rights. It is not considered that the decision would result in a violation of any person's rights under the Convention.

Consideration has been given to s149 of the Equality Act 2010 (as amended), which has imposes a public sector equality duty that requires a public authority in the exercise of its functions to have due regard to the need to:

- (a) Eliminate discrimination, harassment, victimisation and any other conduct prohibited by the Act
- (b) Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
- (c) Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

It is considered that the decision would have regard to this duty.

11. **CONCLUSIONS & PLANNING BALANCE**

- 11.1 The application seeks outline approval for the development of a mixed use garden village development consisting of up to 1700 residential dwellings and land reserved for Travelling showperson plots, and up to 9,556 sq.m of non residential uses, including primary school and nursery, and retail, business and community floorspace. The site forms an allocated site within the Local Plan under Policy SD9. The application is consistent with the requirements of the Local Plan site allocation and as such the principle of the development is considered acceptable. All matters are reserved for subsequent approval with the exception of 5 points of access into the site. Surrey County Highways Authority consider these points of access to be acceptable.
- 11.2 The application has considered the impact of the development including traffic generation and includes appropriate mitigation measures. This includes proposals for off site highway

works and contributions towards A320 works. Subject to a package of highway works and improvements secured by legal agreement, it is considered that the local highway network and accesses could safely accommodate the extra traffic. The application includes a range of sustainable transport measures, including enhanced cycle routes and pedestrian footways to improve wider connectivity. The application will support improvements to facilities and accessibility of Longcross Station as well as supporting an enhanced stopping service, as well as supporting public bus service provision to service the new Garden Village. These measures will have wider benefits beyond the site boundaries.

- 11.3 The application has been submitted with an Environmental Statement in accordance with relevant legislation. This has not identified any significant environmental effects that could not mitigated or reduced through mitigation.
- 11.4 The development will provide a significant proportion of the Boroughs housing requirements as identified over the local plan period contributing to housing need including affordable housing and travelling showperson plots reflecting policy requirements. Significant weight is attributed to this. The application includes a range of community facilities including a new primary school with early years provision, as well as employment land and supporting commercial and community uses. The provision of green infrastructure including on and off-site site SANG, formal and informal sports provision and equipped play areas will be publicly accessible and as such provide wider benefits. The application provides public access to the site which is currently not available enabling a wider appreciation of existing heritage assets (Barrowhill House Grade II Listed Building, Broze age Bowl Barrow Scheduled Monument and non-listed Multi Gradient Vehicular drop). These are considered benefits which weigh in favour of the proposed development.
- 11.5 Whilst it is recognised that there will be some negative impacts or harms resulting from the development, which have been considered within the report above. This includes the loss of trees and habitat requiring mitigation, potential limitations of the space available on the eastern side of Kitsmead Lane and potential impacts of noise requiring careful consideration of layout and design as part of RMAs. Mitigation measures, with controls through planning conditions have sought to minimise impacts such as through measures secured through a CEMP to minimise and mitigate harmful impacts associated with construction phase. This is an allocated site within the Local plan and the substantial benefits of the scheme are material considerations. The application is proposed in outline which seeks to establish the principle of the quantum of development proposed (in addition to the details of the 5 proposed points of access), and therefore the detailed design and layout will form reserved matters for future consideration.

These are balanced against cumulative benefits of the development which are significant and weigh heavily in favour of the proposed development.

- 11.6 The development has been assessed against Development Plan, including the Runnymede 2030 Local Plan, the policies of the NPPF, guidance in the PPG, and other material considerations including third party representations. It has been concluded that the development would not result in any harm that would justify refusal in the public interest. The decision has been taken in compliance with the requirement of the NPPF to foster the delivery of sustainable development in a positive and proactive manner.

12. FORMAL OFFICER RECOMMENDATION

It is recommended the Planning Committee authorises the HoP:

A To approve the application subject to the negotiation and completion of a S106

agreement and relevant planning conditions to his satisfaction (as detailed in part 9 of the report and indicatively set out at appendix one)

OR

- B To refuse planning permission at the discretion of the HoP should the S106 not progress to his satisfaction or if any other material planning matters arise prior to the issuing of the decision that in the opinion of the HoP would warrant the refusal of the application**

Appendix 1:

As set out at part 9 of the report draft indicative conditions are detailed below. As set out above there are matters which are likely to move between S106 obligation and condition, and amended or further conditions that may well arise as a result of discussion with the applicant or consultees.

Draft Indicative conditions:

01 Outline application (standard time limit)

Before commencement of any part of the development under this permission details of the appearance, layout and scale, the access (internal access) and the landscaping (hereafter referred to as the 'reserved matters') for each phase or sub phase of the development including for the relevant part of the site access and any related engineering works, shall be submitted to and approved in writing by the Local Planning Authority before any development is commenced, and shall be carried out as approved.

Reason: To comply with Section 51 of Part 4 of the Planning and Compulsory Purchase Act 2004.

02 Outline application (reserved matters standard time limit)

(a) Application for approval of the first phase reserved matters referred to in Condition 1 shall be made to the Local Planning Authority before the expiration of two years from the date of this permission.

(b) Reserved matters for subsequent phases of the development shall be made to the Local Planning Authority no later than two years from the date of approval of the previous reserved matter application and not later than 10 years from the date of this permission.

(c) The development hereby permitted shall commence either before the expiration of five years from the date of this permission, or before the expiration of two years from the date of approval of the reserved matters for that first phase of the development, whichever is the later.

All subsequent applications for approval of reserved matters shall be made to the LPA before the expiration of ten years from the date of this permission.

Reason: To comply with Section 51 of Part 4 of the Planning and Compulsory Purchase Act 2004 and to recognise the scale of the development and the need to develop the site in phases.

03 Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the location plan and parameter plans (received 2nd November 2023) as listed below:

Application Boundary Plan - P20-0942_23 Rev H

Parameter Plans - Land Use (P20-0943_08.2 Rev. AF)

Parameter Plan – Green Infrastructure (P20-0943_29M)

Parameter Plan – Access and Movement (P20-0943_28P)

Parameter Plan – Demolition Plan (P20-0943_26F)

Reason: To ensure high quality design and to ensure compliance with the policies of the of the Runnymede 2030 Local Plan including Policy SD9 and guidance in the NPPF.

04 Phasing Plan

Prior to the submission of the first reserved matters application, a phasing plan shall be submitted to and agreed in writing with the Local Planning Authority. The Phasing Plan shall include details of the location of the phases of the development and a programme of phasing for the implementation of the development. The phasing plan shall also include delivery/construction of key infrastructure including the vehicular access points and main internal road links, accommodation bridge, travelling show people plots, school, village centre, areas of open space, sport facilities and SANG, public house, utilities and enabling work including drainage infrastructure.

The development shall be carried out in accordance with the approved phasing plan.

Reason: To provide clarity to the phased delivery of the development and ensure the delivery of appropriate land use and infrastructure and in the interests of sustainable development and delivery of the Garden Village compliant with the Runnymede Borough Local Plan 2030 including Policy SD9 to accord with the NPPF.

05 Reserved matters

All applications for reserved matters in respect of each phase of development shall be made in substantial accordance with the Site-Wide Phasing Strategy pursuant to condition 4

Reason: To secure an appropriate sequence and timeframe for the delivery of land uses and infrastructure to ensure a sustainable development which accords with the Runnymede Borough Local Plan 2030 including Policy SD9 to accord with the NPPF.

06. Reserved matters

All applications for reserved matters in respect of each phase of development shall be made in accordance with the approved Design Codes and neighbourhood masterplans pursuant to Condition 13 and 14.

Reason: To secure a sustainable high quality Garden Village development which accords with the Runnymede Borough Local Plan 2030 including Policy SD9 to accord with the NPPF.

07 Maximum quantum of development

Subject to the approval of all applications for detailed reserved matters pursuant to Condition 2 above, and having regard to the Environmental Statement and parameter plans approved, this consent allows for a maximum number of 1,700 residential dwellings (Class C3) and a Class C2 care home.

Reason: To secure a high quality development and ensure compliance with the approved parameter plans and information submitted to support the application including Environmental Statement, and secure compliance with the Runnymede Borough Local Plan 2030 including Policy SD9 to accord with the NPPF.

08 Travelling show person plots

Development shall provide a minimum of 10 Travelling show person plots, to be built with a minimum size of a quarter of an acre per plot and reflective of the requirements set out Design Guide SPD, including suitable access to the public highway and to enable large vehicles to manoeuvre within the site.

Reason: To secure the delivery of Travelling Show person plots, to accord with policies SL22 and SD9 of the Runnymede Borough Local Plan 2030 and the NPPF.

09 Community facilities provision

Reserved Matters Application to provide adequate community facilities to provide for the sustainability of the settlement, the amount of floor space and development provided shall be in close accordance with the quantum's set out in the outline application which are:

- Around 9,556 sq.m of non-residential uses including:
 - Circa 1,000 sq.m retail, food and drink (Class E and F2)
 - Circa 770 sq.m for a public house (sui-generis use)
 - Circa 1,500 sq.m flexible community facilities (class E, F1 and F2)
 - Circa 300 sq.m employment uses (class E)
 - A 2 FE Primary School including early years provision (use Class F1)
 - public open space including allotments, sports pitches and ancillary facilities (Use Class F2), Suitable Alternative Natural Greenspace (SANG) (Use Class F2)
 - No less than 40% of the site shall be for green infrastructure

The space provided shall be provided in accordance with the phasing plan.

Reason: To accord with the terms of the application, having regard to the Environmental Statement, and to ensure an appropriate range of land uses to accord with the policies of the Runnymede 2030 Local Plan and specifically with reference to Policy SD9, and guidance within the NPPF.

10 Public House provision

The public house shall be provided in the location illustrated on the 'Land use' parameter plan to be used for public house. Should the land not be used for a public house or alternate food and drink venue then the land indicated for the public house shall be incorporated into the open space.

Reason: The location for the public house was agreed for this prominent location due to the benefits of it being linked to the open space. The visual intrusion across the open space would be outweighed by the benefits of having such a facility in such a location.

11 Site Accesses

No part of the development shall be first occupied unless and until the proposed access to the relevant part of the site has been constructed and provided in general accordance with the drawing numbers 2204410-SK016B, 2204410-SK049A, 2204410-SK050A, 2204410-18E, 2204410-19C with visibility zones and thereafter the visibility zones shall be kept permanently clear of any obstruction over 0.6m high. The above drawing numbers provide three vehicular accesses to Longcross Road, and the two vehicular accesses to Kitsmead Lane. The above works shall be carried out under S278 of the Highways Act 1980 at developers' expense and will be subject to a detailed design and stakeholder engagement exercise. Any necessary supporting additional traffic management measures or other supporting measures for these junction works shall also be provided at developers' expense.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users, and to comply with Policy SD4 of the Runnymede 2030 Local Plan and guidance within the NPPF and Surrey County Councils Local Transport Plan.

12 Bridge Improvement, Management and Maintenance Strategy

Prior to the submission of any reserved matters application, a Bridge Improvement, Management and Maintenance Strategy shall be submitted to the local planning authority for approval in respect of the M3 Accommodation Bridge which links the application site to the northern site, such strategy to address the following matters:

- (i) Structural and landscaping improvements to be undertaken to the bridge;
- (ii) Carriageway layouts for vehicles, cycles and pedestrians
- (iii) Priority Movement measures favouring non-car modes
- (iv) Implementation strategy and timing,
- (v) Timescales/triggers for public use by pedestrians, cyclists and vehicles
- (vi) On-going management and maintenance measures

Development to be carried out in accordance with the approved strategy.

Reason: To encourage active and sustainable travel, enabling direct access to Longcross Railway station and between the northern and southern sites to form a cohesive Garden Village to accord with Policy SD9,SD3, SD4 and SL1 policies of the Runnymede 2030 Local Plan and in order that the development should meet the objectives of National Planning Policy Framework and Surrey County Councils Local Transport Plan.

13 Design Codes – Site Wide

A site wide Design Codes shall be submitted to and approved in writing by the Local Planning Authority before the approval of the first reserved matters under Condition 2 for any buildings. The Design Codes shall have regard to the approved parameter plans, the site-wide strategies submitted to support the application and Site-wide Illustrative Masterplan.

The Design Codes shall address the following:

- i) Differing character or neighbourhood areas

- ii) Landmark buildings and view/focal points
- iii) Buildings forms and density
- iv) Architectural style and treatment
- v) Heights
- vi) Treatment of highways;
- vii) Building materials palette
- viii) Surface materials palette;
- ix) Street furniture and design and lighting design;
- x) Soft landscape;

All reserved matters submissions must demonstrate compliance with the approved Design Code.

Reason: To ensure high quality and sustainable Garden Village and to comply with Policy EE1 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

14 Design Codes – Neighbourhood Master Plans Design Code

Prior to or alongside any reserved matters application being made pursuant to Condition 2, a Neighbourhood Master Plan and Design Code (NMDC), shall be submitted to and approved by the local planning authority for the Neighbourhood to which the proposed application for reserved matters approval relates. The NMDC shall be supported by appropriate drawings and a written statement and shall include consideration and detail of the following issues (not excluding other site specific issues that are identified) for that Neighbourhood and how these fit within, and connect with, the wider site having regard to other approved NMDCs, reserved matters and physical development as may exist at that time and policy requirements:

Land Uses

- (a) Land uses and maximum floor space Neighbourhoods (including floorspace to be provided for retail uses);
- (b) Indication of affordable housing locations, and opportunities for custom or self-build housing;
- (c) Land set aside for non-residential and commercial uses (such as offices, education and community uses) and open spaces and site infrastructure, including drainage, food production and sporting facilities;

Phasing

- (d) General sub-neighbourhood phasing to include confirmation of where development is likely to start, when bus stops are to be provided; when non-residential land uses (including open spaces) are to be provided; highway and utilities infrastructure and public art provision,

Levels

- (a) Levels plan(s) to show details of final ground levels and interfaces with existing ground levels, to include Neighbourhood-wide cross section plans;

Trees and ecology

- (b) Identification of trees and woodland to be retained
- (c) Updated ecological surveys and where appropriate mitigation measures

Archaeology and Heritage Trail

- (f) Identification of heritage assets, assessment of archaeological remains together with any associated mitigation required, including public art/ heritage trail information.

Form of development and design coding

- (g) Extent of development Neighbourhoods and density
- (h) Building height ranges
- (i) Location of landmark buildings and features, including public art
- (j) Cross sections and perspectives of key streets, buildings and open spaces including adjacent Neighbourhoods (as built or as completed) where necessary in order to facilitate integration.

- (k) Boundary treatment details to the Neighbourhood - existing and proposed treatments
- (l) Design Codes that address:
 - i. Architectural style and treatment
 - ii. Treatment of public highways
 - iii. Building materials pallet
 - iv. Surface materials pallet
 - v. Street furniture and design, including lighting design and street signage
 - vi. Soft landscaping
 - vii. Frontage types
 - viii. Building form and height
 - ix. Means of enclosure

Access and car parking

- (m) Access routes (vehicular, public transport, pedestrian and cycle) and connections to surroundings Neighbourhoods and when these will become available for use;
- (n) Indication of the location of permissive ways which form connections with the boundary of the site, existing public rights of way, heritage trail and key landscape features;
- (o) Car parking quantum's, typologies, distribution and management measures;

Utilities Statement and Energy

- (p) Identification of existing and proposed primary utility corridors below and above ground
- (q) Measures to deliver sustainable design, renewable and low carbon energy in accordance with policies SD7, SD8 and SD9 of the adopted Runnymede Local Plan 2030.

Community Development

- (s) Buildings, other assets and infrastructure intended for community stewardship purposes.

Development Cessation

- (t) Actions in the event of un-planned or unexpected development cessation shall be stipulated, to ensure an acceptable living and movement environment for existing site residents.

Reason: To ensure high quality and sustainable Garden Village and to comply with Policies of the Runnymede 2030 Local Plan including Policy EE1 and SD9 and guidance within the NPPF.

15 Self and custom build plots

Each reserved matters application which includes residential development will need to demonstrate consideration of the feasibility of the provision/delivery of custom and/or self-build plots, which has regard to Policy SL24 of the Local Plan and takes account of current demand, having regard to Councils Self-build and custom housebuilding register.

Reason: To ensure a wide choice of high quality homes and to comply with Policy SL24 and SD9 of the Runnymede 2030 Local Plan and guidance in the NPPF.

16 Tree Protection

Prior to the commencement of any works hereby approved and pursuant to any reserved matters application approval, including site clearance, demolition and any enabling works, and before any equipment, machinery or materials are brought on to the site, a Tree Protection Plan and method statement shall be submitted to the Local Planning Authority for approval and then subsequently approved tree protective measures shall be installed in accordance with the approved details. The plan shall have regard to the trees, groups of trees and woodland areas identified for retention under Condition 14 above.

The works shall be carried out in accordance with the approved protection plan and method statement. The protective measures shall remain in place until all works are complete and all machinery and materials have finally left site (for the associated RMA). Nothing shall be stored or placed in any area fenced in accordance with this condition, nor shall any fires be started, no tipping, refuelling, disposal of solvents or cement mixing carried out and ground levels within those areas shall not be altered, nor shall any excavation or vehicular access, other than that detailed within the approved plans, be made without the written consent of the LPA.

There shall be no burning within six metres of the canopy of any retained tree(s). Where the approved protective measures and methods are not employed or are inadequately employed or any other requirements of this condition are not adhered to, remediation measures, to a specification agreed in writing by the LPA, shall take place prior to first occupation of the development, unless the LPA gives written consent to any variation.

Reason: To protect the trees to be retained, enhance the appearance and biodiversity of the surrounding area and to comply with Policies EE1, EE9, EE11 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

17 Drainage (LLFA)

Each phase of the development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

a) Evidence that the proposed final solution will effectively manage the 1 in 30 (+35% allowance for climate change) & 1 in 100 (+45% allowance for climate change) storm events, during all stages of the development.

i. The main highway drainage and site wide infrastructure SuDS features. The final solution should follow the principles set out in the approved drainage strategy. Associated discharge rates and storage volumes shall be provided using maximum staged discharge rates as set out in Table 4 (Drainage Strategy Statement – Revision 1.7, Calcinotto, July 2023, reference: 114505).

ii. Each Individual Phase: Associated discharge rates and storage volumes shall be provided using a maximum discharge rate (litres/sec/ha rate) applied to the positively drained areas of the site as set out in Table 12 (Drainage Strategy Statement – Revision 1.7, Calcinotto, July 2023, reference: 114505). Each phase must include multifunctional sustainable drainage systems in addition to the main site wide infrastructure SuDS features.

b) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.).

c) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.

d) Details of drainage management responsibilities and maintenance regimes for the drainage system.

e) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason: To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site and to comply with policies SD9 and EE13 of the Local Plan.

18 Drainage – Verification (LLFA)

Prior to the first occupation of each phase of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority for the associated phase of development. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the Drainage System is constructed to the National Non-Statutory Technical Standards for SuDS and to comply with policies SD9 and EE13 of the Local Plan.

19 Contamination (Controlled Waters) (Environment Agency)

No development approved by this planning permission shall commence until a remediation strategy to deal with the risks associated with contamination of the site has been submitted to, and approved in writing by, the Local Planning Authority. This strategy will include the following components:

1. A site investigation scheme, based on the preliminary risk assessment, to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.
2. The results of the site investigation and the detailed risk assessment referred to in (1) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.
3. A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy in (2) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the written consent of the local planning authority. The scheme shall be implemented as approved.

Reason: To prevent deterioration of groundwater quality. This condition is supported by Policy EE2: Environmental Protection of the Runnymede Local Plan 2030 and paragraph 121 and 183 of the NPPF.

20 Contamination – Verification (controlled waters)

Prior to the first occupation or use of the associated phase or sub phase of development (any part of the permitted development being occupied) a verification report demonstrating the completion of works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to, and approved in writing, by the local planning authority for the associated phase or sub phase of development. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met.

Reason: To prevent deterioration of groundwater quality. This condition is supported by Policy EE2: Environmental Protection of the Runnymede Local Plan 2030 and paragraph 121 and 183 of the NPPF.

21 Contamination – Infiltration (Environment Agency)

In accordance with paragraph 174 of the NPPF, no infiltration of surface water drainage into the ground is permitted other than with the written consent of the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason: To prevent deterioration of groundwater quality. This condition is supported by Policy EE2: Environmental Protection of the Runnymede Local Plan 2030 and paragraph 183 of the NPPF.

22 Drainage - Foul Water (Thames Water)

There shall be no occupation beyond the 16th dwelling until confirmation has been provided that either:-

1. All foul water network upgrades required to accommodate the additional flows from the development have been completed; or
2. A development and infrastructure phasing plan has been agreed with Thames Water to allow additional development to be occupied.

Where a development and infrastructure phasing plan is agreed, no occupation of those additional dwellings shall take place other than in accordance with the agreed development and infrastructure phasing plan.

Reason: Network reinforcement works are required to accommodate the proposed development. Any reinforcement works identified will be necessary in order to ensure sufficient capacity for foul drainage and avoid sewage flooding and/or potential pollution incidents, and to accord with policies in the Runnymede Local Plan 2030 and NPPF.

23 Construction Environmental Management Plan (CEMP)

No development shall take place (including any advance infrastructure, site preparation, groundworks demolition or trial trenching) until details of a Construction Environment Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority for each phase or reserved matters application including in respect of site clearance, preparation and infrastructure/utilities works. The CEMP shall include, but not be limited to the following, and shall have regard to the recommendations contained within the Environmental Statement:

- The proposed construction traffic routes to the site, to be identified on a plan;

- Construction Traffic Management Plan (to include the co-ordination of deliveries and plant and materials and the disposing of waste resulting from demolition and/or construction so as to avoid undue interference with the operation of the public highway, particularly during the Monday-Friday AM Peak (0800-0900) and PM Peak (1630-1800) periods, an estimate of the hourly and daily movement of the construction traffic, the hours of construction work and deliveries);
- Construction Logistics Plan and Construction Phase Travel Plan
- Area(s) for the parking of vehicles of site operatives and visitors;
- Area(s) for the loading and unloading of plant and materials;
- Site office locations and area(s) for the storage of plant and materials to be used in constructing the development;
- Details for the management and storage of materials and construction waste including a scheme for waste recycling;
- Construction Dust Management Plan including wheel washing measures to control the emission of dust and dirt during construction including on the public highway;
- Details to control pollution including water pollution prevention
- Management strategies and mechanisms to deal with environmental impacts such as emissions to air, water and land, Including noise & vibration, air quality and dust, light, odour and general discharges and appropriate mitigation strategies;
- Dust Management Plan
- Noise management plan – including measures to monitor and mitigate noise levels during demolition and construction phases
- Lighting Strategy
- Measures for protection of key habitats and for protected species
- In conjunction with the above, to include measures to avoid or reduce adverse impacts to sensitive receptors during construction including protection of existing habitats to be retained
- Risk Assessments and Method Statements for the works; and contact details of personnel responsible for the construction works.

The approved CEMP shall be adhered to at all times during the construction of the development.

Reason: To mitigate any adverse impact from the construction phase of the development on the strategic road network to ensure that it continues to be an effective part of the national system of routes for through traffic in accordance with section 10 of the Highways Act 1980 and to satisfy the reasonable requirements of road safety, and to safeguard residential amenity and to protect ecology and biodiversity during site preparation, demolition and construction works. To accord with the policies within the Runnymede 2030 Local Plan and mitigation measures identified through the Environmental Assessment, and the NPPF.

24 Construction Transport Management Plan (CTMP) (CHA)

No development shall commence until a Construction Transport Management Plan to include details of:

- (a) parking for vehicles of site personnel, operatives and visitors
- (b) loading and unloading of plant and materials

- (c) storage of plant and materials
- (d) programme of works (including measures for traffic management)
- (e) provision of boundary hoarding behind any visibility zones and details of maintenance
- (f) HGV deliveries and hours of operation
- (g) vehicle routing to and from the Strategic Highway Network
- (h) measures to prevent the deposit of materials on the highway
- (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
- (j) on-site turning for construction vehicles,

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development. For the avoidance of doubt, the planning application is not currently at the stage whereby appropriate construction vehicle routing and volumes can be determined and this will therefore be determined via the above CTMP submission in association with a full construction phasing plan.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policy SD4 of the Runnymede 2030 Local Plan and guidance within the NPPF and Surrey County Councils Local Transport Plan.

25 Site Waste Management Plan

Each reserved matters application shall include a Site Waste management plan to be submitted for approval to reduce waste arising from demolition and construction phase, to provide for the efficient use of minerals and encourage the re-use of construction and demolition waste at source or its separation and collection for recycling and detailing how hazardous waste (where applicable) will be managed/handled. The development shall be carried out fully in accordance with the approved details.

Reason: In order to achieve sustainable development and to comply with Policy EE2, SD7 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF

26 Landscape and Biodiversity Management Plan

A detailed Landscape and Biodiversity Management Plan shall be submitted with each reserved matters application which shall have regard to the Environmental Statement, and shall set out how existing and proposed landscape, trees, habitat and ecological protection, mitigation, compensation and enhancement measures relating to the proposal will be implemented, managed and monitored. The development shall be implemented in accordance with the details as approved.

Reason: To preserve and enhance the biodiversity of the site, to secure a high quality development and to comply with Policies EE1, EE9, EE11, EE12 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

27 Biodiversity Net Gain

Prior to or alongside the submission of the first phase reserved matters application an 'Overall Biodiversity Gain Plan' shall be submitted to and approved in writing by the LPA, this shall inform a Landscape and Biodiversity Management Plan (LBMP) which shall be submitted for approval with each RMA and shall include a 'Phase Biodiversity Gain Plan' which sets out the Phases contribution to biodiversity net gain and confirm the progress towards the overall biodiversity gain objective, together with a 'Woodland Management Plan' to demonstrate sufficient compensation and mitigation, and including management of tree and woodland belts and to protect, manage and the enhance site for nature conservation. The Biodiversity Gain Plan and each Phase Biodiversity Gain Plan should be based upon valid habitat survey data, but also reflect the baseline biodiversity unit value set out by the Biodiversity Metric Calculation Tool Version 13.

No development shall commencement within the associated phase until the Overall Biodiversity Gain Plan and Landscape and Biodiversity Management Plan has been approved and development shall be implemented in accordance with the approved details and timescales.

Reason: To preserve and enhance the biodiversity of the site and to comply with Policies EE9, EE11, EE12 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

28 Grassland mitigation

No development shall take place (including any advance infrastructure, site preparation, groundworks or demolition) until details of a Lowland dry acid grassland mitigation strategy and incorporating a Translocation Method Statement has been submitted to and approved in writing by the Local Planning Authority. This shall identify those areas within the site to be retain or for translocation off site and include (but not limited to) associated measures for protection during construction, measures for translocation, including aftercare, management, maintenance and monitoring. Development shall be undertaken in full accordance with the details as approved.

Reason: To secure appropriate protection and mitigation having regard to important ecological habitats and to comply with Policies EE9, EE11, EE12 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

29 SANG (phasing plan)

Prior to or alongside the submission of the first reserved matters application for residential, a phasing plan for the delivery of Suitable Alternative Natural Greenspace (SANG) which shall be delivered in accordance with the submitted and approved Chertsey Common Phase 2 SANG Delivery and Management Plan Rev B (September 2023) and Barrowhills – SANG Delivery and Management Plan Rev B (October 2023), and shall include a time frame for delivery relative to the proposed housing, shall be submitted to and approved in writing by the Local Planning Authority. Each phase or reserved matters application that includes residential use shall demonstrate the delivery of an appropriate quantum of SANG. No residential use associated with that phase shall be occupied unless and until the associated SANG provision has been delivered and is available for public use in accordance with the associated SANG Delivery and Management Plan.

Reason: To ensure that the development, either on its own or in combination with other plans or projects, does not have a significant adverse effect on a European site within the Conservation of Habitats and Species Regulations 2010 and to comply with Policy EE10 and SD9 of the Runnymede 2030 Local Plan and guidance in the NPPF.

30 Heritage - WSI

No development shall take place until the applicant has secured the implementation of a programme of archaeological work in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the Planning Authority

Reason: To allow archaeological information to be recorded and to comply with Policy EE7 of the Runnymede 2030 Local Plan and guidance within the NPPF.

31 Heritage – Historic Building Recording

No development shall take place (including demolition and site clearance) until the applicant has secured the implementation of a programme of Historic Building Recording to Level III as set out in *Understanding Historic Buildings: A Guide to Good Recording Practice, Historic England 2016* and in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the Planning Authority which has regard to listed and non-listed heritage assets.

Reason: To secure a record of heritage assets (including non-listed) and to comply with Policy EE4 and EE8 of the Runnymede 2030 Local Plan and guidance within the NPPF.

32 Heritage - Scheme for the interpretation of designated and non-designated assets

No development shall take place (including demolition and site clearance until a scheme has been submitted to and approved in writing by the Local Planning Authority for the interpretation of designated and non-designated heritage assets, including details of how this will be implemented through the phases of the development.

Reason: To secure an appropriate interpretation of heritage assets which reflect the diversity of the sites heritage to be incorporated and inform the design of the new development and to comply with Policy EE3, EE4, EE7, EE8 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

33 Heritage - Scheduled Ancient Monument

Prior to the public access of the Barrowhills on site SANG and/or access to the Scheduled Ancient Monument (SAM), details of protection measures (to include low but robust natural barrier between open space/path and barrow), management plan of the SAM (to ensure vegetation growth on the barrow is maintained in order to preserve its legibility and archaeological integrity). It should be noted that vegetation management in the scheduled area will require Scheduled Monument Consent (SMC), and the provision of interpretation to be provided close to the barrow (ideally along the path in the public open space to the north) which would provide visitors a chance to learn about and appreciate the significance of the monument. The measures shall be submitted to and approved in writing by the LPA and implemented in accordance with the approved details and timescales.

Reason: In order to preserve or enhance the Scheduled Ancient Monument and its setting and to comply with Policy EE7 of the Runnymede 2030 Local Plan and guidance within the NPPF

34 Heritage - Scheduled Ancient Monument (Construction)

Prior to commencement of development a scheme for the protection of existing Heritage Assets during construction (statutory listed buildings and Scheduled Ancient Monument) shall be submitted to and approved in writing by the LPA. Development shall be implemented in accordance with the approved details.

Reason: In order to protect and preserve existing Heritage Assets within the site to be

retained and to comply with Policy EE4 and EE7 of the Runnymede 2030 Local Plan and guidance within the NPPF

35 Public Art Strategy

Prior to or alongside the submission of the first phase reserved matters application, a public art strategy shall be submitted and approved in writing by the Local Planning Authority, which shall detail proposals for the incorporation of public art within the development including phasing and timescales for delivery.

Reason: To comply with Policy SD9 of the Local Plan and NPPF

36 Ecological Survey and mitigation strategy

Alongside the submission of each RMA an updated ecological survey and Ecological Mitigation and Habitat Management plans, which have regard to the measures proposed within the submitted Environmental Statement shall be provided for the relevant part of the site and the findings of the survey, and any recommended mitigation, submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the approved details and timeframes. This should include breeding bird surveys and bat activity surveys undertaken no more than 3 years prior to the application, and a phase 1 habitat survey no more than 18 months prior to the application.

Reason: To protect and enhance the biodiversity ecological value of the site and ensure provision of suitable mitigation for protected species and to comply with Policies EE9, EE11 and EE12 of the Runnymede 2030 Local Plan and guidance within the NPPF.

37 Amphibians mitigation strategy

Prior to or alongside the submission of the first RMA, a Mitigation strategy for the translocation of Great Crested Newts (GCN) (and other amphibians) shall be submitted to and approved in writing by the Local Planning Authority, and shall be informed by the measures within the Environmental Statement. The development shall be implemented in accordance the approved details

Reason: To protect Great Crested Newts and other amphibians and to comply with Policy EE9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

38 Badger mitigation strategy

Each reserved matters application shall be accompanied by an updated 'badger sett and habitat impact assessment and mitigation strategy' which shall be submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance the approved details.

Reason: To protect badgers and to comply with Policy EE9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

39 Highways – Parking and turning

The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority at the Reserved Matters Stages for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policy SD4 of the Runnymede 2030 Local Plan and guidance within the NPPF

40 Highways – EV charging

Each reserved matters application shall be accompanied by an Electric Vehicle Strategy to include proposals for charging opportunities for all residential (including communal residential) and non-residential parking spaces including for the primary school and other (non-residential) uses. This shall include proposals for charging facilities to be provided in accordance with SCC standards and reflecting the most current standard at the time of RMA approval, or alternative strategy which takes into account any advances in technology and changes to legislation at the time of the submission of the RMA. The strategy shall be implemented as approved prior to occupation and shall thereafter be retained and maintained to the satisfaction of the Local Planning Authority.

Reason: To ensure sustainable design and to comply with Policy SD7 and SD9 of the Runnymede 2030 Local Plan and guidance in the NPPF.

41 Highways – Reserved matters submission

The development hereby approved shall not be first occupied / first opened for trading unless and until the following facilities have been provided in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority at the Reserved Matters stage for:

- (a) The secure parking of bicycles within the development site,
- (b) Facilities within the development site for cyclist to [change into and out of cyclist equipment /shower],
- (c) Facilities within the development site for cyclists to store cyclist equipment,
- (d) The addition of bus stops located so that every property is within 300 metres, to include:
 - Raised kerbing to a height of 140mm over a 9.0m length to ensure level access onto and off the buses
 - A clearway with a 17m bus cage to protect the bus stop
 - A bus shelter with seating and lighting.
 - A RTPI display board
 - A bus stop flag and pole
 - A clear bus routing plan to tie into construction phasing such that all parts of the development can be connected by bus without the need for reversing of vehicles and to allow for the free passage of buses.
- (e) Information to be provided to residents / staff / visitors regarding the availability of and whereabouts of local public transport / walking / cycling / car sharing clubs / car clubs/transport hubs,

and thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan.

42 Highways – Cycle Spaces

Public Cycle spaces (including at Longcross Railway Station) shall be delivered in accordance with a Phasing Plan to be submitted to and approved in writing by the Local Planning Authority prior to or alongside the first reserved matters application, to include:

- (a) A timeframe for delivery of cycle parking in general accordance with the build programme for the Longcross South residential development;
- (b) Details of the number and type of cycle spaces and storage facilities to be provided at each phase (to include a mix of short and long-term parking);
- (c) Details of provision for e-bikes and e-scooters and a timescale for delivery.

The approved facilities shall be delivered in accordance with the Phasing Plan and shall thereafter be retained and maintained to the satisfaction of the Local Planning Authority.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

43 Highways – Bridge over M3 measures

Prior to first use of the bridge over the M3, measures to restrict access by private vehicles (with the exception of buses), to include some or all the following:

- (a) Suitable signage, lining, and surfacing to restrict private vehicle access along Accommodation Bridge in both directions;
- (b) Double yellow line waiting restrictions around the bridge to ensure that access for buses is maintained at all times;
- (c) Introduction of traffic enforcement cameras in both directions
- (d) Bus gate, or any alternative enforcement measures

shall be designed and implemented in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

44 Highways – Trumps Green Road traffic calming measures

Prior to the occupation of the 150th dwelling, a Traffic Regulation Order shall be promoted (funded by the Developer) to extend the existing 30mph speed limit on Trumps Green Road from The Mount Close to the proposed new traffic signal junction at Chobham Lane/Kitsmead Lane. To assist in ensuring compliance with the proposed extended length of 30mph restriction on Trumps Green Road and support the implementation of new junction arrangements at Lyne Road, Wellington Avenue and Kitsmead Lane, the applicant shall implement a scheme of traffic calming measures along Trumps Green Road between Lyne Road and Kitsmead Lane. The appropriate form and location of traffic calming measures is subject to a detailed design and stakeholder engagement exercise in conjunction with the proposed promotion of a 30mph traffic regulation order, as well as the detailed design of the aforementioned junction mitigation measures which are identified in outline on drawings. Appropriate traffic calming measures will be the subject of a detailed design and stakeholder engagement exercise and will be agreed with Runnymede Borough Council and Surrey County Council to include the following measures:

- Raised table arrangements on Trumps Green Road included as part of junction mitigation measures at Lyne Road Underbridge, Wellington Avenue and Kitsmead Lane.

- Speed cushions on Trumps Green Road between the abovementioned junction improvement locations.
- Road narrowing or gateway features on Trumps Green Road coupled with surface markings reinforcing a new 30mph speed limit.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

45 Highways – Public access routes

A scheme shall be submitted to and approved in writing by the Local Planning Authority with each Reserved Matters stage to show the proposed internal network of connecting footway and cycle way routes within the site, and where appropriate connections to external links. The network shall provide adequate connectivity in both north/south and east/west directions and shall connect to key transport facilities such as transport hubs, greenspaces, school/key facilities and the M3 overbridge pedestrian/cycle link. Development shall be implemented in accordance with the approved details.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

46 Highways – Transport Masterplan

Prior to or alongside the first reserved matters application for residential development, details shall be submitted to and agreed with the LPA to provide a strategy and masterplan to identify the following which shall be provided within the proposed development:

- (a) The location of proposed bus stop infrastructure and routing
- (b) The proposed routes and connections for pedestrian and cyclists ensuring the most convenient, shortest routes between site facilities and external connections
- (c) The location of strategic “transport hubs” within the site connecting together groups of transport facilities such as bus stops, cycle parking, EV charging facilities, car clubs, cycle hire etc
- (d) The locations of key local facilities within the site
- (e) The proposed routes for emergency service vehicles to provide adequate access to all dwellings
- (f) Proposed traffic calming measures, and parking enforcement measures.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

47 Highways – Off-Site Highway works

Prior to the occupation of the 150th dwelling the proposed off-site junction works to Longcross Road/Accommodation Road, Trumps Green/Wellington Avenue, Trumps Green Road/Lyne Road Underbridge, Chobham Lane/Kitsmead Lane Stonehill Road/Holloway Hill/Longcross Road, Kitsmead Lane/Longcross Road, shall be constructed and provided in general accordance with the drawing numbers 2204410-SK027A, 2204410-SK028B, 2204410-SK036B, 2204410-SK045C, 2204410-SK046A, JNY9453-SK018 C. The above works shall be carried out under S278 of the Highways Act 1980 at developers’ expense and will be subject to a detailed design and stakeholder engagement exercise. Any necessary supporting additional traffic management

measures or other supporting measures for these junction works shall also be provided at developers' expense.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

48 Highways – Phasing Plan

Prior to construction taking place at the site, details shall be submitted to and agreed with the LPA showing the proposed site Phasing Plan. The Phasing Plan will need to support all stages of the development, including the provision for sustainable modes of transport- bus services, walking and cycling. This shall prioritise the early connection of the Garden Village north and south of the M3 via the Accommodation Bridge.

Reason: To encourage active and sustainable travel, and in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with Policies SD3, SD4, SD9 and SL1 of the Runnymede 2030 Local Plan and guidance within the NPPF, and Surrey County Councils Local Transport Plan

49 Energy/sustainability

Each phase of development/reserved matters application shall include an Energy Statement for the phase to demonstrate compliance with the committed principles in the Energy & Sustainability Statement, and should continue to demonstrate how opportunities for renewable/low carbon energy have been maximised by producing and using renewable/low carbon energy on-site and to take into account advances in technology and changes to legislation. Development shall be carried out in accordance with the approved details and thereafter retained, maintained and operational unless otherwise agreed in writing by the LPA.

Reason: To ensure sustainable design and to comply with Policies SD7, SD8, SD9 and EE2 of the Runnymede 2030 Local Plan and guidance within the NPPF.

50 Water conservation

With each reserved matters application, details of the water efficiency measures and strategy to demonstrate how greywater recycling systems and rainwater harvesting techniques have been incorporated, shall be submitted to and approved in writing by the Local Planning Authority. Such details as approved shall be fully implemented and retained for the lifetime of the development

Reason: In order to achieve water efficiency and sustainable development and to comply with Policy SD7 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

51 Sports – Ground Conditions (Sports England)

Prior to the commencement of development in respect of residential dwellings, the following shall be undertaken:

a. A detailed assessment of ground conditions of the land proposed for the new playing field land ancillary to the proposed primary school and east of Kitsmead Lane shall be undertaken (including drainage and topography) to identify constraints which could affect playing field quality; and

b. Based on the results of this assessment to be carried out pursuant to (a) above of this condition, a detailed scheme to ensure that the playing fields will be provided to an and approved in writing by the Local Planning Authority.

The works shall be carried out in accordance with the approved scheme within a timescale to be first approved in writing by the Local Planning Authority.

Reason: To ensure that site surveys are undertaken for new or replacement playing fields and that any ground condition constraints can be and are mitigated to ensure provision of an adequate quality playing field and to accord with Runnymede 2030 Local Plan Policy SL26 and SD9

52 Sports (Sports England)

Any areas of playing fields and artificial grass pitches shall be used for Outdoor Sport and for no other purpose (including without limitation any other purpose in Class D2 Use Classes Order 2005, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification).

Reason: To protect the playing field/artificial grass pitch from loss and/or damage, to maintain the quality of and secure the safe use of sports pitch/es and to accord with Runnymede 2030 Local Plan Policy SL26 and SD9

53 Sports (Sport England)

The new playing fields and pitches shall be constructed and laid out in accordance with the standards and methodologies set out in the guidance note "Natural Turf for Sport" (Sport England, 2011), and shall be made available for use before occupation [or other specified timeframe] of the development hereby permitted.

Reason: To ensure the quality of pitches is satisfactory and they are available for use within the agreed timescales and to accord with Runnymede 2030 Local Plan Policy SL26 and SD9.

54 Sports (Sport England)

Prior to the bringing into use of the playing field/pitches east of Kitsmead Lane, a Management and Maintenance Scheme for the facility including management responsibilities, a maintenance schedule and a mechanism for review shall be submitted to and approved in writing by the Local Planning Authority after consultation with Sport England. The measures set out in the approved scheme shall be complied with in full, with effect from commencement of use of the playing field/pitches.

Reason: To ensure that new facility/ies is capable of being managed and maintained to deliver a facility which is fit for purpose, sustainable and to ensure sufficient benefit of the development to sport (National Planning Policy Framework (NPPF) para 97) and to accord with LP Policy Runnymede 2030 Local Plan Policy SL26 and SD9.

55 Sports (Sport England)

Prior to first use of the primary school a community use scheme must be submitted to and approved in writing by the Local Planning Authority after consultation with Sport England. The scheme shall apply to the playing field/pitches; artificial grass pitch (AGP) and MUGA and shall include details of pricing policy, hours of use, access by non-educational establishment users, management responsibilities, a mechanism for review and a programme for implementation. The approved scheme shall be implemented upon the start of use of the primary school and shall be complied with for the duration of the use of the development.

Reason: To secure well managed safe community access to the sports facility, to ensure sufficient benefit to the development of sport and to accord with LP Policy

56 External Lighting Strategy

Prior to the construction of each phase of development an External Lighting Strategy shall be submitted to include details of proposed external lighting. Where appropriate such details shall include proposed hours of use and measures to ensure that no direct light is projected into the atmosphere above the lighting installation and shall be designed to have regard to any neighbouring sensitive receptors including regard to the protection of wildlife (including bats). Development shall be carried out in accordance with the approved details and be retained as such thereafter.

Reason: To protect the amenities of occupiers of nearby properties and to protect wildlife and to comply with Policies EE2, EE9 and SD9 of the Runnymede 2030 Local Plan and guidance within the NPPF.

57 Noise Strategy

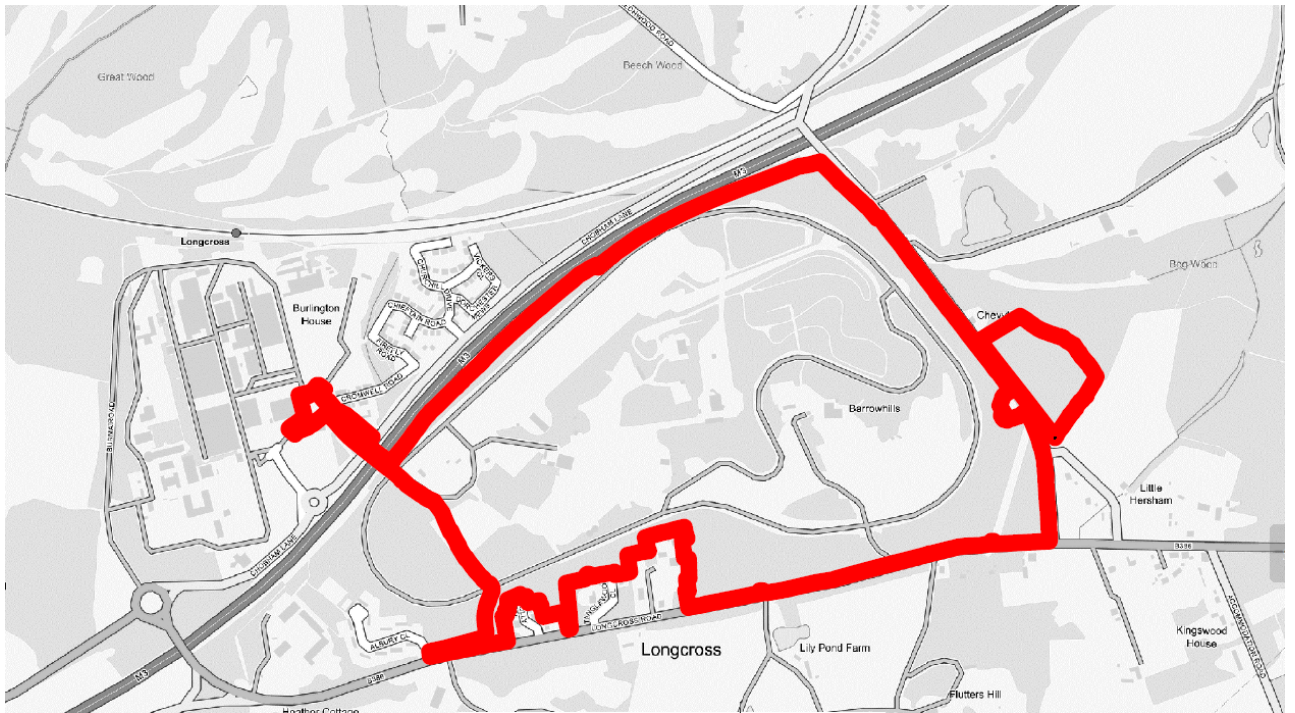
Prior to or alongside each reserved matters application, a fully detailed scheme (noise strategy) for protecting the proposed development (including where appropriate any garden) from noise, shall be submitted to and approved in writing by the Local Planning Authority. This shall include details of any mitigation measures required including acoustic barriers/fence (where required) and the associated triggers for provision. The details of the acoustic barrier(s)/fence shall include details of any associated land level changes/ earth bund, siting (which should have regard to minimise tree loss and visual impact) and include details of new soft landscaping/ tree planting, details of materials/visual appearance. The development shall be implemented in accordance with the strategy and details as approved prior to first occupation of the associated reserved matters application unless an alternative timeframe for implementation has been agreed in writing by the Local Planning Authority.

Reason: To protect the occupants of the new development from noise disturbance, (particularly road noise including the adjacent M3 motorway), and to ensure mitigation measures are visually acceptable. To comply with Policies EE1, EE2 and SD9 of the Runnymede 2030 Local Plan and guidance in the NPPF

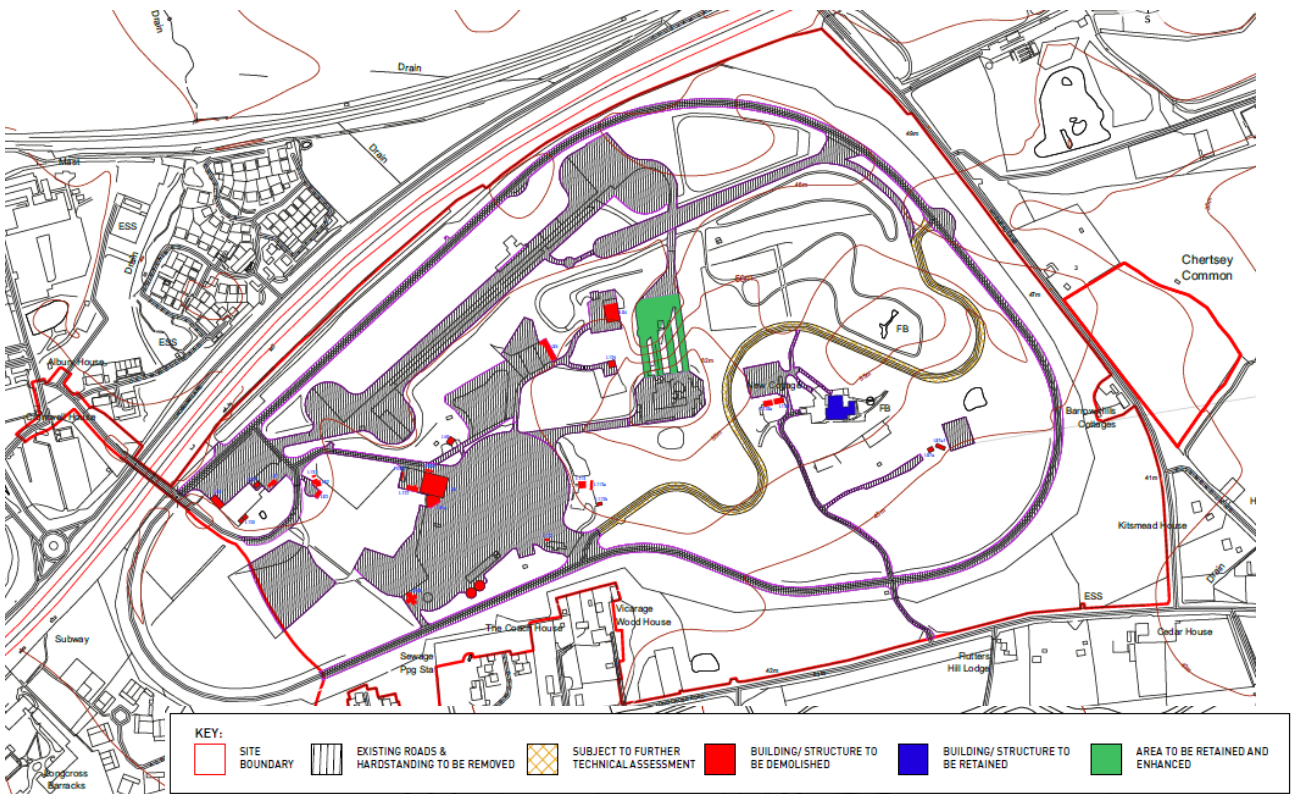
Illustrative Masterplan



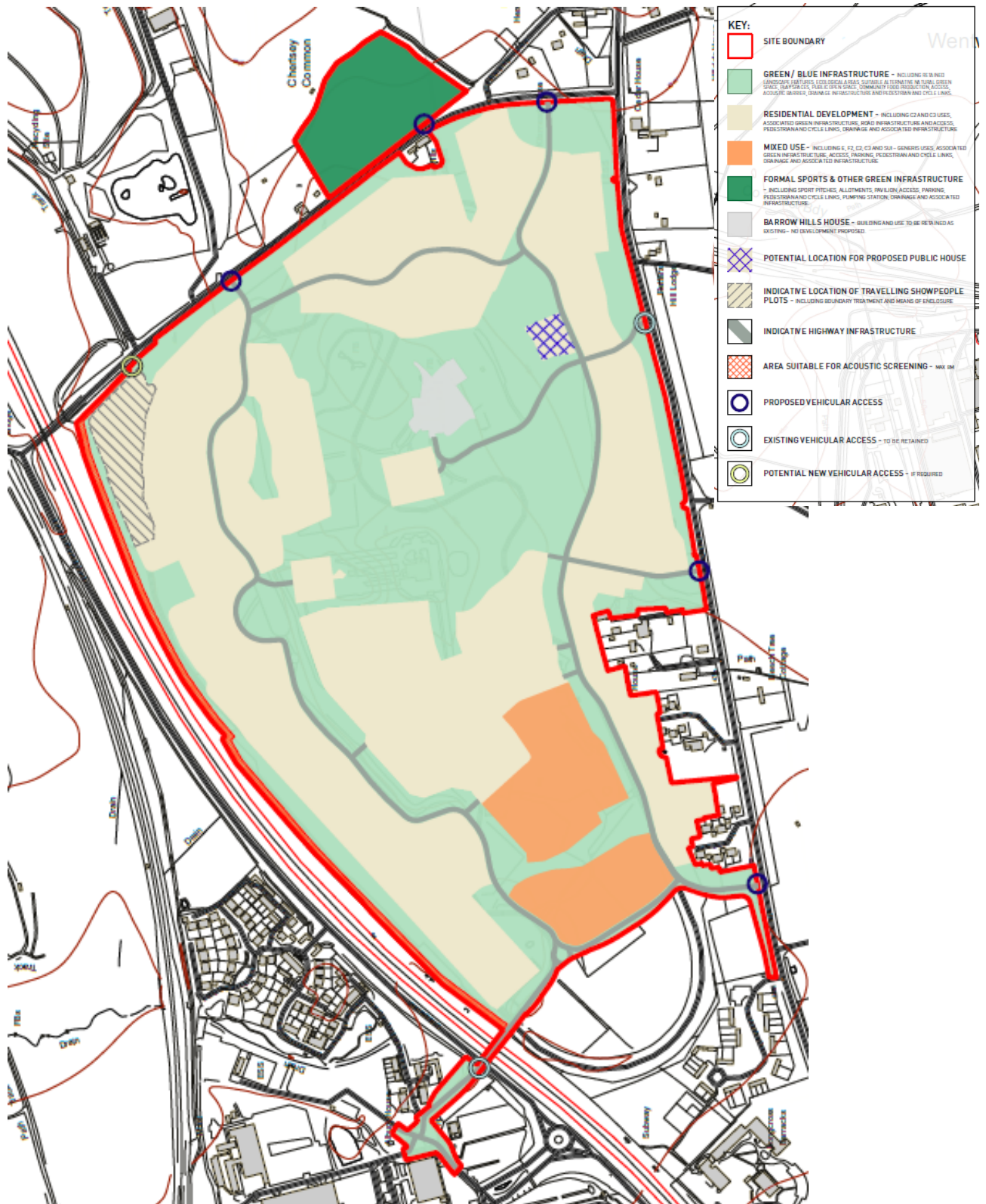
Site Location Plan



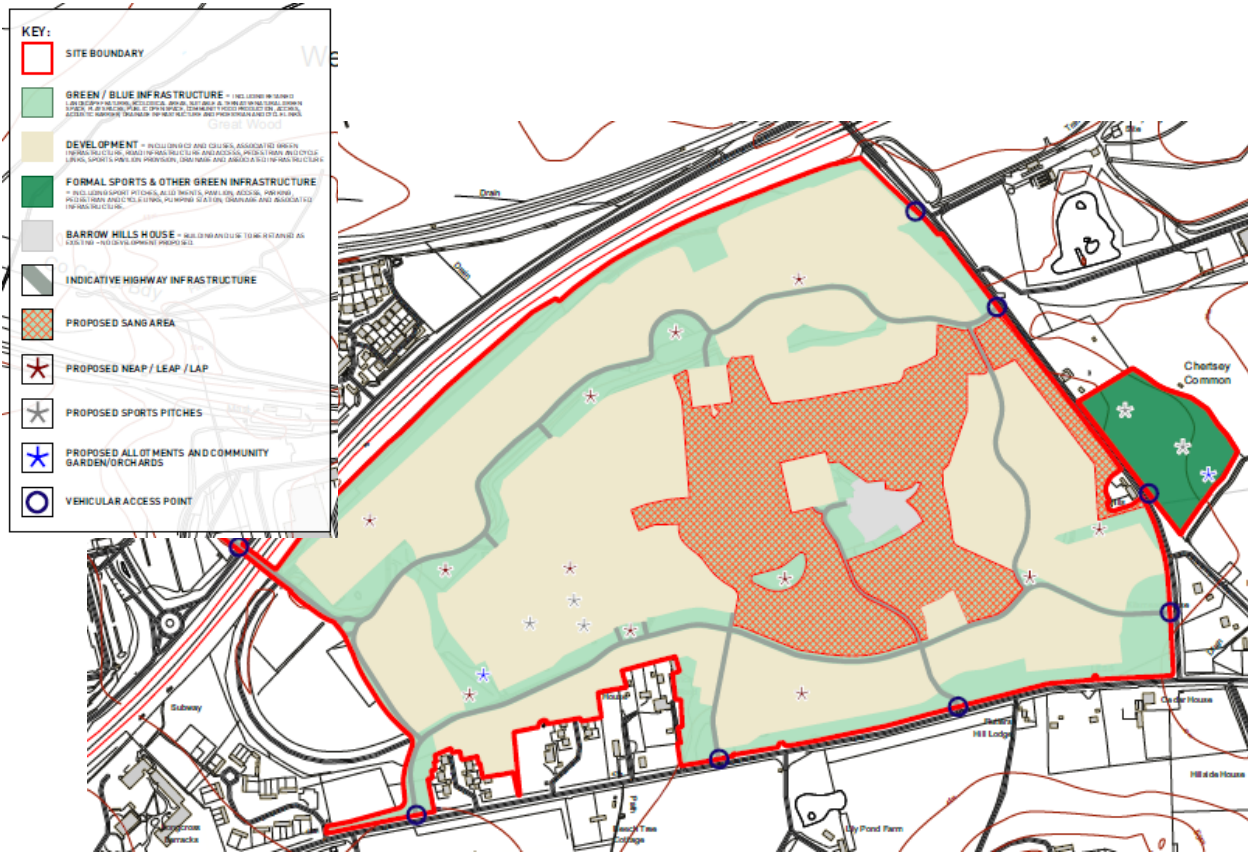
Parameter Plan - Demolition



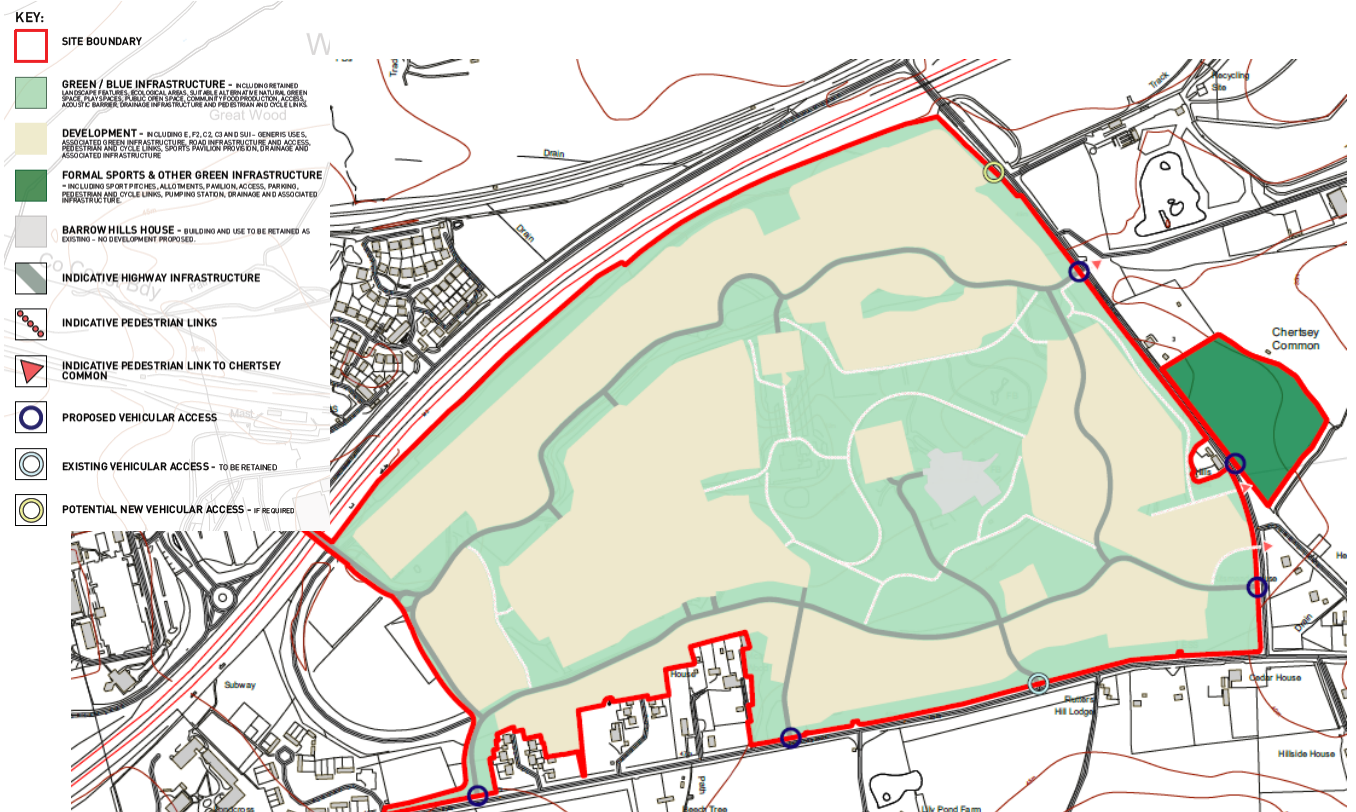
Parameter Plan – Land Use



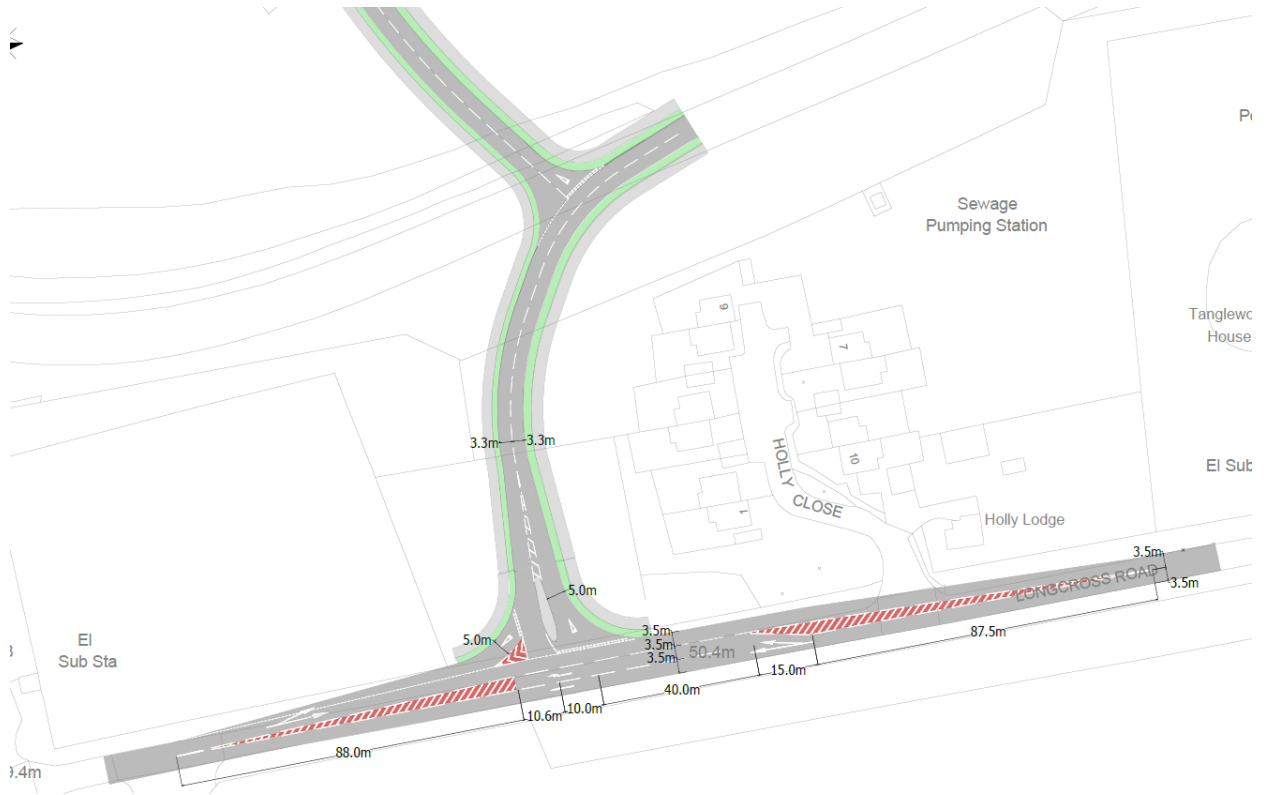
Parameter Plan – Green Infrastructure



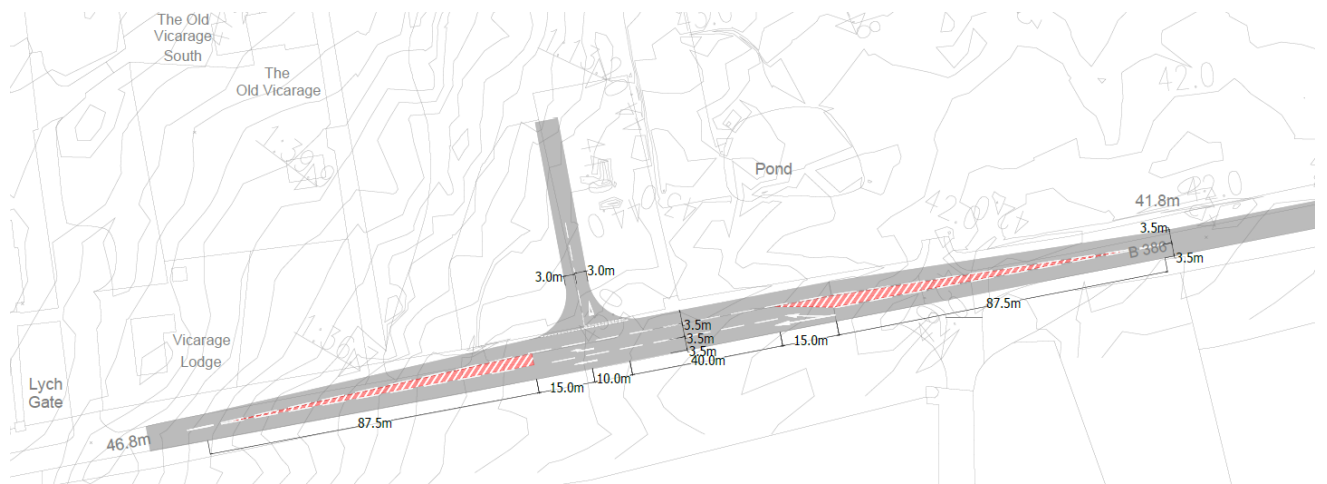
Parameter Plan – Access and Movement



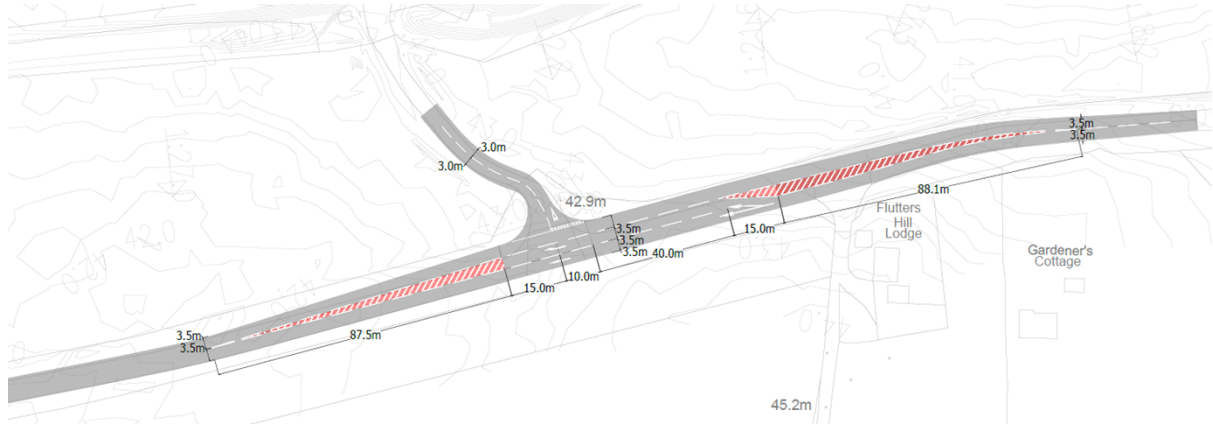
Proposed Access - Longcross Road West



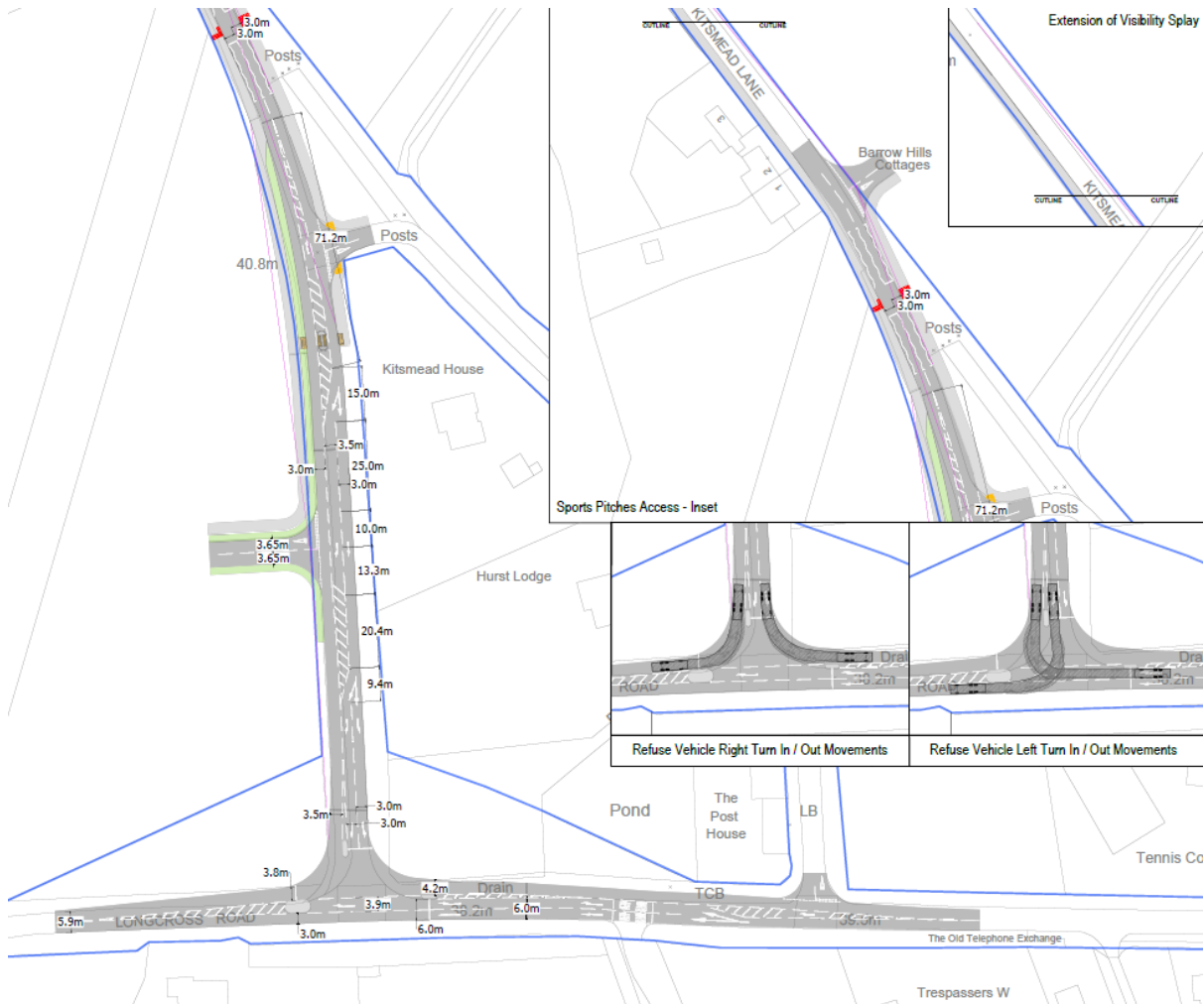
Proposed Access - Longcross Road Central



Proposed Access – Longcross Road East (existing revised Barrowshill entrance)



Proposed Access – Kitsmead Lane – South Access



Proposed Access – Kitsmead Lane North Access (including indicative access to Travelling showpeople plots)





FOR LOCATION PURPOSES ONLY

Runnymede Borough Council
Runnymede Civic Centre
Station Road
Addlestone
Surrey KT15 2AH

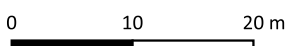
Date: 11/04/2024

Beechtree



© Crown copyright and database rights 2024 Ordnance Survey 100006086

Scale: 1:625



RU.23/1035



COMMITTEE AGENDA REFERENCE: 5C

APPLICATION REF:	RU.23/1035
LOCATION	16 A Beechtree Avenue, Englefield Green, Surrey, TW20 0SR
PROPOSAL	Change of use from women's refuge (sui-generis) to residential (Class C3), including rooftop and side extensions to provide 8 self-contained units, provision of additional car parking, bin storage and associated landscaping.
TYPE	Full Planning Permission
EXPIRY DATE	16/10/2023
WARD	Englefield Green West
CASE OFFICER	Adam Jackson
REASON FOR COMMITTEE DETERMINATION	Application for 5 or more dwelling recommended for approval.
<i>If you have questions about this report please contact Ashley Smith, Victoria Gibson or the case officer.</i>	

1. SUMMARY OF RECOMMENDATION

It is recommended the Planning Committee authorises the HoP:	
1.	To grant planning permission subject to the completion of a S106 legal agreement which secures mitigation against the impacts of the development on the Thames Basin Heaths Special Protection area, and the proposed affordable housing and relevant conditions.
2.	To refuse permission should the legal agreement not proceed to the HoP's satisfaction on the grounds of harm to the SPA and failure to provide affordable housing and/or if any significant material considerations arise prior to the issuing of the decision that in the opinion of the HoP would warrant refusal of the application. Reasons for refusal relating to any such matter are delegated to the HoP.

2. DETAILS OF THE SITE AND ITS SURROUNDINGS

- 2.1 The application site comprises a former women's refuge and its associated car park and garden. The existing building has a gross internal area of approximately 360sqm, spread across two floors. The existing building is 8.2m tall with a hipped roof and 2 single storey wings which are between 4.3 and 4.5m tall. The site is accessed via a driveway off Beechtree Avenue and sits behind existing dwellings. The surrounding area consists predominantly of residential developments, including semi-detached and

terraced dwellings, and flatted developments.

- 2.2 To the rear of the application site is Kings Lane Open Space, Egham Football Club and an area of woodland. Trees on site and to the rear of the site are not protected, however do make a positive contribution to the character and appearance of the area.
- 2.3 The application site is within the 5km buffer of the Thames Basin Heaths Special Protection Area and the woodland adjacent to the rear boundary of the site is within a Biodiversity Opportunity Area and is classed as Priority Habitat.
- 2.4 The development is proposed to be 100% affordable (affordable rent).

3. APPLICATION DETAILS

- 3.1 The application is for the change of use of the site and existing building from a women's refuge (sui-generis) to residential (Class C3). As part of the change of use, it is proposed to extend the building to the side and at roof level, and the resulting building will provide 8 self-contained flats (2 x 1 bed & 6 x 2 bed). It is also proposed to provide additional car parking space (12 x spaces), refuse/recycling storage, cycle storage and new soft landscaping.
- 3.2 The resulting building will have a gross internal area of approximately 625sqm set across three storeys. The main part of the building is approximately 9.25m tall with a pitched roof sloping up to an area of flat roof. The two storey elements either side of the main part of the building are 6.5m with flat roofs. The proposed development is proposed to be finished in a mixture of brick and zinc cladding with a mixture of tiled and flat roof sections, however it is recommended that final details of materials are secured via condition.
- 3.3 A communal garden area is provided to the side/rear of the site. The development will utilise the existing access and new tactile paving is proposed at the head of the access to improve highway safety.
- 3.4 The application originally proposed 11 flats; however, this was reduced to 8 following a reduction in the size of the extensions. The size of the extensions was reduced on the recommendation of Officers, following concerns being raised with the impact of the development on existing occupiers.

4. RELEVANT PLANNING HISTORY

- 4.1 The following history is considered relevant to this application:

Reference	Details	Decision
RU.96/0563	Erection of detached two storey seven bedroom special needs home.	Granted (14/08/1996)
RU.97/0114	Detached part one, part two storey special needs house with parking following the demolition of ambulance garages.	Granted (18/04/1997)

5. SUMMARY OF MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

- 5.1 The Runnymede 2030 Local Plan was adopted on 16 July 2020 and the policies have to be read as a whole. Any specific key policies will be referred to in the planning considerations where necessary.
- 5.2 The Englefield Green Village Neighbourhood Plan has also now been formally adopted and forms part of the statutory development plan for Runnymede. The plan is used alongside the Runnymede 2030 Local Plan, and any specific key policies will be referred to in the planning considerations where necessary.
- 5.3 Supplementary Planning Documents and Guidance which may be a material consideration in determination:
- Runnymede Design Supplementary Planning Document
 - Infrastructure Delivery and Prioritisation Supplementary Planning Document
 - Thames Basin Heaths Supplementary Planning Document
 - Green and Blue Infrastructure Supplementary Planning Document
 - Affordable Housing Supplementary Planning Document
 - First Homes Interim Policy Statement
 - Runnymede Parking Guidance Supplementary Planning Document
 - Trees, Woodland and Hedgerows Supplementary Planning Guidance

6. CONSULTATIONS CARRIED OUT

Consultees responses

Consultee	Comments
Lead Local Flood Authority	Not satisfied that the proposed surface water drainage scheme meets requirements, however, suggests that an amended scheme can be secured via condition.
County Highway Authority	No objections subject to conditions which secure: <ul style="list-style-type: none"> • Modified access and visibility zones • Bicycle parking. • Car parking • Electric vehicle charging points
Surrey Wildlife Trust (in their role as ecological advisor to the	Recommends approval subject to conditions which secure: <ul style="list-style-type: none"> • A Landscape and Ecological Management Plan • A Construction Environmental Management Plan

LPA)	Also recommends: <ul style="list-style-type: none"> • No net increase in lighting on woodland or trees • Compliance with the Thames Basin Heaths Special Protection Area avoidance strategy.
RBC Drainage	Recommends approval subject to a condition to secure the detailed surface water drainage scheme.
RBC Affordable Housing Officer	Fully support the application as it will provide needed affordable housing for local people.
RBC Recycling Officer	Supports the location of the Bin Store, however requests further details of its design.
Thames Water	No objections.

Representations and comments from interested parties

6.2 53 neighbouring properties were consulted in addition to being advertised on the Council's website.

2 letters of objection, from 1 address have been received. These letters object to the proposal, however, relate of the original plans; no letters have been received in response to consultation carried out on the amended plans. Notwithstanding, the concerns raised can be summarised as follows:

- Overlooking of neighbouring properties
- Noise pollution from the development
- Pollution from additional vehicles
- Light pollution from the outdoor lighting
- Smell/odour from the proposed bin store.

6.3 1 letter has also been received in support of the proposal, and can be summarised as follows:

- Housing is badly needed.
- No objection if the housing is for those waiting for housing or for first time buyers.

6.4 The Englefield Green Residents Association have also commented on the application. Their comments also relate to the original plans, and can be summarised as follows:

- No details of the affordable housing
- Objects to any student accommodation

7. PLANNING CONSIDERATIONS

7.1 In the determination of this application regard must be had to the Development Plan and

national policy within the NPPF. The application site is located within the urban area where the principle of such development is considered to be acceptable subject to detailed consideration. This must be considered in light of the presumption in favour of sustainable development advocated by the NPPF. The key planning matters are:

- The impact on the character and appearance of the area
- Residential amenity
- Highway safety and parking
- Biodiversity
- The impact on the Thames Basin Heaths Special Protection Area
- Renewable Energy
- Affordable Housing

Principle of Development

- 7.2 The existing lawful use of the site is as a women's refuge (Sui-Generis). Policy SD5 of the Runnymede 2030 Local Plan describes supported accommodation, such as this, as social & community infrastructure, however policy CF1 of the Englefield Green Village Neighbourhood Plan sets out specific community facilities which it considers should be protected, which does not include this site, and it is noted that the site, according to the applicant, has been vacant since May 2021.
- 7.3 Furthermore, policy SD5 of the Local Plan also considers affordable housing as Social & Community Infrastructure, and this application proposes 100% affordable housing (affordable rent), despite not being required by policies within the Local and Neighbourhood Plan. The proposal therefore replaces one piece of community infrastructure with another.
- 7.4 Given the above, there is no objection to the loss of the existing use.

The impact on the character and appearance of the area

- 7.5 Policy EE1 of the Runnymede 2030 Local Plan sets out that all development proposals will be expected to achieve high quality and inclusive design. Policy ND5 of the Englefield Green Village Neighbourhood Plan also sets out that proposals for good quality design will be supported where they are in accordance with the guidelines and codes set out in the Englefield Green Design Codes.
- 7.6 The application site falls within the West Character Area of the Englefield Green Design Codes, which is characterised by post war and late 20th century medium to high density housing, including some blocks of flats.
- 7.7 The existing building is an 8.2 metre tall, 2 storey detached property, most recently in use as a women's refuge. The property has a hipped roof and steps down to single storey sections on either side, however, is otherwise one of the larger buildings within the surrounding area.
- 7.8 Whilst the proposed extensions would add further mass and bulk to the existing building at first floor and above, the overall increase in height is just approximately 1m and amended plans have been received during the application which have significantly reduced both the height of the side extensions and the overall width of the proposed development.

- 7.9 The site is also set well back from the main road and is at a lower level, such that the building would not be overly prominent. It is not considered therefore that the additional height or mass would cause any material harm to the character and appearance of the area. It is also noted that there is a large 3-storey block of flats opposite the site access.
- 7.10 A partial flat roof is proposed as part of the extensions, which is not reflective of the existing roof or the roof design of adjacent properties, however as above, given the location of the property, its lower ground level and the relatively minimal increase in height, it is not considered that this would harm the character and appearance of the area in this instance.
- 7.11 In addition to the above, the building resulting from the proposed extensions has roughly the same footprint as the existing property, and retains its general form, with one larger section in the middle and two smaller wings either side. The extensions are therefore considered to be appropriate in terms of their scale and form.
- 7.12 To the rear, just outside of the application site are several mature trees. The trees make a positive contribution to the character and appearance of the area, furthermore policy NE3 of the Neighbourhood Plan sets out that the removal or loss of mature trees will be resisted.
- 7.13 The application has been supported by an Arboricultural Impact Assessment, which concludes that the removal of one tree (T3 – B Category) is necessary due to the provision of parking to the rear of the site, however that the rest of the trees will not be materially impacted and can be protected during construction activities. It will be necessary to secure a final Arboricultural Method Statement and Tree Protection Plan via condition in order to ensure an acceptable impact on retained trees. A suitable landscaping plan and strategy, including new and replacement trees and vegetation can also be secured via condition.
- 7.14 Overall, it is not considered that the proposed would result in harm to the character and appearance of the area. The proposal is therefore considered to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND5 of the Englefield Green Village Neighbourhood Plan and Design Codes, the Runnymede Design SPD and guidance within the National Planning Policy Framework.

Residential amenity

- 7.15 Policy EE1 of the Runnymede 2030 Local Plan sets out that new development proposals will be supported where they ensure no adverse impacts on the amenities of occupiers of the proposed development or to neighbouring properties/occupiers. Policy ND1 of the Englefield Green Village Neighbourhood Plan also sets out that development proposals should not result in any significant adverse impact on the amenities of the occupiers of neighbouring properties.

Impact on existing residents

- 7.16 With regards to the impact on existing residents, there are neighbouring properties to the north, east and south of the application site. Separation distances to properties to the north and east are considered sufficient to prevent any overbearing impact, however it is considered that the first-floor side (north) facing windows should be obscurely glazed to prevent any overlooking of these properties or their gardens.
- 7.17 The closest neighbour is to the southeast. The southern wing, which is adjacent to this neighbour, is increasing in height by approximately 2m compared to the highest part of the roof of the existing wing. Notwithstanding, the existing separation distance of approximately 4m is being retained and the southern wing is adjacent to the rear part of this neighbour's garden. It is not therefore considered that there would be a material overbearing impact on

this neighbour either. This neighbour, due to being to the southeast of the proposed development, will also not experience any material loss of direct sunlight. It should also be noted that the scale of the proposed extensions, including that to the south wing, has been reduced during the application in order to protect residential amenity.

- 7.18 The first floor south facing window would not be afforded direct views into this neighbour's garden, however the second-floor window which is set further back would, and therefore it is considered necessary for this to be obscurely glazed. The east facing bathroom windows within the first and second floors within the southeast corner of the building would also overlook this neighbour's garden and as such should also be obscurely glazed. It is acknowledged that the proposed development results in an additional storey, and therefore additional windows, however subject to the suggested windows being obscurely glazed, it is not considered that the development would result in any material loss of privacy compared to the existing.
- 7.19 It is not considered that obscurely glazing any of the windows suggested within the north, east or south elevations would negatively impact the standard of amenity for the future occupiers of the relevant flats, as the windows are either to non-habitable rooms or the rooms, they serve are dual aspect.

Amenity for the proposed residents

- 7.20 With regards to the amenity afforded to the future occupiers of the flats, the flats all accord with the minimum internal space standards set out in policy SL19 of the Local Plan, and are dual aspect, thereby ensuring good levels of light.
- 7.21 A communal garden area is also proposed to the rear/side of the site, which is considered large enough for all the future residents. This area is labelled as public open space on the proposed land uses plan, however, is more likely to only be used by residents given the location of the site, and the size of this space. There are no actual requirements for public open space given the size of the development. Recycling and refuge facilities as well as a cycle store are also proposed, and final details of these can be secured via condition.
- 7.22 Overall, it is considered that the development will not materially harm the amenity of neighbouring occupiers, and that the future residents of the development will be provided with a suitable residential environment.

Highway safety and parking

- 7.23 The application is supported by a Transport Statement, which concludes that the development would not result in a material increase in traffic coming to and from the site compared to the existing use. This has been calculated on the basis that the existing lawful use as sheltered accommodation to generate an average of 0 AM or PM peak time trips. The Transport Statement is also based on a scheme for 11 flats, as originally proposed, rather than 8. There is not therefore considered to be any material traffic implications arising from the development.
- 7.24 The development will use the existing access and tactile paving is proposed at the entrance of the access in order to improve highway safety.
- 7.25 The proposed development will be provided with 12 parking spaces for the flats and 2 visitor parking spaces. This is in excess of the suggested number of spaces for a development of this size as set out in the Runnymede Parking Guidance Supplementary Planning Document, however this is considered appropriate given the location of the development and the likelihood of cars being the predominant mode of transport for the future occupiers.

Despite the additional hardstanding created by the parking spaces, sufficient space will remain on the site for soft landscaping.

- 7.26 The Parking Guidance SPD and policy TT1 of the Englefield Green Village Neighbourhood Plan also required each dwelling to be provided with an electric vehicle charging point. Electric Vehicle charging points have been shown on the proposed Parking Layout Plan, and a condition is suggested to secure this.
- 7.27 A cycle store is proposed within the garden area for the flats, and a condition is recommended which will secure these, and to ensure compliance with the Parking Guidance SPD and policy TT2 of the Neighbourhood Plan.
- 7.28 It is noted that several neighbouring properties can currently access their gardens via gates within the existing car park area of the application site, however it is understood that there is no formal right of way across the site for these properties. This is therefore a civil matter between parties and not a matter for planning.

Biodiversity

- 7.29 The application site is adjacent to a biodiversity opportunity area to the rear of the site. Policy EE9 of the Runnymede 2030 Local Plan sets out that the Council will seek net gains in biodiversity, and policy EE11 sets out that further fragmentation of Green Infrastructure will be avoided through the encouragement of development proposal which restore, maintain and enhance habitat connectivity. Policies NE1 and NE2 of the Englefield Green Village Neighbourhood Plan are consistent with policies EE11 and EE9 respectively.
- 7.30 Whilst it is necessary to remove one mature tree from the site, the majority of the trees on and adjacent the site are to be retained. Sufficient space around the development also exists to provide new planting and soft landscaping. Furthermore, the application has been supported by a Biodiversity Impact Calculation report, which predicts a net gain in biodiversity of 12.31%.
- 7.31 There are some inconsistencies between the report and the landscaping shown on the proposed plans, however it is considered that final details of both landscaping and biodiversity enhancements can be secured by condition.
- 7.32 The woodland adjacent to the rear boundary of the site as well as being a biodiversity opportunity area is also classed as Priority Habitat (Deciduous Woodland), however, with the exception of some of the parking spaces, the existing buffer between the development and the woodland will be retained, and conditions requiring hard and soft landscaping details and a landscape environmental management plan will ensure that biodiversity enhancements to this buffer are secured in accordance with requirements set out in policy NE2 of the Englefield Green Village Neighbourhood Plan.
- 7.33 The existing building has been assessed as having low bat roosting potential, and as such a follow-up bat emergence survey was carried out. However, no bats were seen to have emerged from the building during the survey and as such there is a likely absence of roosting bats within the building and no further surveys are considered to be required.
- 7.34 The habitat to the rear of the site is suitable for foraging and commuting bats and as such, to prevent light spill into this area and protect this habitat, it is considered necessary to condition that details of any proposed external lighting are first approved by the Local Planning Authority.
- 7.35 The Ecological Assessment submitted with the application concludes that other species such as badgers are likely absent from the site, however a construction environmental

management plan, which sets out precautionary approach to work is considered necessary to ensure there is no harm. This can be secured via condition.

The impact on the Thames Basin Heaths Special Protection Area

- 7.36 The application site is located within the 5km buffer of the Thames Basin Heaths Special Protection Area. The SPA is designated under UK and European law due to the presence of breeding populations of birds. These birds' nest on or near the ground and as such are susceptible to disturbance from informal recreational use such as walking and dog walking.
- 7.37 To ensure no adverse effects from new residential development, additional recreational use, and to satisfy the Habitat Regulations, an avoidance strategy has been agreed with Natural England in the form of SANG (Suitable Alternative Natural Greenspace). The purpose of SANG is to attract potential new users away from the SPA. Strategic Access Management and Monitoring (SAMM) also forms part of the avoidance strategy.
- 7.38 For a development of this size, the following monetary contributions are due:
- SANG = £12,558.65, and
 - SAMM = £5,711.09
- 7.39 These contributions can be secured via a S106 legal agreement, which the applicant has indicated they are willing to enter into. Subject to securing these fees, it is not considered that the development will have an adverse impact on the integrity of the Thames Basin Heaths Special Protection Area.
- 7.40 It is recommended that members defer the application to the HoP to grant planning permission subject to the completion of this agreement.

Renewable Energy

- 7.41 The application has been supported by an Energy Statement which sets out that development will seek to improve the fabric efficiency of the building as recommended by the Council's energy hierarchy, and that solar photovoltaics will be installed on the flat roof of the building. The Planning Statement also commits to installing the solar panels. A condition is therefore recommended which secures the details of the solar panels.

Affordable Housing

- 7.42 The applicant is proposing to provide a 100% affordable rent affordable housing scheme. A S106 legal agreement is currently being drafted which will secure the proposed affordable housing, and it is therefore recommended that members defer the application to the HoP to grant planning permission subject to the completion of this agreement.

Drainage

- 7.43 A Surface Water Drainage Strategy has been submitted in support of the application, however this does not include soakage or infiltration testing test results and as such it has not been demonstrated that the proposed drainage strategy is feasible. The Lead Local Flood Authority advise that an updated drainage strategy should be secured via condition.

8. PLANNING OBLIGATIONS/COMMUNITY INFRASTRUCTURE LEVY (CIL)

- 8.1 The proposal would ordinarily be CIL liable, however on the assumption that 100%

affordable housing is to be secured/provided, the proposal would not attract a CIL fee.

9. EQUALITY AND HUMAN RIGHTS CONSIDERATIONS

- 9.1 Consideration has been given to Articles 1 and 8 of the First Protocol of the European Convention on Human Rights. It is not considered that the decision would result in a violation of any person's rights under the Convention.

Consideration has been given to s149 of the Equality Act 2010 (as amended), which has imposes a public sector equality duty that requires a public authority in the exercise of its functions to have due regard to the need to:

- (a) Eliminate discrimination, harassment, victimisation and any other conduct prohibited by the Act.
- (b) Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it.
- (c) Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

It is considered that the decision would have regard to this duty.

10. CONCLUSIONS

- 10.1 The proposed development is considered to have an acceptable impact on the character and appearance of the area, would provide future residents with an acceptable residential environment, would not materially harm the amenity of existing neighbours, and will have an acceptable impact on highway safety. Subject to the completion of the S106 agreement, the proposed development will also have an acceptable impact on the Thames Basin Heaths Special Protection Area. The proposed development will provide benefits in terms of biodiversity, renewable energy and affordable housing.
- 10.2 The development has been assessed against the following policies SD3, SD4, SD7, SD8, SL19, SL20, EE1, EE2, EE9, EE10, EE11 and EE13 of the Runnymede 2030 Local Plan, policies ND1, ND5, ND6, NE1, NE2, NE3, TT1 and TT2 of the Englefield Green Village Neighbourhood Plan, the policies of the NPPF, guidance in the PPG, and other material considerations including third party representations. It has been concluded that the development would not result in any harm that would justify refusal in the public interest. The decision has been taken in compliance with the requirement of the NPPF to foster the delivery of sustainable development in a positive and proactive manner.

11. FORMAL OFFICER RECOMMENDATION

The HoP be authorised to grant planning permission subject to the completion of a Section 106 legal agreement under the Town and Country Planning Act 1990 (as amended) to secure the following obligations:

(i) Mitigation against the impact of the development on the Thames Basin Heaths Special Protection Area in the form of:

- SANG = £12,558.65, and
- SAMM = £5,711.09

(ii) 100% affordable housing (affordable rent)

And the subject to the following planning conditions:

1. Full application (standard time limit)

The development for which permission is hereby granted must be commenced not later than the expiration of three years beginning with the date of this permission.

Reason: To comply with Section 51 of Part 4 of the Planning and Compulsory Purchase Act 2004.

2. List of approved plans

The development hereby permitted shall not be carried out except in complete accordance with the following approved plans:

- Location Plan (21.1580.001 - Rev B)
- Proposed Ground Floor Site Plan (21.1580.100 – Rev D)
- Proposed First Floor Site Plan (21.1580.101 – Rev E)
- Proposed Second Floor Site Plan (21.1580.102 – Rev D)
- Proposed Site Plan (21.1580.103 – Rev C)
- Proposed Parking Layout (21.1580.200 – Rev B)
- Proposed Storey Heights (21.1580.201 – Rev B)
- Proposed Boundary Treatments (21.1580.202 – Rev B)
- Proposed Materials Plan (21.1580.203 – Rev B)
- Proposed Refuse Strategy (21.1580.204 – Rev B)
- Proposed Land Uses Plan (21.1580.205 – Rev B)
- Proposed Secured by Design Plan (21.1580.206 – Rev B)
- Proposed Adjacent Dimensions Plan (21.1580.208 – Rev B)
- Proposed Flat Block Plans Sketch (21.1580.400 – Rev C)
- Proposed Flat Block Elevations (21.1580.410 – Rev B)
- Site Access Layout (6805.002 – Rev A)

Reason: To ensure high quality design and to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND5 of the Englefield Green Village Neighbourhood Plan, and guidance in the NPPF

3. External materials (details required)

Notwithstanding that shown on the approved plans, prior to the above ground construction of the development hereby permitted is commenced, details of the materials to be used on the external elevations shall be submitted to and approved by the Local Planning Authority. The development shall thereafter be carried out and maintained in accordance with the approved details.

Reason: To ensure high quality design and to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND5 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

4. Tree protection

Prior to the commencement of any works hereby approved, including demolition, and before any equipment, machinery or materials are brought on to the site, an Arboricultural Method Statement and Tree Protection Plan shall be submitted to the Local Planning Authority for approval and then subsequently approved. Tree protective measures shall be installed in accordance with the approved Tree Protection Plan and once in place, photographic evidence of the protective measures shall be submitted to the Local Planning Authority (LPA) for approval.

The works shall be carried out in accordance with the approved protection plan and method statement. The protective measures shall remain in place until all works are complete and all machinery and materials have finally left site. Nothing shall be stored or placed in any area fenced in accordance with this condition, nor shall any fires be started, no tipping, refuelling, disposal of solvents or cement mixing carried out, and ground levels within those areas shall not be altered, nor shall any excavation or vehicular access, other than that detailed within the approved plans, be made without the written consent of the LPA.

There shall be no burning within six metres of the canopy of any retained tree(s). Where the approved protective measures and methods are not employed or are inadequately employed or any other requirements of this condition are not adhered to, remediation measures, to a specification agreed in writing by the LPA, shall take place prior to first occupation of the development, unless the LPA gives written consent to any variation.

Reason: To protect the trees to be retained, enhance the appearance and biodiversity of the surrounding area and to comply with policies EE1, EE9 and EE11 of the Runnymede 2030 Local Plan; policies NE1, NE2 and NE3 of the Englefield Green Village Neighbourhood Plan; and guidance within the NPPF.

5. Landscaping

a. No above ground development shall take place until full details of both hard and soft landscaping works have been submitted to and approved in writing by the Local Planning Authority (LPA). The approved hard and soft landscaping works shall thereafter be carried out as approved prior to the first occupation of the development. The scheme shall include indications of all changes to levels, hard surfaces, walls, fences, access features, minor structures, the existing trees and hedges to be retained, together with the new planting to be carried out and details of the measures to be taken to protect existing features during the construction of the development.

b. All hard and soft landscaping works shall be carried out in accordance with the approved details. Arboricultural work to existing trees shall be carried out prior to the commencement of any other development; otherwise all remaining landscaping work and new planting shall be carried out prior to the occupation of any part of the development or in accordance to the timetable agreed with the LPA. Any trees or plants, which within a period of five years of the commencement of any works in pursuance of the development die, are removed, or become seriously damaged or defective, shall be replaced as soon as practicable with others of similar size and species, following consultation with the LPA, unless the LPA gives written consent to any variation.

Reason: To preserve and enhance the character and appearance and biodiversity of the surrounding area and to comply with Policies EE1, EE9 and EE11 of the Runnymede 2030 Local Plan; policies NE1, NE2 and NE3 of the Englefield Green Village Neighbourhood Plan; and guidance within the NPPF.

6. Obscure glazing

Before the first occupation of the proposed development, the first and second floor side (north) facing windows, the second floor side (south) facing window, and the first and second floor front (east) facing windows serving the bathrooms within the south east corner of the building, shall be fitted with obscured glazing (at Pilkington Glass Level 4 or equivalent), and any part of the windows that are less than 1.7 metres above the floor of the room in which they are installed shall be non-opening and fixed shut. The window(s) shall be permanently retained in that condition thereafter.

Reason: To avoid overlooking into the adjoining property and to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND1 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

7. No additional windows

Notwithstanding the provisions of Classes A, B and C of Schedule 2, Part 1 and of the Town and Country Planning (General Permitted Development) Order 2015 (as amended), or any orders amending or re-enacting that Order with or without modification, windows, dormer windows, roof lights or other openings shall not be formed at first floor or above (including the roof) in the north, east or south elevations of the development hereby approved (other than those expressly authorised in the drawings) without consent in writing from the Local Planning Authority.

Reason: In the interests of the amenities of the occupiers of the adjoining residential properties and to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND1 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

8. Bin store provision

Notwithstanding that shown on the approved plans, prior to the commencement of the above ground construction of the development hereby permitted, details of the siting, size and design of the refuse and recycling bin storage area for the flats shall be submitted to and approved in writing by the Local Planning Authority. The refuse and recycling bin stores and facilities shall then be provided in accordance with the approved details prior to the first occupation of the development and retained as approved thereafter.

Reason: In the interests of amenity, to provide adequate refuse and recycling facilities and provide satisfactory form of development and to comply with policy EE1 of the Runnymede 2030 Local Plan, policy ND5 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

9. Cycle storage

The development hereby approved shall not be first occupied unless and until each of the proposed dwellings have been provided with bicycle parking in a robust, secure enclosure in accordance with the approved plan. The bicycle parking shall thereafter be retained and maintained in accordance with the approved details. Within the proposed cycle storage, facilities for the charging of e-bikes are to be provided, consisting of a standard three-point plug socket.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with policy SD7 of the Runnymede 2030 Local Plan, policy TT2 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

10. Access

No part of the development shall be first occupied unless and until the proposed modified access to 16A Beechtree Avenue has been constructed and provided with visibility zones in general accordance with the approved plans. Thereafter the visibility zones shall be kept permanently clear of any obstruction over 0.6m high.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to comply with policy SD4 of the Runnymede 2030 Local Plan, and guidance within the NPPF.

11. Electric Vehicle Charging

The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge Electric Vehicle Charging Point (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with the Proposed Parking Layout (21.1580.2200 – Rev B). The Electric Vehicle Charging Points shall thereafter be retained and maintained operation in accordance with these approved details.

Reason: In order that the development does not prejudice highway safety nor cause inconvenience to other highway users and to comply with policy SD4 of the Runnymede 2030 Local Plan, policy TT1 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

12. Parking/Turning

The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: In order that the development does not prejudice highway safety nor cause inconvenience to other highway users and to comply with policy SD4 of the Runnymede 2030 Local Plan, policy TT1 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

13. Landscape Environmental Management Plan (LEMP)

The above ground construction of the development hereby approved shall not commence until a Landscape Environmental Management Plan, which details the management measures to deliver the biodiversity net gain set out in the Biodiversity Impact Calculation Report, prepared by The Ecology Co-op (dated 26th June 2023), has been submitted to and approved in writing by the Local Planning Authority. The Landscape Environmental Management Plan shall include, but not be limited to the following:

- a) Description and evaluation of features to be managed.
- b) Ecological trends and constraints on site that might influence management.
- c) Aims and objectives of management.
- d) Appropriate management options for achieving aims and objectives.
- e) Prescriptions for management actions, together with a plan of management compartments.
- f) Preparation of a work schedule (including annual work plan capable of being rolled forward over a five-year period).
- g) Details of the body or organisation responsible for implementation of the plan.

- h) Ongoing monitoring and remedial measures.
- i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.

The development shall thereafter be implemented and maintained in accordance with the approved details.

Reason: To enhance the biodiversity of the site and to comply with policies EE9 and EE11 of the Runnymede 2030 Local Plan; policies NE1, NE2 and NE3 of the Englefield Green Village Neighbourhood Plan; and guidance within the NPPF.

14. Construction Environmental Management Plan (CEMP)

Prior to commencement of development, including demolition, a Construction Environmental Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The Plan shall include but not be limited to:

- a) A map showing the location of all of the ecological features.
- b) A risk assessment of the potentially damaging construction activities.
- c) Practical measures to avoid and reduce impacts during construction.
- d) Details of the location and timing of works to avoid harm to biodiversity features.
- e) Details of the responsible persons and lines of communication.
- f) Details of protective fences, exclusion barriers and warning signs.

The development shall take place fully in accordance with the approved details.

Reason: To ensure there is no harm to biodiversity or protected species and to comply with policies EE9 and EE11 of the Runnymede 2030 Local Plan; policies NE1, NE2 and NE3 of the Englefield Green Village Neighbourhood Plan; and guidance within the NPPF.

15. External lighting

Before any external lighting is installed at the site, details shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include proposed hours of use and measures to ensure that no direct light is projected into the atmosphere above the lighting installation. Development shall be carried out in accordance with the approved details and be retained as such thereafter.

Reason: To protect wildlife and to comply with Policies EE2 and EE9 of the Runnymede 2030 Local Plan; policies ND1, NE1 and NE2 of the Englefield Green Village Neighbourhood Plan; and guidance within the NPPF.

16. Renewable energy (as approved)

Prior to the first occupation of the development hereby permitted, solar photovoltaic panels, in accordance with the details set out in the Energy Statement prepared by thermenergy, dated July 2023, shall be submitted to and approved in writing by the Local Planning Authority. The solar panels shall thereafter be retained and maintained operational for the lifetime of the development.

Reason: To ensure sustainable design and to comply with policies SD7 and SD8 of the Runnymede 2030 Local Plan, policy ND6 of the Englefield Green Village Neighbourhood Plan, and guidance within the NPPF.

17. Surface water drainage

The above ground works of the development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

- a) The results of infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels.
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 (+35% allowance for climate change) & 1 in 100 (+45% allowance for climate change) storm events during all stages of the development. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate equivalent to the pre-development Greenfield run-off including multifunctional sustainable drainage systems.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.). Confirmation is required of a 1m unsaturated zone from the base of any proposed soakaway to the seasonal high groundwater level and confirmation of half-drain times.
- d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.
- e) Details of drainage management responsibilities and maintenance regimes for the drainage system.
- f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

The approved drainage strategy shall be implemented in full and shall thereafter be maintained in accordance with the approved details.

Reason: To ensure the design meets the non-statutory technical standards for SuDS and the final drainage design does not increase flood risk on site and to comply with policy EE13 of the Runnymede 2030 Local Plan policy NE2 of the Englefield Green Village Neighbourhood Plan, and guidance in the NPPF.

18. Drainage verification

Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

Reason: To ensure the design meets the non-statutory technical standards for SuDS and the final drainage design does not increase flood risk on site and to comply with policy EE13 of the Runnymede 2030 Local Plan policy NE2 of the Englefield Green Village Neighbourhood Plan, and guidance in the NPPF.

Appendices – 16A Beechtree Avenue – RU.23/1035

Location Plan



Proposed Site Plan



Proposed Land Uses Plan



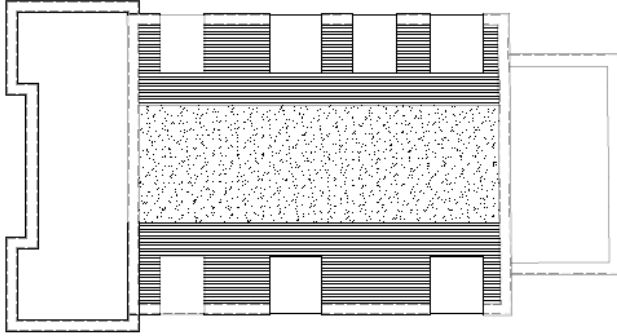
KEY

- Site Boundary (0.41Acres)
- Residential & Outbuildings
(0.07 Acres) 17%
- POS & LAP
(0.09 Acres) 22%
- Landscaping
(0.08 Acres) 20%
- Roads, Parking & Paths
(0.17 Acres) 41%

Proposed Access Plan



Proposed Floor Plans



ROOF PLAN



SECOND FLOOR PLAN



FIRST FLOOR PLAN

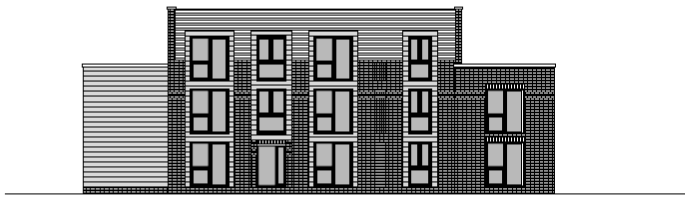


GROUND FLOOR PLAN

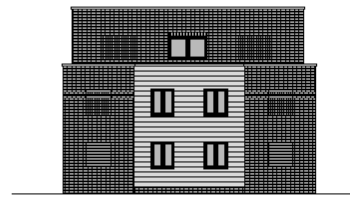
Proposed Elevations



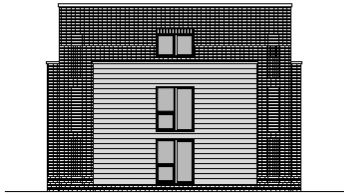
PROPOSED FRONT ELEVATION (WITH EXISTING CONTEXT)



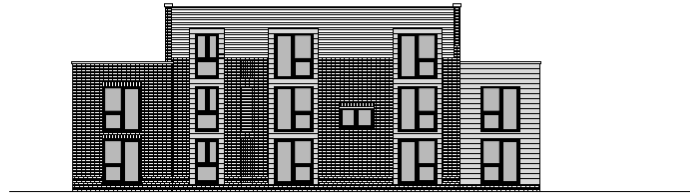
PROPOSED FRONT ELEVATION



PROPOSED SIDE ELEVATION 1



PROPOSED SIDE ELEVATION 2



PROPOSED REAR ELEVATION